

RUTH AND GOING

ENGINEERING — PLANNING — ARCHITECTURE



FUTURE WIDTH LINE STUDY

SANTA CLARA COUNTY

CALIFORNIA



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ENGINEERING — PLANNING — ARCHITECTURE

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February 12, 1971

Mr. James T. Pott
Santa Clara County
Department of Public Works
70 West Hedding Street
San Jose, California 95110

Dear Mr. Pott:

We take pleasure in submitting this report in accordance with our agreement dated July 23, 1969, pertaining to Future Width Lines of County Roads. This report summarizes the available data on major roadways and presents recommended policies of street width, street dedication and improvement, access and utility location.

In a subsequent report a roadway network for the South County area is presented. Ultimately our objective is the preparation of a set of maps depicting future width lines for all major roadways in the County.

Throughout the preliminary stages of this project we have worked closely with the fifteen jurisdictions of Santa Clara County. We are pleased at this opportunity to participate in formulating these policies which we consider logical and equitable.

Very truly yours,


LEO W. RUTH, JR.

LWR:ed

PART I

FUTURE WIDTH LINE STUDY

- **COUNTY OF SANTA CLARA
BOARD OF SUPERVISORS**

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2nd DISTRICT - DOMINIC CORTESE
3rd DISTRICT - CHARLES A. QUINN
4th DISTRICT - RALPH H. MEHRKENS
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- **COUNTY EXECUTIVE**

HOWARD W. CAMPEN

- **DEPARTMENT OF PUBLIC WORKS**

JAMES T. POTT, Director

DEAN LARSON, Engineering Services Division

ALFRED CHAN, Engineering Services Division

PREPARED FOR THE COUNTY OF SANTA CLARA · FEB., 1971.

RUTH AND GOING INC.

ACKNOWLEDGEMENTS

We express our gratitude to the State of California, Division of Highways, The League of California Cities, The County Engineers Association of California, The Municipal Public Works Officials of Santa Clara County, to each of the fifteen jurisdictions within Santa Clara County, and especially to members of the staff of the Santa Clara County, Department of Public Works for their time, resources and most willing assistance.

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SUMMARY AND RECOMMENDATIONS

The recommended policies for street width, street dedication, improvement, access and utility location for Santa Clara County are summarized below.

STREET WIDTH POLICY

It is recommended that Santa Clara County adopt street cross section standards which conform to the recommended right-of-way and roadway widths for all unincorporated areas, except that in instances where County land exists within or adjacent to a City and it is clear that this area is substantially developed and within the zone of influence of that City, these higher standards would not necessarily be required.

The typology of roads or streets recommended are:

Minor Streets:

40' roadway within a
60' right-of-way

Residential Cul-de-Sacs:

36' roadway within a
56' right-of-way

Collector Streets:

40' roadway within a
60' right-of-way

Industrial Streets:

50' roadway within a
70' right-of-way

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36' roadway within a
56' right-of-way

Collector Streets:

40' roadway within a
60' right-of-way

Industrial Streets:

50' roadway within a
70' right-of-way

Arterial Streets:

Undivided

72' roadway within a
92' right-of-way including a (4 lane)
4' paved, striped median

Divided

Two 34' roadways within a
110' right-of-way including a (4 lane)
22' effective median
Two 46' roadways within a
134' right-of-way including a (6 lane)
22' effective median

Expressways:

Two 34' roadways within a
110' right-of-way including a (4 lane)
22' effective median
Two 46' roadways within a
134' right-of-way including a (6 lane)
22' effective median

Frontage Roads:

34' roadway within a
48' right-of-way

STREET DEDICATION POLICY (Each lot or parcel)

Category 1: Single family residential on large parcels having twenty to sixty acres, with frontage on a County maintained road.

Dedication required: One-half of the proposed right-of-way width, not to exceed 30 feet.

Category 2: Single family and all duplex residential units on parcels having less than 20 acres.

Dedication required: One-half of the proposed right-of-way width, not to exceed 30 feet.

Category 3: All other uses.

Dedication required: One-half of the proposed right-of-way width not to exceed 67 feet.

STREET IMPROVEMENT POLICY (Each lot or parcel)

Category 1: Single family residential on large parcels having twenty to sixty acres, with frontage on a County maintained road.

Improvement required for:

- (a) Publicly maintained roads: None, except for driveway approaches.
- (b) Private roads: Stage improvements as required commensurate with the need for safe traffic movements and adequate access; however, if the parcels are not approved building sites, improvements need not be required.

Category 2: Single family and all duplex residential units on parcels having less than 20 acres.

Improvement required for:

- (a) Publicly maintained roads: One-half of the proposed right-of-way width not to exceed 30 feet.
- (b) Private roads: Stage improvements commensurate with the need for safe traffic movements and adequate access not to exceed 30 feet.

Category 3: All other uses.

Improvement required: One-half of the proposed right-of-way not to exceed a maximum of 46 feet.

ACCESS POLICY

The County should acquire access rights to expressways, wherever feasible. Where the County has purchased rights-of-way but not acquired access rights, a developer requesting access to the expressway should be required to dedicate and improve an additional 12 feet of roadway width.

The County should encourage backup or side on lot treatments where applicable, thus retaining the capacity of the roadway, reducing conflicting movements and providing ease of traffic circulation while limiting access.

UTILITY POLICY

Recommendations for utility locations are:

- (1) to place the gas, primary and secondary electrical conduits, telephone lines and television cables in a joint trench which splits the back edge of the sidewalk;

- (2) to locate electroliers at the back of walk for monolithic sidewalks less than six feet in width and in all other cases two feet from face of curb;
- (3) to situate the storm drain five feet from the roadway centerline and the sanitary sewer five feet on the opposite side of the centerline;
- (4) to establish the water main at five feet from face of curb on the far side from the sanitary sewer.

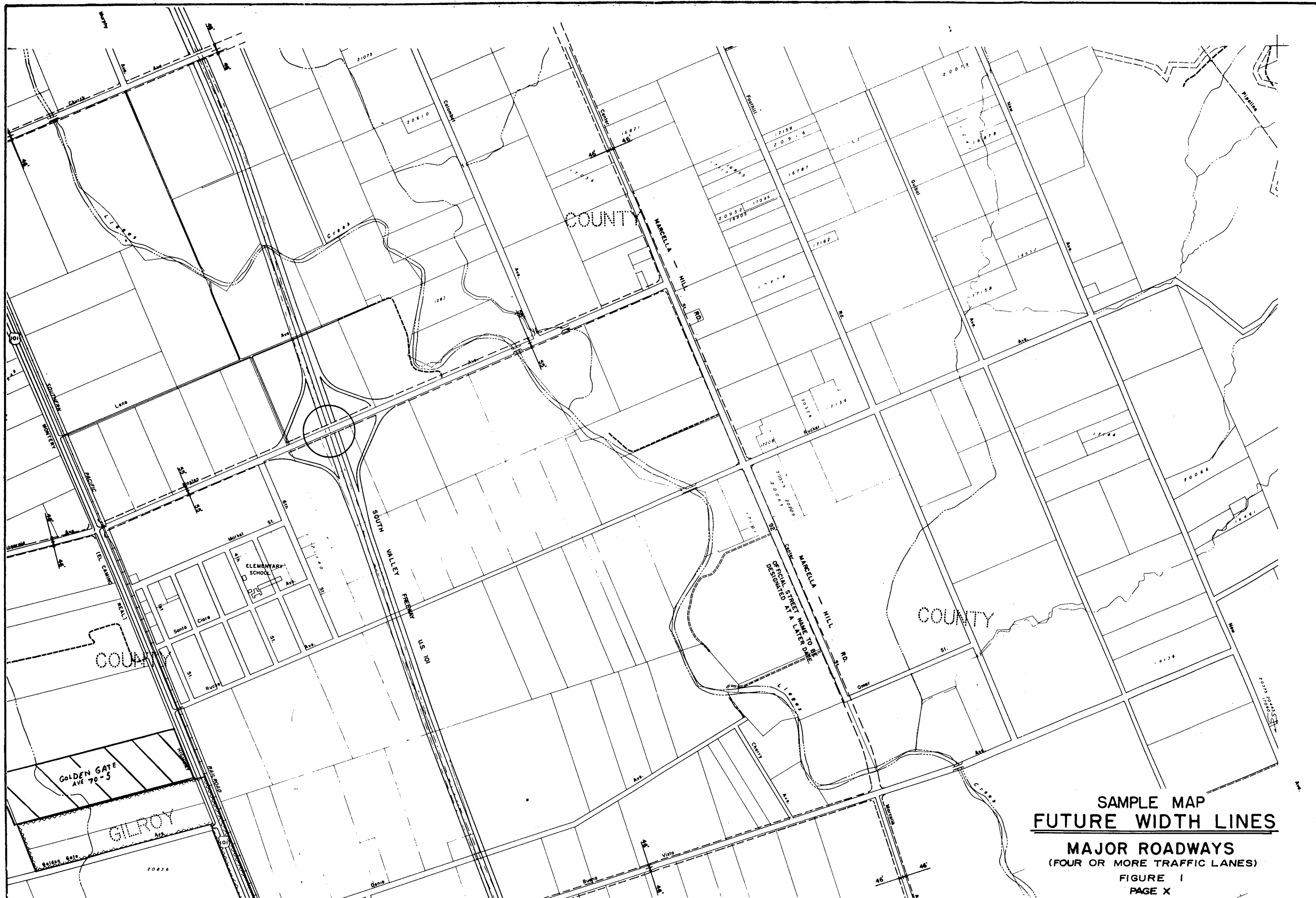
For a major roadway, except expressways, cross section modifications are:

- (1) to situate the parallel water mains each at five feet from face of curb;
- (2) to establish two parallel sanitary sewers, each at ten feet from face of curb.

FUTURE WIDTH LINE MAPS

It is recommended that a set of maps depicting future width lines for all major roadways in Santa Clara County be prepared. These maps will enable the County to identify the amount of street dedication and improvement that should be required when development occurs adjacent to these planned roadways. A sample map is shown as Figure 1.

Future Width Line maps for the South County Area are subsequently presented as a separate submittal along with a supplemental report.



INTRODUCTION

Santa Clara County is beset with the problem of establishing street dedication and improvement requirements for the entire County as specific individual areas develop. At the present time there exists a variety of standards for street widths, an assortment of street dedication and improvement policies, and differences in access and utility location policies among the individual jurisdictions located within the County. Since development occurs in a sporadic fashion, it is necessary to establish and adopt street width, street dedication and improvement, access and utility location policies which may be uniformly applied to all developments within the County.

Generally the cities have disagreed with the County's street dedication and street improvement policies, and the requirement for increased right-of-way widths. The County standards for right-of-way widths for major roadways while in excess of the cities current standards provide for increased vehicular capacity and safety. In most instances the major roadways in the cities do not conform to the County's right-of-way width standards. The primary reason is that the lands adjacent to these major roadways are developed probably to their fullest potential, and have been for some time. Merely to provide increased width appears unjustifiable for these areas at this time. Also the majority of the cities have required street dedication and street improvement in excess of the amount recommended as the County's policy. If the cities were to adopt the County's policy as stated later in this report, supplementary funds would be required to purchase and improve the right-of-way which was not dedicated and improved by the developer.

This report comprises the initial phase of work to ultimately establish future width lines for County roads. In a subsequent report the roadway network for the South County Area is examined in greater detail. Inherent to this report is the assumption that the public will continue to utilize privately owned vehicles as a primary mode of transportation. The introduction of mass transit will not influence the recommended roadway widths as they are a function of the safe and efficient utilization of the traveled ways.

Presented herein are the recommended policies for street width, street dedication and improvement, access and utility location, together with data on the cities' major roadways, capital improvements and official plan lines. It is imperative that a workable system of uniform land development policies be created and then consistently applied to all developments within Santa Clara County. This culmination point may be accomplished only through the coordinated efforts of the individual jurisdictions located within Santa Clara County.

STREET WIDTH POLICY

Discussion

The fifteen cities within Santa Clara County were contacted and their policies for street dedication, improvement and cross sections ascertained. Some of the cities have a complete policy established by Ordinance to cover these items but in many instances the final policy interpretation is open to some variation as interpreted by the City Staff. In these instances, an effort was made to determine the current interpretation and this interpretation, along with the original Ordinance, is what is reflected in the following discussion. Other standards which were reviewed consisted of the standards established by the League of California Cities, the Public Works Guide, County Engineers Association of California and the Santa Clara County Municipal Public Works Officials' Organization. The State of California was also contacted to determine the effect of Federal-aid Secondary (FAS) Standards and proposed State freeway off-ramp construction on County roadways.

Definitions

Various terms as used in this report are subsequently defined:

Major Roadway A roadway with four or more moving traffic lanes.

Roadway That portion of the right-of-way which is improved, designed or ordinarily intended for vehicular use.

Right-of-Way A general term denoting land, property, or interest therein, usually in a strip acquired for or devoted to a road, street or highway.

Border That portion of the right-of-way which lies between the curb line and the property line.

Median That portion of a divided highway separating the

traveled ways for traffic in opposite directions.

Private Road Any road not part of the City, County or State road systems.

Other definitions conforming to Santa Clara County Municipal Public Works Officials' Standards (modified as necessary to meet County expressway needs), are utilized.

Minor Street A street that serves the local needs of residential properties in a neighborhood having intersections at grade with direct access to abutting properties.

Collector Street A street that serves abutting property and carries traffic to the arterials and expressways having intersections at grade with direct access to abutting property.

Industrial Street A street serving traffic within an industrial development having intersections at grade with direct access to abutting property.

Arterial Street A street that serves a large volume of vehicular traffic with intersections at grade and generally having direct access to abutting property on which geometric design and traffic control measures are used to expedite the safe movement of through traffic.

Expressways A divided street or highway which serves large volumes of through traffic with restricted access and with grade separation at major intersections as necessary. Generally adjacent property does not have access directly to an expressway and parallel frontage roads are constructed where necessary.

Frontage Road A street that serves local needs having intersections at grade with direct access to abutting property on one

side only. This type of street is utilized in conjunction with and adjacent to a major traffic carrier which does not have access to abutting property, or where there is no access on one side of the road as in the case of a railroad or channel.

Street Cross Sections

The recommended street cross section standards for various street classifications are similar to those standards adopted by the Municipal Public Works Officials of Santa Clara County for minor and collector streets. For industrial streets, arterials and expressways, the recommended standards vary from the Municipal Public Works Officials of Santa Clara County Standards in order to provide for increased traffic safety, ease of turning movements and future lanes. Major roadways with four or more moving lanes generally adhere to minimum FAS Urban Extension Standards, thus retaining eligibility for UE funds.

Hillside standards are omitted from the discussion since each instance is considered unique. The variation in the amount of right-of-way which may be required, for example, for separate truck lanes creates awkward situations in attempting establishment of hillside standards.

Other roadways, such as recreational or scenic routes requiring special consideration, were also excluded.

Minor Streets: The prevailing right-of-way width for a minor street is 60 feet with 40 feet provided between curb faces. This cross section provides for two 12-foot traffic lanes, two 8-foot parking lanes and two 10-foot sidewalk and planter areas. In some instances this cross section may be narrowed such as on short cul-de-sacs or where a 60-foot right-of-way is not available. The method of narrowing this cross section is usually to reduce the distance between curb faces to 36 feet. This will then provide for a 10-foot traffic lane in each

direction and two 8-foot parking lanes. With this reduced roadway section, the right-of-way required is 56 feet. Another manner of decreasing the right-of-way width is to decrease the 10-foot area on each side of the roadway which has been provided for sidewalk, utilities and planter area. In some instances, the border area may be reduced to a minimum of 5 feet on each side with the sidewalk and utilities in easements outside of the right-of-way. An additional 5 feet of building setback should be required in these cases.

The current County right-of-way width standard for a residential cul-de-sac street is 52 feet.

The following recommendation for minor streets does not preclude innovative roadway designs which are acceptable to the Department of Public Works.

Recommendation (1) That all minor streets with the exception of residential cul-de-sacs have a 40-foot roadway as part of a 60-foot right-of-way width;

(2) that residential cul-de-sacs have a 36-foot roadway combined with a 56-foot right-of-way width.

Collector Streets: Collector streets for the majority of the cities within Santa Clara County have a right-of-way width of 60 feet with a paved roadway width between curbs of 40 feet. This street cross section provides for two 12-foot traffic lanes, two 8-foot parking lanes and two 10-foot sidewalk and planter areas. This cross section can only be reduced in width by reducing the border area. The roadway width of 40 feet should not be reduced for a collector street, as the narrower lane width which would result will not provide for a safe movement of large volumes of traffic.

Recommendation: That all collector streets have a 40-foot roadway combined with a 60-foot right-of-way width.

Industrial Streets: Many of the cities do not designate a specific industrial street but utilize a collector street for this purpose. A street designated specifically to serve an industrial area must have certain characteristics which are not found within a minor or collector street. These characteristics are a greater roadway width to more conveniently and safely provide parking and travel for the average truck unit, and freedom of turning movements into and out of industrial properties. The cross section recommended is 50 feet from face of curb to face of curb with a 10 foot border area for landscaping, utilities and sidewalk if required. This width slightly exceeds the 48 foot width recommended by the League of California Cities, and provides for a more convenient turning movement for a truck entering or leaving a driveway at 90° to the street. This cross section could be narrowed by reducing the area provided for sidewalk, landscaping and utilities to 5 feet on each side of the roadway, resulting in a right-of-way requirement of 60 feet. The roadway width should not be reduced below a 50 foot width.

Recommendation: That all industrial streets so designated have a 50-foot roadway combined with a 70-foot right-of-way width.

Arterial Streets: City standards for arterial streets vary to a large degree. The standards which each city has developed, depend on the size of the city and type of development within the city. Larger jurisdictions with complex zoning and land use patterns have developed a greater variety of standards. Some cities utilize a 90-foot right-of-way with roadway widths ranging from 64 to 70 feet. City standards for undivided arterials generally do not provide the extra roadway width required to increase the capacity of the roadway. The conflicts between

parked and moving vehicles, and between vehicles moving in opposite directions remain uncorrected. Nor do the standards for divided arterials provide the accepted minimum requirements for a left-turn storage lane with median island. To eliminate these shortcomings and to meet the minimum requirements for UE funds, the standards for Santa Clara County should follow the Federal-aid Secondary Urban Extension Standards for undivided arterials. These standards provide 12-foot moving lanes, 8-foot parking lanes with 2-foot shyaway, 10-foot sidewalk and utility areas, and a minimum of 4 feet for a paved median. This 72-foot roadway also provides sufficient width for a conversion to six 12-foot moving lanes with parking banned if in the future increased capacity is required and additional right-of-way is not obtainable. However, in most instances the arterial streets should have an 18-foot curb to curb median with a 2-foot shyaway on both sides. This 22-foot effective median may be used for left-turn control, or a continuous left-turn lane and left-turn storage at intersections.

If widening is required on an existing street to increase the capacity of the roadway, it may be necessary to incorporate a two-way center stacking lane within the street cross section. This street cross section would include 10-foot border areas, 8-foot parking lanes with 2-foot shyaway, 12-foot moving lanes and a 14-foot two-way center stacking lane with a 2-foot shyaway. The required right-of-way is 106 feet with an 86-foot roadway width.

Recommendation: (1) That undivided arterials have a minimum 92 foot right-of-way width in combination with a 72 foot roadway. The roadway includes as one of its elements, a 4 foot paved, striped median;

(2) That divided arterials have rights-of-way of 110 feet (4 moving lanes) and 134 feet (6 moving lanes) with dual roadways of 34 feet and 46 feet respectively. Raised medians should have a 22-foot effective width. At major intersections where a left-turn storage lane is provided, the face of curb to face of curb median width should not be reduced to less than 6 feet.

Expressways: Expressway standards have been established by the League of California Cities, Municipal Public Works Officials of Santa Clara County and the State of California, Division of Highways. The standards recommended provide for a four-lane roadway section and six-lane roadway section. The cross sections recommended provide for 12-foot moving lanes, 8-foot disabled vehicle parking lanes, 10-foot planter and utility areas, 2-foot shyaway on each side of the moving lane roadway and an 18-foot curb to curb median. This median width will provide for storage at intersections.

Recommendation: That expressways have either 4 or 6 moving lanes with restricted access. Required rights-of-way are 110 feet and 134 feet with dual roadways of 34 feet and 46 feet respectively. Raised medians should have a 22-foot effective width. At major grade intersections where a left-turn storage lane is provided, the curb to curb median width should not be reduced to less than 6 feet.

Frontage Roads: Frontage roads are utilized adjacent to a roadway, railroad, channel or other facility that prohibits or restricts access to abutting properties on one side. The cross section is similar to a collector street, except for three modifications. The three changes are (1) elimination of parking and sidewalk on the side adjacent to the non-access roadway, (2) reduction of the distance from face of curb to the non-access roadway fence to 4 feet, and (3) a 2-foot shyaway from one curb. The resulting required right-of-way is therefore 48 feet. Normally this cross section is adequate to serve the needs of abutting lands zoned for residential duplexes and less intense uses. However, this width may not be sufficient for landscaping adjacent to railroads or channels. Additional right-of-way may be required if landscaping is to be provided.

For more intense uses a frontage road cross section of two 12-foot moving lanes, two 8-foot parking lanes, a 10-foot planter and utility area on one side and a 2-foot distance from face of curb to the non-access roadway fence should be considered. Many instances have been noted where vehicular parking occurred on both sides of a frontage road, even though parking had been provided on only one side. In instances such as these, major conflicts are added, rather than alleviated. Reductions of conflicting movements may be obtained by providing two parking lanes in the 52-foot right-of-way. If the 48-foot right-of-way is used, parking restrictions on the side adjacent to the major roadway should be enforced.

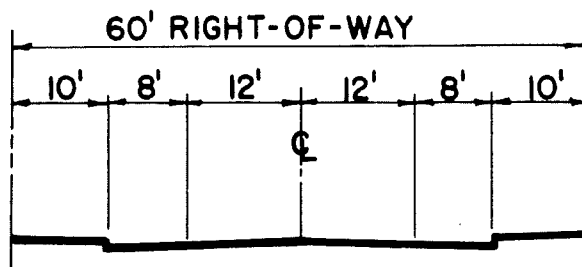
Recommendation: That all frontage roads have a minimum roadway of 34 feet in combination with a 48-foot right-of-way width.

MASS TRANSIT

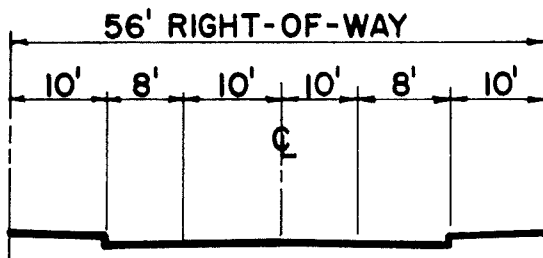
The effect of motorized mass transit on the County roadway system was explored. The street cross sections proposed can effectively accomodate mass transit vehicles. The lane widths are adequate for mass transit vehicles and the parking lanes wide enough to provide passenger loading space.

SUMMARY

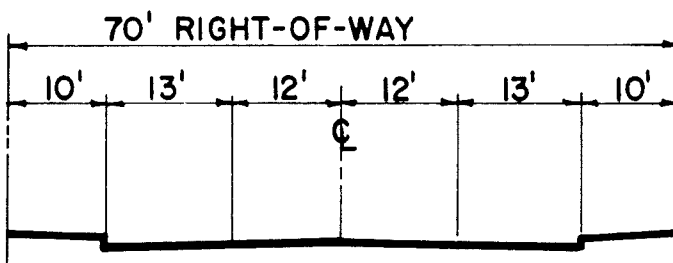
It is recommended that Santa Clara County adopt street cross section standards which conform to the recommended right-of-way and roadway widths as individually summarized above for all unincorporated areas, except that in instances where County land exists within the environs of a potential annexing city and it is clear that this area is substantially developed, these higher standards would not necessarily be required.



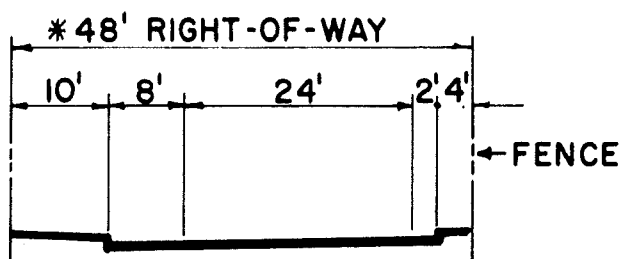
MINOR STREET & COLLECTOR STREET



RESIDENTIAL CUL-DE-SAC STREET



INDUSTRIAL STREET



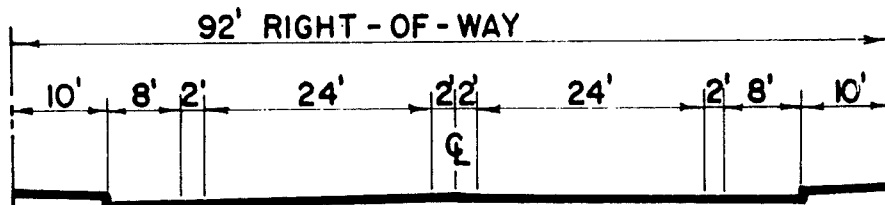
FRONTAGE ROAD

* INCREASE WIDTH IF
LANDSCAPING OR
PARKING ON TWO
SIDES IS REQUIRED.

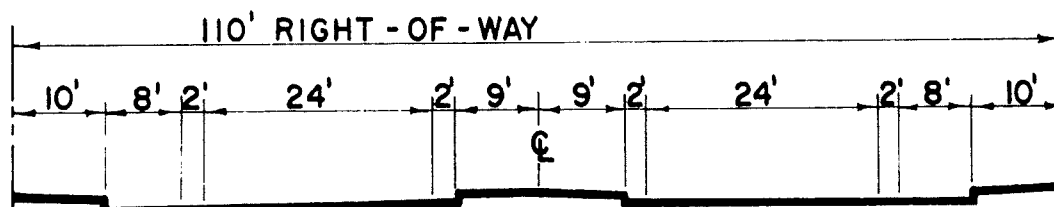
RECOMMENDED STREET CROSS SECTION STANDARD



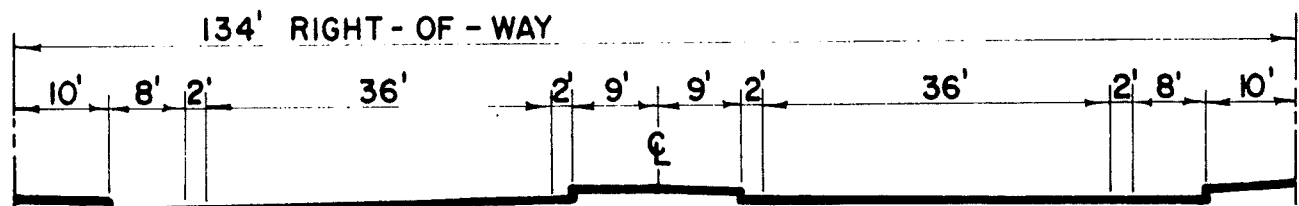
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4 LANE ARTERIAL



4 LANE DIVIDED ARTERIAL

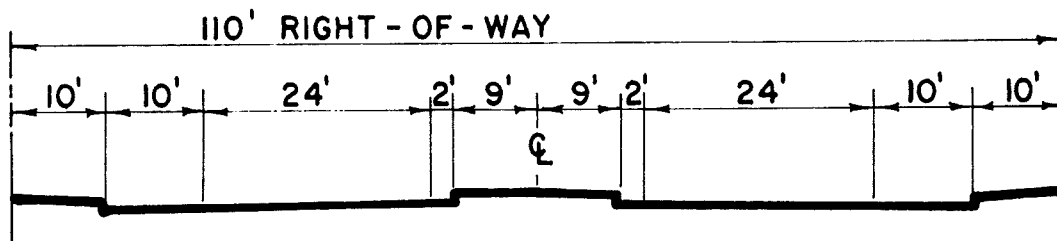


6 LANE DIVIDED ARTERIAL

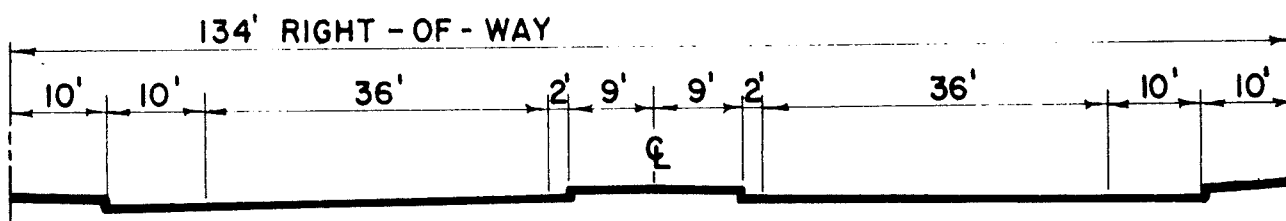
RECOMMENDED STREET CROSS SECTION STANDARD



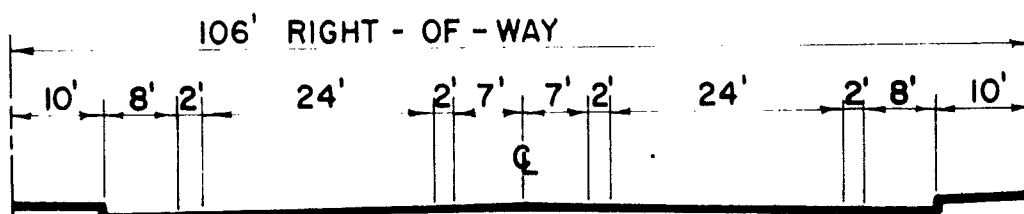
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4 LANE EXPRESSWAY



6 LANE EXPRESSWAY



4 LANE ARTERIAL TWO WAY CENTER LANE STACK
(FOR WIDENING ON EXISTING STREETS WHERE RIGHT-OF-WAY IS LIMITED)

RECOMMENDED STREET CROSS SECTION STANDARD



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TABLE I

MUNICIPALITIES' EXISTING STANDARDS FOR RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR SANTA CLARA COUNTY

JURISDICTION Street Component	STREET CATEGORY					
	Minor	Collector	Industrial	Arterial		Expressway
Campbell						
R/W	60'	60'	60'	80'	90'	120'
F/C to F/C	40'	40'	40'	64'	2-32'	2-44'
Moving Lanes	2-12'	2-12'	2-12'	4-12'	4-12'	6-12'
Parking Lanes	2- 8'	2- 8'	2- 8'	2- 8'	2- 8'	2- 8'
Median Width	-	-	-	-	6'	12'
Border	2-10'	2-10'	2-10'	2- 8'	2-10'	2-10'
Cupertino						
R/W	60'	60'	70'	80'	90'	120'
F/C to F/C	40'	40'	50'	60'	2-28'	2-43'
Moving Lanes	2-12'	2-12'	2-12'	4-11'	4-14'	6-12'
Parking Lanes	2- 8'	2- 8'	2-13'	2- 8'	-	2- 7'
Median Width	-	-	-	-	14'	14'
Border	2-10'	2-10'	2-10'	2-10'	2-10'	2-10'
Gilroy						
R/W	60'	64'	70'	86'	102'	
F/C to F/C	40'	42'	50'	66'	2-32'	
Moving Lanes	2-12'	2-12'	2-12'	4-12'	4-12'	
Parking Lanes	2- 8'	2- 9'	2-13'	2- 9'	2- 8'	
Median Width	-	-	-	-	18'	
Border	2-10'	2-11'	2-10'	2-10'	2-10'	
Los Altos						
R/W	40'	50'	60'	90'	100'	
F/C to F/C	24'	30'	40'	2-32'	2-32'	
Moving Lanes	2-12'	2-15'	2-12'	4-12'	4-12'	
Parking Lanes	-	-	2- 8'	2- 8'	2- 8'	
Median Width	-	-	-	6'	16'	
Border	2- 8'	2-10'	2-10'	2-10'	2-10'	
Los Altos Hills						
R/W	60'	60'	60'			
F/C to F/C	20'	22'	24'			
Moving Lanes	2-10'	2-11'	2-12'			
Parking Lanes	-	-	-			
Median Width	-	-	-			
Border	2-20'	2-19'	2-18'			

MUNICIPALITIES' EXISTING STANDARDS FOR
RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR
SANTA CLARA COUNTY

TABLE I
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TABLE I

MUNICIPALITIES' EXISTING STANDARDS FOR RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR SANTA CLARA COUNTY

JURISDICTION Street Component	STREET CATEGORY									
	Minor	Collector	Industrial		Arterial				Expressway	
Los Gatos										
R/W	56'	60'	60'	64'	84'	100'	108'	124'	100'	124'
F/C to F/C	36'	40'	44'	48'	64'	2-32'	88'	2-44'	2-24'	2-36'
Moving Lanes	2-10'	2-12'	2-12'	2-12'	4-12'	4-12'	6-12'	6-12'	4-12'	6-12'
Parking Lanes	2- 8'	2- 8'	2-10'	2-12'	2- 8'	2- 8'	2- 8'	2- 8'	-	-
Median Width	-	-	-	-	-	16'	-	16'	16'	16'
Border	2-10'	2-10'	2- 8'	2- 8'	2-10'	2-10'	2-10'	2-10'	2-18'	2-18'
Milpitas										
R/W	50'	60'	80'		90'	124'				
F/C to F/C	36'	40'	60'		70'	2-44'				
Moving Lanes	2-10'	2-12'	4-11'		4-12'	6-12'				
Parking Lanes	2- 8'	2- 8'	2- 8'		2-11'	2- 8'				
Median Width	-	-	-		-	16'				
Border	2- 7'	2-10'	2-10'		2-10'	2-10'				
Monte Sereno										
R/W	50'	60'	60'		84'	100'				
F/C to F/C	33'	40'	40'		64'	2-32'				
Moving Lanes	2-12'	2-12'	2-12'		4-12'	4-12'				
Parking Lanes	1- 9'	2- 8'	2- 8'		2- 8'	2- 8'				
Median Width	-	-	-		-	16'				
Border	2-8.5'	2-10'	2-10'		2-10'	2-10'				
Morgan Hill										
R/W	60'	60'	60'		80'					
F/C to F/C	40'	40'	40'		60'					
Moving Lanes	2-12'	2-12'	2-12'		4-11'					
Parking Lanes	2- 8'	2- 8'	2- 8'		2- 8'					
Median Width	-	-	-		-					
Border	2-10'	2-10'	2-10'		2-10'					
Mountain View										
R/W	60'	60'	70'		90'	100'	120'	120'		
F/C to F/C	40'	40'	50'		70'	2-32'	2-39'	2-44'		
Moving Lanes	2-12'	2-12'	2-12'		4-12'	4-12'	4-13'	6-12'		
Parking Lanes	2- 8'	2- 8'	2-13'		2-11'	2- 8'	2-13'	2- 8'		
Median Width	-	-	-		-	16'	18'	16'		
Border	2-10'	2-10'	2-10'		2-10'	2-10'	2-12'	2- 8'		

MUNICIPALITIES EXISTING STANDARDS FOR
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MUNICIPALITIES' EXISTING STANDARDS FOR RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR SANTA CLARA COUNTY

JURISDICTION	STREET CATEGORY									
	Street Component	Minor	Collector	Industrial	Arterial					Expressway
Palo Alto	R/W	60'	60'	60'	86'	100'				
	F/C to F/C	40'	40'	40'	60'	2-32'				
	Moving Lanes	2-12'	2-12'	2-12'	4-11'	4-12'				
	Parking Lanes	2- 8'	2- 8'	2- 8'	2- 8'	2- 8'				
	Median Width	-	-	-	-	12'				
	Border	2-10'	2-10'	2-10'	2-13'	2-12'				
San Jose	R/W	60'	64'	80'	80'	86'	90'	106'	120'	130'
	F/C to R/C	36'	40'	60'	60'	64'	66'	2-34'	96'	2-46'
	Moving Lanes	2-10'	2-12'	4-11'	4-11'	1-12'	4-12'	4-12'	6-12'	6-12'
	Parking Lanes	2- 8'	2- 8'	2- 8'	2- 8'	2- 8'	2- 9'	2-10'	2-12'	2-10'
	Median Width	-	-	-	-	-	14'	-	14'	20'
	Border	2-12'	2-12'	2-10'	2-10'	2-11'	2-12'	2-12'	2-12'	2- 9'
Santa Clara	R/W	60'	60'	70'	60'	84'	90'	100'	110'	120'
	F/C to F/C	40'	40'	50'	48'	64'	64'	2-32'	2-36'	2-44'
	Moving Lanes	2-12'	2-12'	2-12'	2-12'	4-12'	4-12'	4-12'	6-12'	6-12'
	Parking Lanes	2- 8'	2- 8'	2-13'	2-12'	2- 8'	2- 8'	2- 8'	-	2- 8'
	Median Width	-	-	-	-	-	16'	18'	16'	-
	Border	2-10'	2-10'	2-10'	2- 6'	2-10'	2-13'	2-10'	2-10'	2- 8'
Saratoga	R/W	56'	60'	60'		90'	100'	120'		
	F/C to F/C	36'	40'	40'		64'	2-32'	2-44'		
	Moving Lanes	2-10'	2-12'	2-12'		4-12'	4-12'	6-12'		
	Parking Lanes	2- 8'	2- 8'	2- 8'		2- 8'	2- 8'	2- 8'		
	Median Width	-	-	-		-	16'	16'		
	Border	2-10'	2-10'	2-10'		2-13'	2-10'	2- 8'		
Sunnyvale	R/W	58'	62'	66'	86'	90'	100'	120'		
	F/C to F/C	36'	40'	44'	64'	68'	2-38'	2-44'		
	Moving Lanes	2-10'	2-12'	2-12'	4-12'	4-12'	4-13/2-12'	6-12'		
	Parking Lanes	2- 8'	2- 8'	2-10'	2- 8'	2-10'	-	2- 8'		
	Median Width	-	-	-	-	-	16'	16'		
	Border	2-11'	2-11'	2-11'	2-11'	2-11'	2- 4'	2- 8'		
League of California Cities	R/W	56'	60'	60'	64'	84'	100'	108'	124'	100'
	F/C to F/C	36'	40'	44'	48'	64'	2-32'	88'	2-44'	2-24'
	Moving Lanes	2-10'	2-12'	2-12'	2-12'	4-12'	4-12'	6-12'	6-12'	4-12'
	Parking Lanes	2- 8'	2- 8'	2-10'	2-12'	2- 8'	2- 8'	2- 8'	2- 8'	-
	Median Width	-	-	-	-	-	16'	-	16'	16'
	Border	2-10'	2-10'	2- 8'	2- 8'	2-10'	2-10'	2-10'	2-10'	2-18'

(frontage roads)

MUNICIPALITIES EXISTING STANDARDS FOR
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MUNICIPALITIES' EXISTING STANDARDS FOR RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR SANTA CLARA COUNTY

JURISDICTION			STREET CATEGORY							
Street Component	Minor		Collector	Industrial	Arterial				Expressway	
County Engineers Association of California										
R/W	60'		60'	68'	84'	100'	120'			
F/C to F/C	36'		38'	48'	64'	2-36'	2-35'			
Moving Lanes	2-10'		2-11'	2-12'	4-12'	4-12'	4-12'			
Parking Lanes	2- 8'		2- 8'	2-12'	2- 8'	2-12'	2-11'			
Median Width	-		-	-	-	14'	18'			
Border	2-12'		2-11'	2-10'	2-10'	2- 7'	2-16'			
Mun. Pub. Works Off. of S.C.Co.										
R/W	60'		60'	64'	84'	88'	100'	104'	110'	134'
F/C to F/C	40'		40'	44'	64'	68'	2-32'	2-34'	2-37'	2-49'
Moving Lanes	2-12'		2-12'	2-12'	4-12'	4-12'	4-12'	4-12'	4-12'	6-12'
Parking Lanes	2- 8'		2- 8'	2-10'	2- 8'	2-10'	2- 8'	2-10'	2-13'	2-13'
Median Width	-		-	-	-	-	16'	16'	16'	16'
Border	2-10'		2-10'	2-10'	2-10'	2-10'	2-10'	2-10'	2-10'	2-10'
State Div. of Highways										
R/W									142'	166'
F/C to F/C									2-32'	2-44'
Moving Lanes									4-12'	6-12'
Parking Lanes									2- 8'	2- 8'
Median Width									22'	22'
Border									2-28'	2-28'
S.C.Co. Exist. Standards										
R/W	52'	60'	60'	70'	90'				110'	134'
F/C to F/C	36'	40'	40'	50'	70'				2-34'	2-46'
Moving Lanes	2-10'	2-12'	2-12'	2-12'	4-12'				4-12'	6-12'
Parking Lanes	2- 8'	2- 8'	2- 8'	2-13'	2-11'				2-10'	2-10'
Median Width	-	-	-	-	-				22'	22'
Border	2- 8'	2-10'	2-10'	2-10'	2-10'				2-10'	2-10'
Recommended Standards for S.C.Co.										
R/W	56'	60'	60'	70'	92'	110'	134'		110'	134'
F/C to F/C	36'	40'	40'	50'	2-34'	2-34'	2-46'		2-34'	2-46'
Moving Lanes	2-10'	2-12'	2-12'	2-12'	4-12'	4-12'	6-12'		4-12'	6-12'
Parking Lanes	2- 8'	2- 8'	2- 8'	2-13'	2-10'	2-10'	2-10'		2-10'	2-10'
Median Width	-	-	-	-	4'	22'	22'		22'	22'
Border	2-10'	2-10'	2-10'	2-10'	2-10'	2-10'	2-10'		2-10'	2-10'

NOTE: Medians or Parking Lanes may include side "shy away" area.

MUNICIPALITIES' EXISTING STANDARDS FOR
RIGHTS-OF-WAY AND TRAFFIC LANES
AND RECOMMENDATIONS FOR
SANTA CLARA COUNTY

STREET DEDICATION AND IMPROVEMENT POLICIES

Discussion:

The existing policies for street dedication and improvement of public roads were ascertained from the cities within Santa Clara County. All jurisdictions should possess a street dedication and improvement policy. It was revealed that the larger cities with more complex land uses have a correspondingly complex dedication and improvement policy, while the smaller cities have the simpler policies.

Most cities will attempt to obtain street dedication at the earliest stage possible. This may be at the rezoning, use permit approval, architectural approval or building permit stage. The cities will usually defer actual construction of the improvements until the land is proposed for development.

The manner of guaranteeing the future construction of the improvements varies. Most cities accept a surety type bond as assurance that the improvements will be installed. However, some cities require cash bonds, some will enter into deferred improvement agreements, some property liens, and one city requires the improvements to be installed before the rezoning becomes final.

A policy of street dedication and improvement usually depends on three factors. The first is the type of land use existing or proposed on the abutting property; the second is the type of street with its corresponding right-of-way width; and the third is the extent of access.

It is considered normal to require a development to dedicate and improve a street commensurate with the benefit received from that street. A normal situation may be interpreted as occurring with the concentric widening of an existing road or when the centerline of the

new road is coincident with the developer's property line. All other instances may be classified as other than normal and will account for non-concentric widening, offsets from property lines and frontage roads.

Roadway widths in excess of benefits received by adjacent property owners should be paid for by the public body controlling the development, since the extra roadway width is considered beneficial for the public in general and not specifically for the adjacent development.

The policy recommended for the County establishes three categories of land use as applied to each lot or parcel.

1st Category - is single family residential on large parcels having twenty to sixty acres with frontage on a County maintained road.

2nd Category - is single family and all duplex residential units on parcels having less than twenty acres.

3rd Category - is all other uses. In this third category, triplex and larger buildings are considered "commercial" ventures, just as are riding academies, guest ranches, quarries, golf courses, schools and churches.

The amount of street dedication and improvement required for each category of land use ~~must~~ not only be defined for street width and extent of access, but also for frontage. These items are further explained in the following sections.

Dedication:

Category 1 is comprised of parcels which may be further subdivided at a later date since it is assumed that final development has not occurred. Since street dedication should be obtained at the earliest possible date, it is considered appropriate that dedication of one half of the proposed right-of-way width, not to exceed 30 feet, be secured

at this time. Building setbacks from the future planned roadway may also be required.

Category 2 developments should be required to dedicate a maximum of 30 feet. The 30 feet comprises one half of the right-of-way for a minor or collector street. However, where a proposed street borders a property line, a 40 foot dedication may be required to accommodate two way traffic needs.

Category 3 consists of land essentially developed to its fullest potential. In this instance the maximum dedication should be 67 feet which is one half of the right-of-way for a 134 foot divided arterial.

When property abuts an expressway-type facility where the County has purchased right-of-way but not acquired access rights, the required amount of dedication should be as described in the "Access Policy" section of this report.

If a parcel has frontage on an existing or proposed private road which is part of a road pattern that potentially will be maintained by the County, the property owner should be required to offer for dedication an amount of land equal to that which is stipulated under Category 1, 2 or 3, whichever is applicable. When the road has been improved to County standards, such offer of dedication would be accepted by resolution of the Board of Supervisors.

When circumstances are other than normal, the developer should be responsible for acquiring title to all lands that are necessary to fulfill his minimum requirements for street dedication. Such a situation may occur due to offsets of property lines or in instances where the developer does not own the land which is required for his development.

This land acquisition or sale may require the aid of condemnation procedures by the County. The costs of the condemnation procedures are to be borne by the developer. If a residual parcel is isolated because of a street dedication action, the owner of the isolated parcel should, upon recordation of the final map of his developable land, enter into an agreement with the County to sell that isolated parcel in the future street right-of-way to the adjacent developer. The price of purchase should be at the fair market value of the parcel at the time the adjacent property is developed.

Frontage roads are also an exception to the dedication policy. Since development occurs on only one side, the developer should be required to dedicate the full right-of-way width of the frontage road and the County should not be obligated to acquire any of the required frontage road right-of-way.

The frontage to be dedicated would be the entire frontage of the development with exceptions and exemptions as provided by local ordinances and State law. In instances where remaining land adjacent to the development could not be separately developed, that frontage also would be dedicated. The "development" in this instance is defined as the actual proposed development whether it be a residence (including sideyards), a subdivision, minor land division, single building site, or entire area being rezoned.

When a parcel has frontage and access on two parallel roads, full dedication as defined by categories 1, 2 or 3 should be required on each road. If the parcel has frontage on two parallel roads but access on only one, the total dedication should not exceed that amount which would be required if the development fronted on only a major roadway.

In all instances of street dedication, the required amount should be measured from the present centerline or base line toward the future right-of-way line. This required amount of dedication should be obtained at the earliest possible date, for example, at the rezoning stage, to assure orderly development and insure against future acquisition of higher value improved land. The County should develop the necessary ordinances and procedures to implement such a policy.

Recommendation for Street Dedication: (each lot or parcel). It is recommended that Santa Clara County adopt a street dedication policy based on three categories of land use. Where access is permitted to the roadway in question, the amount of street dedication should be as follows:

Category 1: Single family residential on large parcels having twenty to sixty acres, with frontage on a County maintained road.
Dedication required: One half of the proposed right-of-way width, not to exceed 30 feet.

Category 2: Single family and all duplex residential units on parcels having less than 20 acres.
Dedication required: One half of the proposed right-of-way width not to exceed 30 feet.

Category 3: All other uses.
Dedication required: One half of the proposed right-of-way width, not to exceed 67 feet.

Improvements:

Improvements should be required relative to the type of development. The categories of land use and parcel size employed in the street dedication policy are considered applicable to street improvement. Because street dedication is based upon a need for the improvement of roads that potentially will be maintained by the County, it follows that these street improvements generally should be required at the time of development.

In some instances, however, it may be in the public interest to defer the construction work for the installation of the improvements as part of a larger street improvement project.

For Category 1, properties having access to publicly maintained roads, there should not be a requirement to improve any land for street purposes except, perhaps, for driveway approaches. For private roads and easements, stage improvements should be required commensurate with the need for safe traffic movements and adequate access as outlined in the Santa Clara County Land Development Regulation. However, if the parcels are not approved building sites, improvements need not be required. The parcel should contain only one single family residence plus the necessary farm buildings. Building setbacks from a future planned major roadway may be required.

Category 2 should require the developer whose property has access to a publicly maintained road, to improve a maximum of 30 feet. The 30 feet represents the border area, a parking lane, and a single moving lane of traffic and is one half of the right-of-way for a minor or collector street. For private roads and easements, the requirements are similar, except that if it is not practical to install full improvements at the time of development, these improvements should be staged commensurate with the need for safe traffic movements and adequate access. In the event the development is a subdivision full improvements should be required at the time of filing the subdivision map.

For Category 3, a maximum of 46 feet of improvement should be required. The 46 feet represents the border area, a parking lane and two moving lanes of traffic. For an undivided arterial it is one half of the right-of-way, and for a divided arterial it is the distance from the property line to the outer edge of pavement of the second traveled

way. Additional pavement width and medians are considered beneficial for through traffic. Both the developer and the County should share the right-of-way and improvement costs of the additional pavement width and median area. Since the developer is required to dedicate the additional 21 feet (67 - 46), the County should pay for the installation of these improvements.

An exception to the improvement policy may occur when only one half of the street is improved and the "half-street" adjoins unsubdivided land or land not occupied by a street or public way. In this instance, a minimum of 40 feet of improvements may be required of the developer. Such a roadway should provide a minimum roadway width of one parking lane and two through lanes.

Another exception is the frontage road. The developer should, in this instance, improve the full right-of-way width of the frontage road since development occurs only on one side. The County should not be obligated to improve any portion of the required frontage road right-of-way.

Also included as an exception are road links which may be necessary to complete a road network. Any street improvements required to provide such a link between a private road and a dedicated street, may also be included in the responsibility of the developer.

The frontage to be improved would be the entire frontage of the development with exceptions and exemptions as provided by local ordinances and State law. In instances where remaining land adjacent to the development could not be separately developed, that frontage also would be improved. The "development" in this instance is defined as the actual proposed development whether it be a residence (including sideyards), a subdivision, minor land division, single building site, or entire area being rezoned.

When a parcel has frontage and access on two parallel roads, full improvement as defined by categories 1, 2 or 3 should be required on each road. If the parcel has frontage on two parallel roads but access on only one, the total improvements should not exceed that amount which would be required if the development fronted on only a major roadway. The County may, at its discretion, offer the developer an alternative to construction of the required improvements. This option could entail increased dedication in lieu of the required improvements.

In all instances of street improvement, the required amount should be measured from the proposed right-of-way line toward the center of the road along the frontage of the property to be developed.

Recommendation for Street Improvements: (each lot or parcel)

It is recommended that Santa Clara County adopt a street improvement policy based on three categories of land use. Where access is permitted to the roadway in question, the amount of street improvement should be as follows:

Category 1: Single family residential on large parcels having twenty to sixty acres, with frontage on a County maintained road.

Improvement required for:

- (a) Publicly maintained roads: None, except for driveway approaches.
- (b) Private roads: Stage improvements as required commensurate with the need for safe traffic movements and adequate access; however, if the parcels are not approved building sites, improvements need not be required.

Category 2: Single family and all duplex residential units on parcels having less than 20 acres.

Improvement required for:

- (a) Publicly maintained roads: One half of the proposed right-of-way width not to exceed 30 feet.
- (b) Private roads: Stage improvements commensurate with the need for safe traffic movements and adequate access not to exceed 30 feet.

Category 3: All other uses.

Improvement required: One half of the proposed right-of-way not to exceed a maximum of 46 feet.

CITY or TOWN	ZONE	MINOR		COLLECTOR		INDUSTRIAL		ARTERIAL		EXPRESSWAY	
		D ⁽¹⁾	I ⁽²⁾	D	I	D	I	D	I	D	I
CAMPBELL	ALL	R ⁽³⁾ TO C ⁽⁴⁾ (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (60' MAX.)	R TO C (60' MAX.)	N.A. ⁽⁴⁾	N.A.
CUPERTINO	ALL	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (35')	R TO C (35')	R TO C (60' MAX.)	R TO C (60' MAX.)	N.A.	N.A.
GILROY	ALL	R TO C (30')	R TO C (30')	R TO C (32')	R TO C (32')	R TO C (35')	R TO C (35')	R TO 37.5' (MAX.)	R TO 37.5' (MAX.)	N.A.	N.A.
LOS ALTOS	R-1 RESIDENTIAL	R TO C (25' MAX.)	R TO C (25' MAX.)	R TO C (30')	R TO C (30')	N.A. ⁽⁴⁾	N.A.	C TO 35' FROM C (MAX.)	R TO 35' FROM R (MAX.)	N.A.	N.A.
LOS ALTOS HILLS	ALL	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	N.A.	N.A.	N.A.	N.A.	N.A.	N.A.
LOS GATOS	ALL	R TO C (28')	R TO C (28')	R TO C (30')	R TO C (30')	R TO C (32' MAX.)	R TO C (32' MAX.)	R TO C (62' MAX.)	R TO C (62' MAX.)	R TO C (62' MAX.)	R TO C (62' MAX.)
MILPITAS	ALL (EXCEPT SUBDIVISION DEVELOPM'T)	R TO C (25')	R TO C (25')	R TO C (30')	R TO C (30')	R TO C (40')	R TO 30' FROM R (MAX.)	R TO 45' FROM R (MAX.)	R TO 30' FROM R (MAX.)	N.A.	N.A.
	SUBDIVISIONS	R TO C	R TO C	R TO C	R TO C	R TO C (40')	R TO C (40')	R TO C (62' MAX.)	R TO C (62' MAX.)	N.A.	N.A.
MONTE SERENO	ALL (EXCEPT SINGLE LOT DEVELOPM'T)	R TO C (30' MAX.)	R TO C (30' MAX.)	R TO C (30')	R TO C (30')	N.A.	N.A.	R TO C (50' MAX.)	R TO C (50' MAX.)	N.A.	N.A.
	SINGLE LOT	R TO C (30' MAX.)	NONE	R TO C (30')	NONE	N.A.	N.A.	R TO C (50' MAX.)	NONE	N.A.	N.A.
MORGAN HILL	ALL (EXCEPT SINGLE LOT DEVELOPM'T)	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (40')	R TO C (40')	N.A.	N.A.
	SINGLE LOT	R TO C (30')	NONE	R TO C (30')	NONE	R TO C (30')	NONE	R TO C (40')	NONE	N.A.	N.A.
MOUNTAIN VIEW	ALL	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (35')	R TO C (35')	R TO C (60' MAX.)	R TO C (60' MAX.)	N.A.	N.A.
PALO ALTO	ALL	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (30')	R TO C (50' MAX.)	R TO C (50' MAX.)	N.A.	N.A.
SAN JOSE	R-1	R TO C (30')	R TO C (30')	R TO C (32')	R TO C (32')	R TO C (40')	R TO 30' FROM R (MAX.)	R TO C (65' MAX.)	R TO 30' FROM R (MAX.)	R TO 85' FROM R (MAX.)	R TO 44.5' FROM R (MAX.)
	R-2	R TO C (30')	R TO C (30')	R TO C (32')	R TO C (32')	R TO C (40')	R TO 37.5' FROM R (MAX.)	R TO C (65' MAX.)	R TO 47.5' FROM R (MAX.)	R TO 85' FROM R (MAX.)	R TO 57.5' FROM R (MAX.)
	APARTMENT, COMMERCIAL & INDUSTRIAL	R TO C (30')	R TO C (30')	R TO C (32')	R TO C (32')	R TO C (40')	R TO C (40')	R TO C (65' MAX.)	R TO C (65' MAX.)	R TO 85' FROM R (MAX.)	R TO 85' FROM R (MAX.)
SANTA CLARA	RESIDENTIAL & INSTITUTIONAL	R TO C (30')	R TO 20' FROM CURB EDGE (30')	R TO C (35' MAX.)	R TO 20' FROM CURB EDGE (30' MAX.)	R TO C (30')	R TO 20' FROM CURB EDGE (36')	R TO C (60' MAX.)	R TO 20' FROM CURB EDGE (39' MAX.)	N.A.	N.A.
	INDUSTRIAL & TRAILER COURTS & MOBILE HOMES	R TO C (30')	R TO 24' FROM CURB EDGE (36')	R TO C (35' MAX.)	R TO 24' FROM CURB EDGE (34' MAX.)	R TO C (30')	R TO 24' FROM CURB EDGE (30')	R TO C (60' MAX.)	R TO 24' FROM CURB EDGE (37' MAX.)	N.A.	N.A.
	COMMERCIAL	R TO C (30')	R TO 30' FROM CURB EDGE (40')	R TO C (35' MAX.)	R TO 30' FROM CURB EDGE (34' MAX.)	R TO C (30')	R TO 30' FROM CURB EDGE (36')	R TO C (60' MAX.)	R TO 30' FROM CURB EDGE (41' MAX.)	N.A.	N.A.
SARATOGA	ALL	R TO C (28' MAX.)	R TO C (28' MAX.)	R TO C (30')	R TO C (30')	N.A.	N.A.	R TO C (60' MAX.)	R TO C (60' MAX.)	N.A.	N.A.
SUNNYVALE	RESIDENTIAL & COMMERCIAL	R TO C (28')	R TO C (28')	R TO C (31')	R TO C (31')	R TO C (35')	R TO 20' FROM CURB EDGE (31')	R TO C (60' MAX.)	R TO 20' FROM CURB EDGE (31')	N.A.	N.A.
	INDUSTRIAL	R TO C (28')	R TO C (28')	R TO C (31')	R TO C (31')	R TO C (35')	R TO 22' FROM CURB EDGE (33')	R TO C (60' MAX.)	R TO 22' FROM CURB EDGE (33')	N.A.	N.A.

NOTES
 D - REQUIRED DEDICATION
 I - REQUIRED IMPROVEMENTS
 R - PROPERTY LINE
 C - CENTERLINE
 N.A. - NOT APPLICABLE

SUMMARY OF EXISTING STREET DEDICATION AND IMPROVEMENT POLICIES FOR THE CITIES OF SANTA CLARA COUNTY

ACCESS POLICY

Discussion:

Control of access reduces conflicts, improves driver comfort, decreases accident potential and increases the capacity of the roadway. Access control may be achieved by restricting driveways, employing back up lot designs or acquiring rights-of-access to the major roadway in question from the abutting property owners. The County should determine the extent of access control depending upon the classification of the major roadway.

Controlled access gives preference to through traffic and contains selected crossings either at grade or with grade separation. The majority of major roadways in the County do not have restricted access. To achieve restricted access, the County normally must purchase these rights from abutting property owners and, in a built-up area, the costs are often prohibitive. As an alternative to purchasing access rights, a back-up treatment for residential lots adjacent to major divided roadways should be considered. The justification for back-up lots is based upon a determination that control of access is essential to maintain the design capacity of the roadway through reduction of vehicular and pedestrian conflicts. Access control also creates more driver comfort and results in the decline of the accident rate. Residential development of lands which are abutting these planned divided roadways should back-up whenever feasible. The proper requirements of street dedication and improvement and access waiver should be fulfilled.

To meet certain traffic circulation requirements in a developed area, a minimum of 1,200 feet between median openings should be required with access openings at least 300 feet from a median opening. Circulation patterns within the development may vary with use of cul-de-sacs, minor and collector streets. The maximum extent of circuitous travel of approximately 2,400 feet is within reasonable limits of driver sensitivity. The local jurisdiction should review tentative street patterns to ensure minimum congestion at intersections to the major roadways.

Establishment of a County policy to require street dedications and improvement on back-up lot treatments (double frontage) on both the major roadway on which it backs and also on which it fronts is considered essential. Previous discussions of dedications and improvements required of developers have indicated the recommended policy.

For an expressway type facility where the County has purchased right-of-way but not acquired access rights, the following excerpt from Amendment No. 5 to the County of Santa Clara Expressway Policy Resolution dated May 21, 1962, should apply:

"In no case shall new public access with median opening permitted be closer than one-half mile to the next closest median opening or interchange. New public access, other than at the described one-half mile intervals, shall be limited to right-turn movements only and shall not be considered reason for a median opening. New public access not located at the one-half mile intervals described shall be a minimum distance of 1200 feet to the next nearest access. New public access

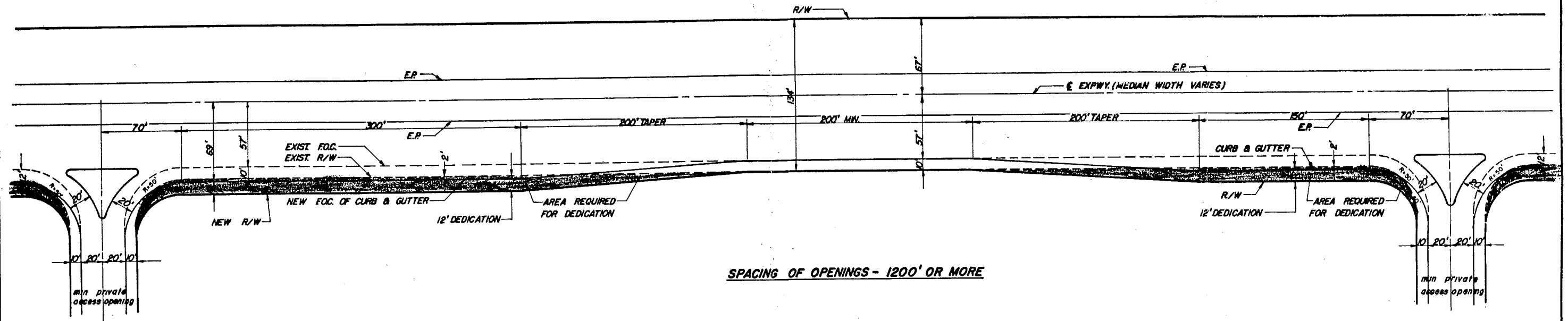
shall not be allowed at less than half-mile intervals for parcels or subdivisions having direct access to other public streets of major importance."

For an expressway type facility where the County has purchased right-of-way but not acquired access rights, private access openings should be controlled to retain the capacity of the roadway. A frontage of 600 feet between private access openings is considered a necessary minimum to insure a reduction of conflicts. When the spacing between private access openings is less than 1,200 feet, the required street dedication and improvement should be 12 feet for the full frontage to provide an extra lane in the vicinity of the traffic generator. When the spacing exceeds 1,200 feet the required street dedication and improvement should be a 12 foot tapered section to provide acceleration and deceleration lanes.

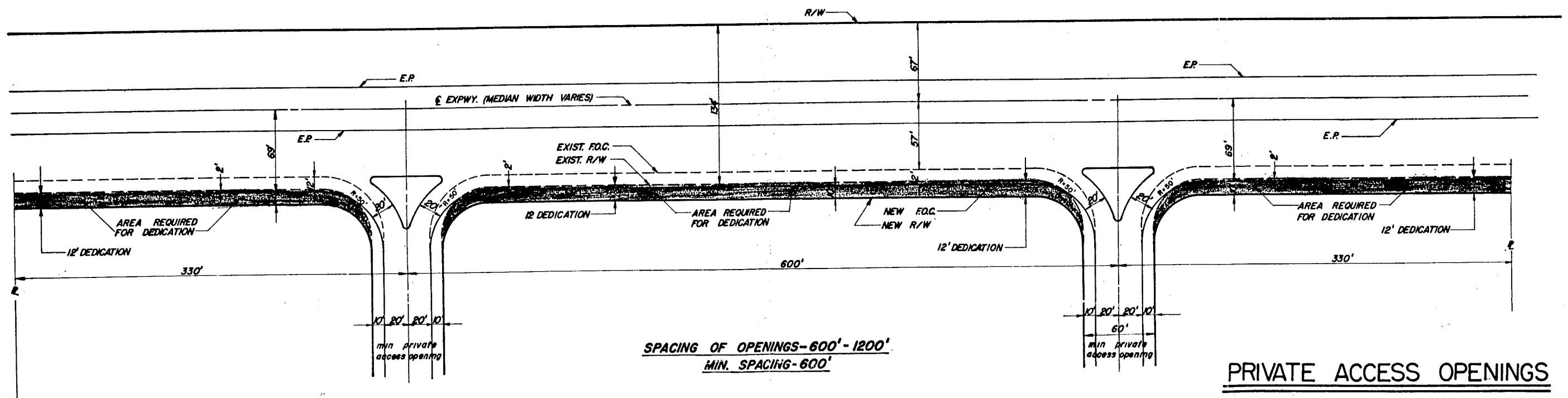
Recommendation: The County should require back-up type of developments adjacent to major roadways wherever it is feasible and practicable. The amount of street dedication and improvement required should be as enumerated in the recommended street dedication and improvement policies.

The County should purchase access rights to expressways, wherever feasible. Where the County has purchased rights-of-way but not acquired access rights, the developer should be required to dedicate and improve 12 feet of roadway width when developing the property adjacent to the expressway.

PRIVATE ACCESS OPENINGS
RIGHT TURNS ONLY



SPACING OF OPENINGS - 1200' OR MORE



SPACING OF OPENINGS - 600' - 1200'
MIN. SPACING - 600'

PRIVATE ACCESS OPENINGS

RIGHT TURNS ONLY

UTILITY LOCATION POLICY

Discussion:

Utility locations vary from jurisdiction to jurisdiction. Some cities within the County have standard drawings of utility locations, while others have flexible policies which they follow. We are concerned here with the storm drain, sanitary sewer, water main and electrolier locations. Also included in the discussion are locations for natural gas lines, primary and secondary electrical conduits, telephone lines and television cables.

Most cities are reliant upon Pacific Gas and Electric Company (PG&E) and Pacific Telephone and Telegraph Company (PT&T) or Western California Telephone Company (WCTC) for determining a trench location for gas, primary and secondary electrical conduits, telephone lines and television cables. Standard drawings are available which indicate preferred locations for these utilities. Whenever possible a joint trench in the border area serves equally well both PG&E and the Telephone Company. In many instances both organizations are not able to reduce costs by joint trenching if departmental approvals are not coincident. Every effort is made, however, to provide joint trenching.

A portion of the joint trench is normally located beneath the sidewalk to provide protection for underground utilities. Coordination and communication between the County and the utility companies are necessary to avoid breaking the sidewalk when repairing these utilities. Control of backfill is also required to maintain sidewalk stability.

The cities vary in their choice of an electrolier location with two feet from face of curb being the most popular. Variable electrolier locations exist because of variable sidewalk requirements in residential and non-residential zones. Sidewalks may be monolithic or may be placed in a separate pour. The width of the sidewalk especially in a non-residential area may vary depending on the type of development. Electrolier locations, therefore, are dependent on the development adjacent to the roadway and on the type of sidewalk constructed.

Water mains, storm drains and sanitary sewers also have variable locations depending upon which utility preceded the other. Some water mains are located within the sidewalk and planter area. Most water mains are located under the roadway pavement and may be five feet to fifteen feet from face of curb. If the City provides its own water facilities, the likelihood is greater that there will be an established pattern. For areas served by a private water company, water main locations may vary to avoid existing utilities.

Storm drains and sanitary sewers may also be located at various points within the street right-of-way. However, the prevailing combination is to locate the storm drain five feet on one side of the centerline and the sanitary sewer at five feet on the other side. These utilities are located in the approximate middle of a traveled way and not subject to direct wheel load pressures.

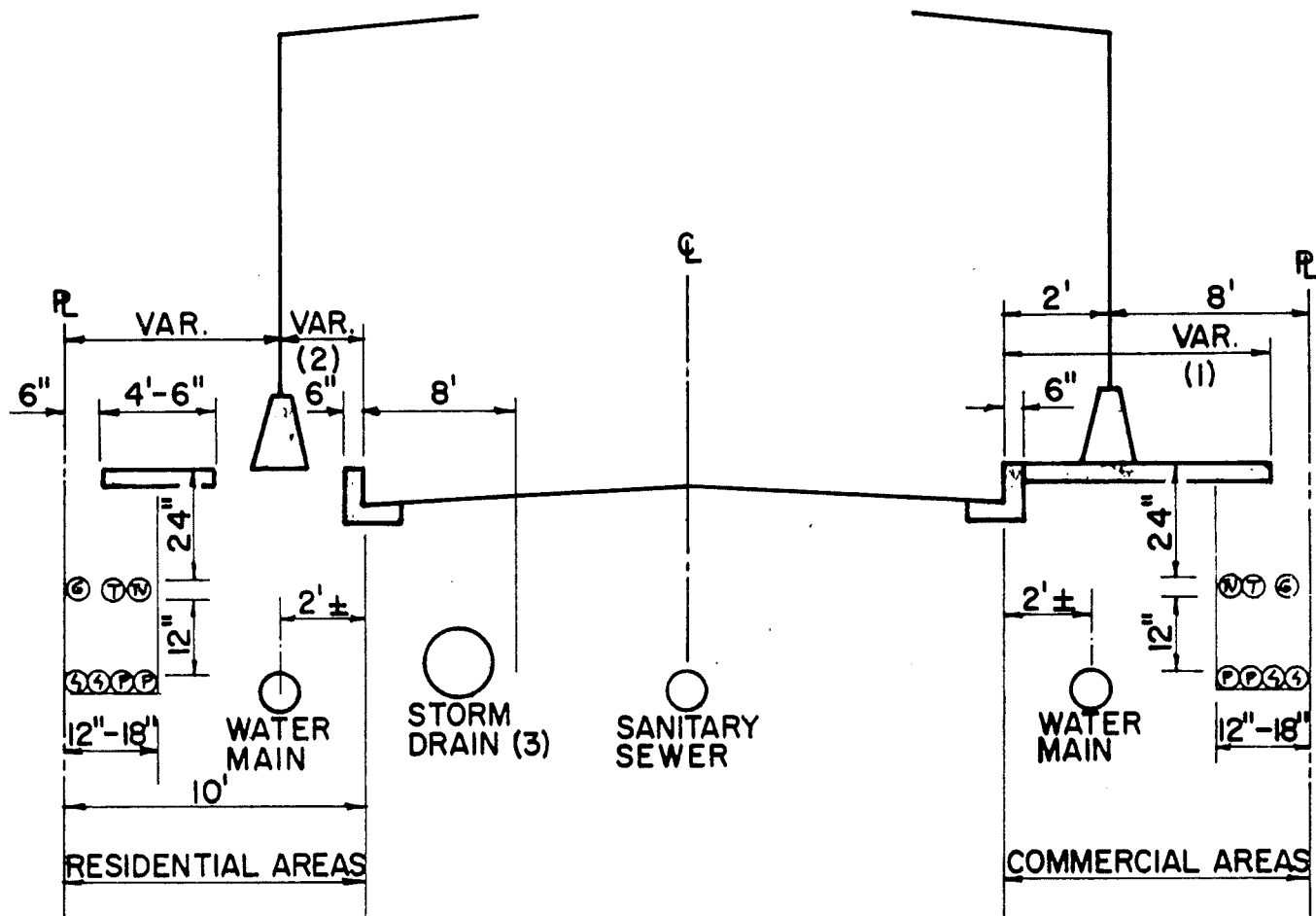
If there is a requirement for parallel sanitary sewers, such as on divided arterials, they should be placed ten feet from each face of curb fronting the property. This location avoids the water mains and provides minimum lateral distances to the property. Also, traffic is impeded on only one lane when repairing the sanitary sewer. If parallel water mains are required, they should each be placed 5 feet from face of curb.

Recommendation: Recommendations for utility locations are:

(1) to place the gas, primary and secondary electrical conduits, telephone lines and television cables in a joint trench which splits the back edge of the sidewalk; (2) to locate electroliers at the back of walk for monolithic sidewalks less than six feet in width and in all other cases two feet from face of curb; (3) to situate the storm drain five feet from the roadway centerline and the sanitary sewer five feet on the opposite side of the centerline; (4) to establish the water main at five feet from face of curb on the far side from the sanitary sewer.

For a major roadway, except expressways, cross section modifications are: (1) to situate the parallel water mains each at five feet from face of curb; (2) to establish two parallel sanitary sewers each at ten feet from face of curb.

As the development occurs, installation of various utilities may not follow the recommended locations due to restrictions of available vacant underground area. The above recommendations should serve as guidelines and not be totally restrictive.



VAR. = VARIABLE

(1) SIDEWALK WIDTH IS VARIABLE FROM 6' TO 10'

(2) ELECTROLIER PLACED AT 2' FROM FACE OF CURB; IF SIDEWALK MONOLITHIC, ELECTROLIER IS PLACED AT BACK OF WALK APPROX. 6' TO 8' FROM FACE OF CURB.

(3) STORM SEWER NORMALLY PLACED WITHIN 8' FROM FACE OF CURB.

PIPES, CONDUITS OR CABLES

Ⓒ GAS

⒫ ELECTRIC PRIMARY

Ⓖ ELECTRIC SECONDARY

Ⓓ TELEPHONE

Ⓜ TELEVISION

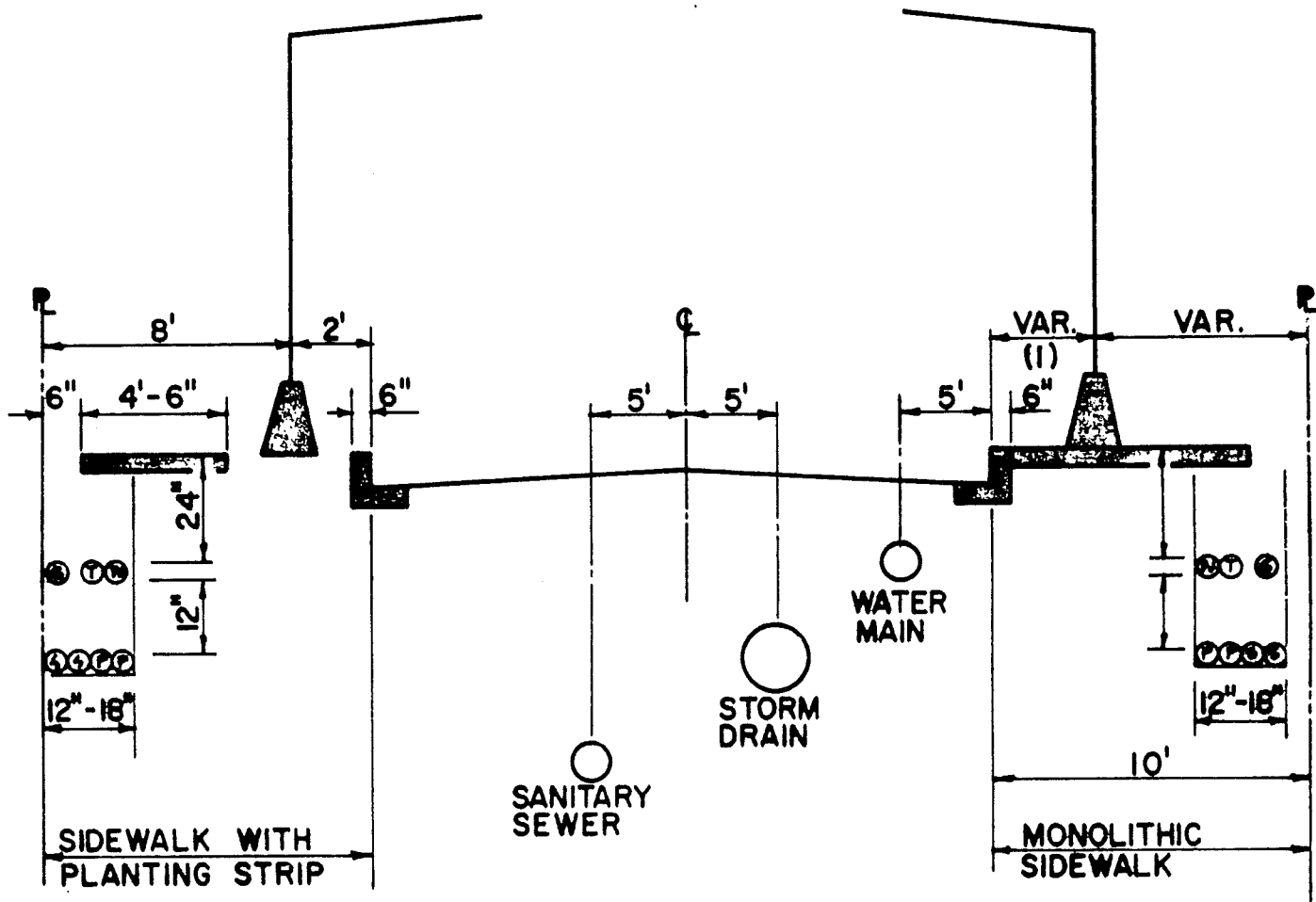
COUNTY OF SANTA CLARA TYPICAL UTILITIES LAYOUT (EXISTING)



RUTH AND GOING

CONSULTING CIVIL ENGINEERS

919 THE ALAMEDA • SAN JOSE, CALIFORNIA



VAR. = VARIABLE

(1) IF MONOLITHIC SIDEWALK 4.5' TO 6' PLACE ELECTROLIER AT BACK OF WALK; IF GREATER THAN 6' PLACE ELECTROLIER 2' FROM FACE OF CURB.

PIPES, CONDUITS OR CABLES

- ⊙ GAS
- ⊙ ELECTRIC PRIMARIES
- ⊙ ELECTRIC SECONDARIES
- ⊙ TELEPHONE
- ⊙ TELEVISION

MINOR OR COLLECTOR ROADWAYS
(TWO TRAFFIC LANES)

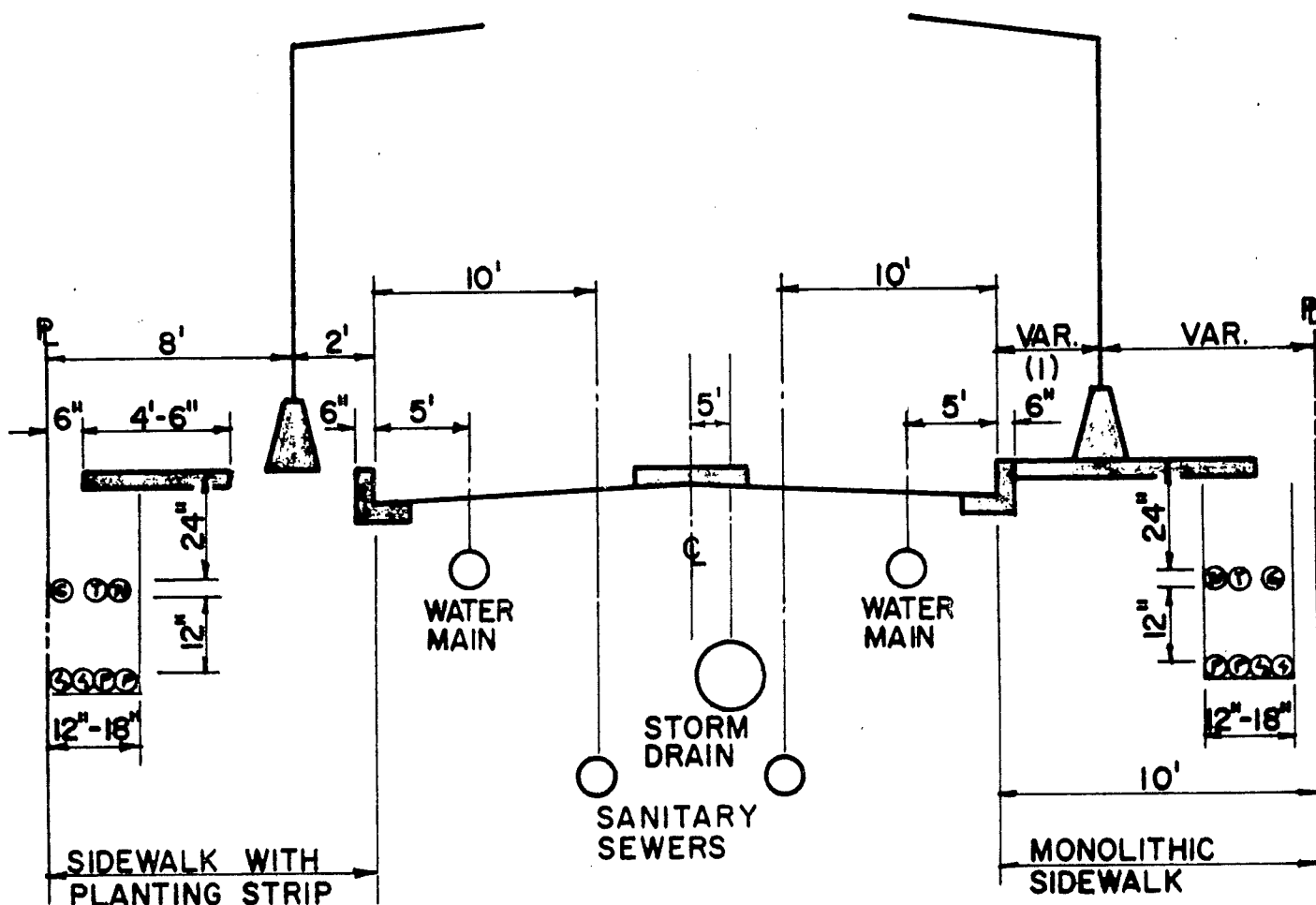
RECOMMENDED TYPICAL UTILITIES LOCATIONS FOR THE COUNTY OF SANTA CLARA



RUTH AND GOING

CONSULTING CIVIL ENGINEERS

919 THE ALAMEDA • SAN JOSE, CALIFORNIA



VAR. = VARIABLE

(1) IF MONOLITHIC SIDEWALK 4.5' TO 6' PLACE ELECTROLIER AT BACK OF WALK; IF GREATER THAN 6' PLACE ELECTROLIER 2' FROM FACE OF CURB.

PIPES, CONDUITS OR CABLES

- Ⓒ GAS
- Ⓔ ELECTRIC PRIMARIES
- Ⓕ ELECTRIC SECONDARIES
- Ⓗ TELEPHONE
- Ⓓ TELEVISION

MAJOR ROADWAYS
(FOUR OR MORE TRAFFIC LANES)

RECOMMENDED TYPICAL UTILITIES LOCATIONS FOR THE COUNTY OF SANTA CLARA



RUTH AND GOING

CONSULTING CIVIL ENGINEERS

919 THE ALAMEDA • SAN JOSE, CALIFORNIA

SUMMARY OF UTILITY LOCATIONS
AND RECOMMENDATIONS FOR
THE COUNTY OF SANTA CLARA

<u>CITY OR TOWN</u>	<u>STORM DRAIN</u>	<u>SANITARY SEWER</u>	<u>WATER MAIN</u>	<u>ELECTROLIER</u>	<u>G, P, S, T, TV</u>
Campbell	5' from CL	5' from CL	5' from F.C. (under pvmt.)	2' from F.C. (toward PL)	In joint trench - split B.W.
Cupertino	5' from CL	5' from CL	3' from F.C. (toward PL) On W or S side or on opp. side of PG&E trench	1.5' from F.C. (toward PL)	In joint trench - split B.W.
Gilroy	Var. dist. from F.C. (under pvmt.)	5' from CL	5' from CL	1.5' from F.C. (toward PL)	G, P, S, TV trenc split B.W.; T in separate trench.
Los Altos	Var. dist. from CL (normally under pvmt.)	On CL	Var. dist. from CL (normally under pvmt.)	Var. dist. from edge of pvmt. (Toward PL)	In joint trench in border area.
Los Altos Hills	Var. dist. from CL	Var. dist. from CL	Var. dist. from CL	None	In joint trench in border area.
Los Gatos	Var. dist. from CL (normally under pvmt.)	Var. dist. from CL (normally under pvmt.)	Var. dist. from F.C. (normally under pvmt.)	±2' from F.C. (toward PL)	In joint trench - split B.W.
Milpitas	7.5' from F.C. (under pvmt.)	On CL	5' from F.C. (under pvmt.)	2' from F.C. for residential area; 7.5' from F.C. for commercial area. (toward PL)	In joint trench - split B.W.
Monte Sereno	Var. dist. (0 to 8') from edge of pvmt. (under pvmt.)	On CL	±2' from rolled curb (toward PL)	Var. dist. from rolled curb (toward PL)	In joint trench - split B.W.
Morgan Hill	Var. dist. from CL (under pvmt.)	Var. dist from CL (under pvmt.)	Var. dist. from F.C. (under pvmt.)	2' from F.C. (toward PL)	In joint trench - split F.W.; if sidewalk mono- lithic, joint trenc in approx. same location as above

SUMMARY OF UTILITY LOCATIONS
AND RECOMMENDATIONS FOR
THE COUNTY OF SANTA CLARA

CITY OR TOWN	STORM DRAIN 12' from CL on S or E side of CL	SANITARY SEWER On CL	WATER MAIN 6' from CL on N or W side of CL	ELECTROLIER 5.5' from F.C. for residential areas; 1.5' from F.C. for commercial areas. (toward PL)	G, P, S, T, TV G is 1' from PL toward roadway; trench for elec. conduits splits B.W.
Mt. View					
Palo Alto	Var. Dist. from CL (under pvt)	On CL	11' from F.C. (under pvt)	Var. dist. from F.C. (toward PL)	G is 6' from F.C. (under pvt); P, S, T, TV in trench - split B.W.
San Jose	5' from CL	5' from CL	5' from F.C. (under pvt)	2.25' from F.C. (toward PL)	In joint trench split F.W.; if sidewalk mono- lithic, trench split B.W.
Santa Clara	5' from CL	5' from CL	7' from F.C. (under pvt)	At B.W. for residential areas; 1' from F.C. for industrial areas	G, T, TV in trench ±2' from PL toward roadway; P, S located under SW for resid. areas and 2'± from F.C. for indus. areas
Saratoga	Var. dist. (0 to 8') from edge of pvmt. (under pvmt.)	On CL	± 2' from F.C. (toward PL)	Var. dist. from F.C. (toward PL)	In joint trench - split B.W.
Sunnyvale	5' from CL	5' from CL	6' from F.C. on N or E side (under pvmt.)	8' from F.C. for residential areas; 3' from F.C. for commercial areas (toward PL)	In joint trench - splits B.W.
Existing Utility Loca- tion Standards for Santa Clara County	Var. dist. (0-8') from EP (under pvmt.)	On CL	± 2' from F.C. (toward PL)	2' to 8' from F.C. for residential areas; 2' from F.C. for commercial areas (toward PL)	In joint trench - split B.W.
Recommended util. location standards for Santa Clara Co.	5' from CL	5' from CL	5' from F.C. on far side from san. sewer	At B.W. for mono- lithic SW greater than 6'; all other cases 2' from F.C.	In joint trench - split B.W.

F.C. = Face of curb
B.W. = Back of walk
F.W. = Front of walk

G = Gas
P = Electric Primaries
S = Electric Secondaries

T = Telephone
TV = Television

APPENDIX I

ALPHABETICAL LISTING
OF
MAJOR ROADWAYS

ALPHABETICAL LISTING OF PLANNED MAJOR ROADWAYS

STREET & SECTION	ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
A.				
1. Abbott Avenue - Harriet Avenue Westmont Avenue to Pollard Road	Campbell	4	50	90
2. Aborn Road				
Capitol Expressway to Tully Road	San Jose	6	40	130
Tully Road to Yerba Buena Road	San Jose	4	0	106
3. Agnew Road				
Coffin Road to Thomas Road	Santa Clara	4	0-60	84
Existing Agnew Rd. to Lafayette St.	Santa Clara	4	60	84
4. Almaden Expressway				
Alma Street to Greystone Lane	County	4	106	N.A.
Greystone Lane to McKean Road	County	4	60	N.A.
Along McKean Road to Bailey Avenue	County	4	50	N.A.
Along Bailey Avenue to U.S. 101	County	6	0-60	N.A.
5. Almaden Road				
Almaden Expressway to Bertram Road	San Jose	4	60	106
6. Alviso - Milpitas Road				
Lafayette Street to S.R. 17	San Jose	6	60	130
7. Arastradero Road				
Foothill Expressway to Page Mill Rd.	Palo Alto	4	40	UNK
8. Auzerais Street				
Race Street to Lincoln Avenue	San Jose	4	60	80
B.				
1. Bailey Avenue				
McKean Road to Scheller Avenue	San Jose	6	0-60	130
2. Bailey Avenue				
S.R. 82 to Stierlin Road	Mt. View	4	60	90
3. Bernal Road				
Camden Avenue (ext.) to Santa Teresa Boulevard	San Jose	4	0-50	106
Santa Teresa Blvd. to Piercy Road	San Jose	6	0-50	106

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>B.</u>					
4. Berryessa Road					
U.S. 101 to Capitol Avenue	San Jose	6	50	130	
Capitol Avenue to Piedmont Road	San Jose	4	0	106	
5. Blossom Hill Road					
S. Bascom Avenue to Harwood Road	Los Gatos	4	40-60	90	
Harwood Road to Camden Avenue	San Jose	4	60	80	
Camden Avenue to Delwood Way	San Jose	4	45-90	90	
Delwood Way to Russo Drive	San Jose	6	80	130	
Russo Drive to Almaden Expressway	San Jose	6	120	120	
Almaden Expressway to U.S. 101	San Jose	6	40-130	130	
U.S. 101 to Hellyer Ext.	San Jose	6	0	130	
Hellyer Avenue to Tennant Avenue	San Jose	4	0	106	
6. Bollinger Road					
Stelling Road to Lawrence Expressway	San Jose	4	0-90	90	
7. Branham					
Ross Avenue to Almaden Expressway	San Jose	4	40-90	90	
Almaden Expressway to Monterey Hwy	San Jose	6	0-130	130	
Monterey Hwy to U.S. 101	San Jose	4	0-90	90	
U.S. 101 to Hellyer Avenue Ext.	San Jose	4	0	106	
8. Brokaw Road					
North 1st Street to S.R. 17	San Jose	6	66-80	130	
9. Buena Vista Avenue					
Murphy Lane to New Avenue	Gilroy	4	0-66	86	
10. Burnett Avenue					
Monterey Hwy to Coyote River	San Jose	4	40	106	
<u>C.</u>					
1. Cahalan Avenue					
Chynoweth Avenue (ext.) to Blossom Hill Road	San Jose	4	0	106	
Blossom Hill Road to Santa Teresa Boulevard	San Jose	4	50-80	90	
2. Camden Avenue					
Winchester Boulevard to S.R. 17	Campbell	4	90	90	
S.R. 17 to Kooser Road	San Jose	6	70-120	120	
Kooser Road to Almaden Expressway	San Jose	6	0-130	130	
Almaden Expressway to Mt. Carmel Dr.	San Jose	4	40	90	
Mt. Carmel Drive to Denhart Road	San Jose	4	0	94	
Denhart Road to McKean Road	San Jose	4	0-50	106	

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
C. 3.	Campbell Avenue				
	Fallbrook Avenue to San Tomas Aquino Road	San Jose	4	40-90	90
	San Tomas Aquino Road to Hunt Way	San Jose	4	40	90
	San Tomas Aquino Road to Hunt Way	Campbell	4	40	80
	Hunt Way to Harrison Avenue	Campbell	4	40	80
	Harrison Avenue to Los Gatos Creek	Campbell	4	40	100
	Los Gatos Creek to Meridian Avenue	Campbell	4	40	80
4.	Capitol Avenue				
	Trimble Road to McKee Road	San Jose	6	50-90	130
	McKee Road to Madden Avenue	San Jose	6	40	120
	Madden Avenue to Alum Rock Avenue	San Jose	6	40	114
	Alum Rock Avenue to Story Road	San Jose	6	40-100	120
5.	Capitol Expressway				
	I-680 to Almaden Expressway	County	6	0-134	N.A.
6.	Center Avenue				
	Middle Avenue to Masten Avenue	Gilroy Morgan Hill	4	40-66	86
7.	Central Expressway				
	De La Cruz Boulevard to San Mateo Co.	County	4	Variable	N.A.
8.	Cherry Avenue				
	Branham Lane to Almaden Expressway	San Jose	4	0-90	90
9.	Chestnut Street				
	Leavesley Road to Luchessa Road	Gilroy	4	0-60	86
10.	Church Street				
	Day Road to Morey Avenue	Gilroy	4	0	86
	Morey Avenue to Leavesley Road	Gilroy	4	0-66	102
11.	Chynoweth Avenue				
	Cahalan Avenue to Monterey Hwy	San Jose	4	0-40	106
	Cahalan Avenue to Almaden Expressway	San Jose	4	0-40	90
12.	Clayton Road				
	Story Road to Mt. Pleasant Road	San Jose	4	40-66	90
13.	Cobb Road				
	Martinvale Road to Bernal Road	San Jose	4	30	106
14.	Cochran Road				
	Monterey Highway to Peet Road	Morgan Hill	4	40	110
	Peet Road to East Main Avenue Via Loop	Morgan Hill			
15.	Coffin Road				
	SPRR to Existing Coffin Road	Santa Clara	6	50	120

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>C.</u>					
16.	Coleman Road				
	Camden Avenue to Almaden Expressway	San Jose	4	0-94	94
	Almaden Expressway to Santa Teresa Boulevard	San Jose	4	0	106
	Santa Teresa Boulevard to Cahalan Avenue	San Jose	4	0	90
17.	Cottle Road				
	Monterey Highway to Beswick Drive	San Jose	6	40-70	130
	Beswick Drive to S.R. 85	San Jose	6	40-70	136
	S.R. 85 to Santa Teresa Boulevard	San Jose	6	40-70	130
	Santa Teresa Boulevard to Manila Dr.	San Jose	4	70	90
18.	Cropley Avenue				
	Capitol Avenue to Piedmont Road	San Jose	4	0-45	90
19.	Curtner Avenue				
	S.R. 17 to Booksin Avenue	San Jose	4	50-80	90
	Booksins Avenue to Cherry Avenue	San Jose	4	50-80	86
	Cherry Avenue to Almaden Expressway	San Jose	4	60	80
	Almaden Expressway to Monterey Hwy	San Jose	6	110	120
	Monterey Highway to S. 7th Street	San Jose	6	120	120
<u>D.</u>					
1.	Dixon Landing Road				
	Exist'g Dixon Landing Road to S.R. 237	Milpitas	4	0	90
2.	Dunne Avenue				
	Dewitt Avenue to ± 1 mile East of Hill Road	Morgan Hill	4	66	90
<u>E.</u>					
1.	East Dana Street				
	Sylvan Way to Moorpark Way	Mt. View	4	60	90
2.	Edenvale Avenue				
	Branham Lane to Blossom Hill Road	San Jose	4	0-40	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
E. 3.	Edmundson Avenue Llagas Road to Monterey Highway	Morgan Hill	4	0-60	UNK
4.	El Monte Avenue Giffin Road to Junipero Serra FWY (1-280)	Los Altos	4	70-100	70 - 100
5.	Evans Road Calle Oriente to Calaveras Road	Milpitas	4	50	90
E. 1.	First Street Morey Avenue to Monterey Highway	Gilroy	4	60	86
2.	Fitzgerald Avenue Monterey Highway to Santa Teresa Expressway	Gilroy	4	40	86
3.	Flickinger Avenue Oakland Highway to Mabury Road	San Jose	4	0-40	106
4.	Foothill Boulevard S.P. R.R. to Stevens Creek Boulevard	Cupertino	4	50-60	110
	Stevens Creek Boulevard to Balboa Rd.	Cupertino	4	50-60	90
5.	Foothill Expressway 1-280 to Oregon-Page Mill Expressway	County	4	Variable	N.A.
	Oregon-Page Mill Expressway to San Mateo County Line	County	4	0	N.A.
6.	Fortini Avenue Almaden Road to Camden Avenue (ext.)	San Jose	4	0-40	106
7.	Fremont Avenue Miramonte Avenue to East City Limits	Los Altos	4	60-100	100
	West City Limits to El Camino Real	Sunnyvale	6	50-60	120
8.	Fruitdale Avenue S. Bascom Avenue to Meridian Avenue	San Jose	4	68-80	86
	Meridian Avenue to Race Street	San Jose	4	66	106

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>G.</u>					
1.	Gilman Road Chestnut Street to Marcella Avenue (Ext.)	Gilroy	4	40	86
2.					
	Grant Road				
	Fremont Avenue to Levin Avenue	Los Altos	4	60	90
	Levin Avenue to Cuesta Drive	Mt. View	4	60	90
	Cuesta Drive to S.R. 82	Mt. View	6	60	120
<u>H.</u>					
1.	Hacienda Avenue Abbott Avenue to Winchester Boulevard Campbell		4	75	90
2.					
	Hamilton Avenue				
	Campbell Avenue to San Tomas Exp.	Campbell	6	120	120
	San Tomas Exp. to S. Bascom Avenue	Campbell	6	86-120	120
	S. Bascom Avenue to Meridian Avenue	San Jose	6	66-86	120
	Meridian Avenue to Hicks Avenue	San Jose	4	0-86	90
3.					
	Harry Road				
	Almaden Road to Camden Avenue (ext.)	San Jose	4	40	106
4.					
	Hill Road				
	Burnett Avenue to Middle Avenue	Morgan Hill	4	66	UNK
5.					
	Hillsdale Avenue				
	Pearl Avenue to S.R. 87	San Jose	4	50	90
	S.R. 87 to Capitol Expressway	San Jose	4	0-50	106
	Capitol Expressway to Branham Lane	San Jose	6	0	130
6.					
	Homestead Road				
	Grant Road to S.R. 85	Los Altos	4	73	86
	S.R. 85 to Lawrence Expressway	Sunnyvale	4	40-60	110
7.					
	Hostetter Road				
	Capitol Avenue to Morrill Road	San Jose	4	0	106
	Morrill Road to Piedmont Road	San Jose	4	0-40	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
J. 1.	Jacklin Road Dempsey Road to N. Park Victoria Dr.	Milpitas	4	0	90
2.	Jackson Avenue Mabury Road to McKee Road	San Jose	4	0-40	90
	McKee Road to Alum Rock Avenue	San Jose	4	60	90
3.	Johnson Avenue Prospect Road to Bollinger Road	San Jose	4	50-80	80
K. 1.	Kalana Avenue San Bruno Canyon To Monterey HWY	San Jose	4	50	106
2.	King Road Berryessa Road to Mabury Road	San Jose	4	50	106
	Tully Road to Capitol Expressway	San Jose	4	50-70	106
L. 1.	Lafayette Street Central Expressway to S.R. 237	Santa Clara	4	0-60	90
2.	Landess Avenue Morrill Avenue to Piedmont Road	Milpitas San Jose	4	55	90
3.	Lark Avenue Winchester Boulevard to S. Bascom Ave.	Los Gatos	6	50-85	120
4.	Lawrence Expressway U.S. 101 to Saratoga Avenue	County	4	Variable	N.A.
	Saratoga Avenue to Pollard Road	County	6	90	N.A.
5.	Leavesley-Ferguson Road Church Street to S.R. 152 (Pacheco Pass Highway)	Gilroy	4	60	110
6.	Leigh Avenue S.W. Expressway to Camden Avenue	San Jose	4	0-90	90
	Camden Avenue to S.R. 85	San Jose	4	86	86
	S.R. 85 to Blossom Hill Road	San Jose	4	90	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>L.</u>					
7.	Lincoln Avenue				
	Park Avenue to W. San Carlos Street	San Jose	4	50	90
	W. San Carlos Street to Pedro Street	San Jose	4	86	86
	Pedro Street to Almaden Expressway	San Jose	4	80	80
8.	Little Orchard Street				
	San Jose Avenue to S.P. R.R.	San Jose	4	0	84
	S.P. R.R. to Curtner Avenue	San Jose	4	80	80
9.	Live Oak Avenue				
	Santa Teresa Expressway to Scheller Avenue	San Jose	4	0-50	106
10.	Llagas Road				
	Old Monterey Road to Watsonville Road	Morgan Hill	4	40-60	UNK
11.	Los Gatos-Almaden Road				
	S. Bascom Avenue to Leigh Avenue	Los Gatos	4	40-50	90
	Leigh Avenue to Harwood Road	San Jose	4	40	90
12.	Los Robles Avenue				
	S.R. 82 to Arastradero Road	Palo Alto	4	40	UNK
13.	Lundy Avenue				
	Trimble Road to Berryessa Road	San Jose	4	0-70	106
<u>M.</u>					
1.	Mabury Road				
	U.S. 101 to Capitol Avenue	San Jose	4	50	106
	Capitol Avenue to White Road	San Jose	4	0-90	90
2.	Magdalena Avenue				
	Junipero Serra FWY (I-280) to Foothill Expressway	Los Altos	4	80-100	100
3.	Main Avenue				
	Monterey Highway to Coyote Road	Morgan Hill	4	66	90
4.	Marcella Avenue				
	Masten Avenue to Gilman Road	Gilroy	4	40	86
5.	Martinvale Road				
	Santa Teresa-Monterey HWY connection to Little Avenue	San Jose	4	0	106
	Little Avenue to Hicks Lane	San Jose	4	0	98
	Hicks Lane to Santa Teresa Boulevard	San Jose	4	50	106

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
M. 6.	Mary Avenue Homestead Road to Central Expressway Central Expressway to Caribbean Drive	Sunnyvale Sunnyvale	4 6	50 0	86 120
7.	Masten Avenue Monterey Highway to New Avenue	Gilroy	4	60	102
8.	Mathilda Avenue Saratoga-Sunnyvale Road to S.R. 82	Sunnyvale	6	120	120
9.	Maude Avenue S.R. 237 to Mathilda Avenue	Sunnyvale	4	40	86
10.	McAbee Road Almaden Expressway to Camden Avenue	San Jose	4	40-60	90
11.	McKean Road Harry Road to Bailey Avenue	San Jose	4	50	Varies (100 Min.)
12.	McKee Road U.S. 101 to King Road King Road to White Road White Road to Alum Rock Avenue	San Jose San Jose San Jose	4 6 4	86 86 86	86 130 86
13.	McLaughlin Avenue San Antonio Street to William Street William Street to Applan Lane Applan Lane to Capitol Expressway Capitol Expressway to Sylvandale Ext.	San Jose San Jose San Jose San Jose	4 4 4 4	40 68 68 0	80 86 90 106
14.	Meridian Avenue Park Avenue to W. San Carlos Street W. San Carlos Street to Fruitdale Avenue Fruitdale Avenue to Campbell Avenue Campbell Avenue to Maykirk Road Maykirk Road to Curtner Avenue Curtner Avenue to Coleman Road Coleman Road to Redmond Avenue Redmond Avenue to Camden Avenue	San Jose San Jose San Jose San Jose San Jose San Jose San Jose	4 4 4 4 4 4 4	60 90 86 100 95 0-90 0 0	86 90 86 100 95 90 106 90
15.	Middle Avenue Monterey Highway to Foothill Road	Morgan Hill	4	66	UNK
16.	Minnesota Avenue Meridian Avenue to Hicks Avenue	San Jose	4	50	80
17.	Miramonte Avenue S.R. 82 to Foothill Expressway Mt. View City Limits to Foothill Expressway	Mt. View Los Altos	4 4	60 60	90 70

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>M.</u>					
18.	Moffett Boulevard U.S. 101 to Moffett Field Main Gate	Mt. View	6	50	120
19.	Montague Expressway U.S. 101 to North 1st Street	County	6	134	N.A.
	North 1st Street to Seeley Road	County	6	0	N.A.
	Seeley Road to Capitol Avenue	County	6	134	N.A.
	Capitol Avenue to I-680	County	6	55	N.A.
20.	Moorpark Avenue Lawrence Expressway to Saratoga Ave.	San Jose	4	68	90
	Saratoga Avenue to ± 750' West of Saratoga Avenue	San Jose	4	68	86
	± 750' West of Saratoga Avenue to San Tomas Expressway	San Jose	4	79	79
	San Tomas Expressway to Winchester Boulevard	San Jose	4	80	80
	Winchester Boulevard to Bascom Ave.	San Jose	4	68	86
	Bascom Avenue to Lincoln Avenue	San Jose	4	68	80
21.	Morrill Road Landess Avenue to Berryessa Road	San Jose	4	40	90
22.	Mount Pleasant Road Clayton Road to Kohler Road	San Jose	4	40	90
23.	Murphy Avenue Oakland Highway to Capitol Avenue	San Jose	6	0-70	130
<u>N.</u>					
1.	North First Street Alviso-Milpitas Road to McGier Lane	San Jose	6	84	120
2.	N. Park Victoria Drive Jacklin Road to Burnett Avenue	Milpitas	4	0	90
<u>O.</u>					
1.	Oakland Highway Trimble Road to U.S. 101	San Jose	6	68	130

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
0. 2.	Ocala Avenue King Road to White Road	San Jose	4	0-60	90
3.	Ogan Road Almaden Expressway to Chynoweth Ave.	San Jose	4	0-40	106
4.	Oregon-Page Mill Expressway U.S. 101 to Alma Street	County	4	Variable	N.A.
	Alma Street to Foothill Expressway	County	4	40-110	N.A.
	Foothill Expressway to I-280	County	4	Variable	N.A.
P. 1.	Page Mill Road I-280 to San Mateo County	Palo Alto	4	40	UNK
2.	Park Avenue Meridian Avenue to S.P. R.R.	San Jose	6	60	130
3.	Payne Avenue Saratoga Avenue to San Tomas Aquino Road	San Jose	4	60	88
	San Tomas Aquino Road to Boynton Ave.	San Jose	4	60	86
	Boynton Avenue to San Tomas Exp.	San Jose	4	60	88
	San Tomas Exp. to Winchester Blvd.	San Jose	4	60	90
4.	Pearl Avenue Hillsdale Avenue to Ogan Avenue	San Jose	4	60-70	90
5.	Pedro Street Race Street to Lincoln Avenue	San Jose	4	60	84
6.	Penitencia Creek Road Capitol Avenue to Boulder Drive	San Jose	4	40	80
7.	Phelan Avenue Monterey Highway to McLaughlin Ave.	San Jose	4	0-60	90
8.	Piedmont Road Calaveras Road to Landess Avenue	Milpitas	4	40	90
	Landess Avenue to Tunis Avenue	San Jose	4	110	110
	Tunis Avenue to Weedon Court	San Jose	4	90	90
	Weedon Court to Hostetter Road	San Jose	4	0	106
	Hostetter Road to Penitencia Creek Road	San Jose	4	50	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>P.</u>					
9.	Pollard Road				
	Arroyo San Tomas Aquino to Burrows Road	Los Gatos	4	40	100
	Burrows Road to Weldwood Avenue	Los Gatos	4	40	84
	Arroyo San Tomas Aquino To S.P. R.R.	Campbell	4	40	90
10.	Porter Avenue				
	W. San Carlos Street to Moorpark Ave.	San Jose	4	45	80
	Moorpark Avenue to S. W. Expressway	San Jose	4	0-90	90
11.	Prospect Road (Half Street Widths)				
	Stelling Road to Saratoga-Sunnyvale Road	San Jose	2	20	45
	Stelling Road to Saratoga-Sunnyvale Road	Saratoga	2	20	50
	Saratoga-Sunnyvale Road to English Drive	San Jose	3	30	60
	Saratoga-Sunnyvale Road to English Drive	Saratoga	3	30	60
	English Drive to Lawrence Expressway	San Jose	4	85	85
	English Drive to Lawrence Expressway	Saratoga	2	35	35
	Lawrence Expressway to Saratoga Ave.	San Jose	3	60	60
	Lawrence Expressway to Saratoga Ave.	Saratoga	3	50	60
<u>Q.</u>					
1.	Quimby Road				
	Tully Road to Rigoletto Drive	San Jose	4	0	102
	Rigoletto Drive to Ruby Avenue	San Jose	4	40	90
	Ruby Avenue to Tully Road	San Jose	4	40	106
	Tully Road to Hills	San Jose	4	0	106
2.	Quito Road				
	Saratoga Avenue to S.R. 85	Saratoga	6	90	120
	S.R. 85 to Pollard Road	Saratoga	6	90	120
<u>R.</u>					
1.	Race Street				
	The Alameda to Fruitdale Avenue	San Jose	4	60	80

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
R. 2.	Redmond Avenue Coleman Road to Cloverhill Drive	San Jose	4	40-60	90
3.	Rengstorff Avenue U. S. 101 to Stierlin Road (ext.)	Mt. View	4	0	90
4.	Richmond Avenue San Bruno Canyon to Monterey HWY	San Jose	4	0-50	106
5.	Ruby Avenue Holderman Drive to Tully Road	San Jose	4	40	90
	Tully Road to San Felipe-Tully Connection	San Jose	4	0-40	106
6.	Russo Drive Cherry Avenue to Blossom Hill Road	San Jose	4	0-60	90
S. 1.	San Antonio Street North 24th Street to King Road	San Jose	4	60	90
2.	San Bruno Canyon Road Bailey Avenue to Hale Avenue	San Jose	4	0	106
3.	San Felipe Road Aborn Road to Yerba Buena Avenue	San Jose	4	0	90
	Yerba Buena Avenue to the Villages Parkway	San Jose	4	40-60	106
	Villages Parkway to Silver Creek Rd.	San Jose	6	40-60	130
4.	San Felipe-Tully Connection San Felipe Road to Tully Road	San Jose	4	0	106
5.	San Martin Avenue Santa Teresa Expressway to New Avenue	Morgan Hill	4	50-80	UNK
6.	San Tomas Aquino Road Saratoga Avenue to Payne Avenue	San Jose	4	0	90
	Payne Avenue to Hamilton Park Drive	San Jose	4	88	88
	Hamilton Park Drive to McCoy Avenue	San Jose	4	60	90
7.	San Tomas Expressway U.S. 101 to S.R. 17	County	4	Variable	N.A.

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
S.					
8.	Santa Teresa Expressway Cottle Road to Monterey Highway Bloomfield Road Interchange	County	4	0-50	N.A.
9.	Santa Teresa-Monterey Connection Santa Teresa Boulevard to Monterey Highway	San Jose	4	0	90
10.	Saratoga Avenue				
	Stevens Creek Boulevard to Doyle Road	San Jose	6	100	120
	Doyle Road to Parkwest Drive	San Jose	6	110	145
	Parkwest Drive to Atherton Avenue	San Jose	6	170	170
	Atherton Avenue to Graves Avenue	San Jose	6	145	145
	Graves Avenue to Quito Road	San Jose	6	100	120
11.	Saratoga-Sunnyvale Road				
	S R. 82 to Crawford Avenue	Sunnyvale	4	80	86
	Crawford Avenue to Homestead Road	Sunnyvale	6	60-110	120
	Homestead Road to Bollinger Road	Cupertino	6	70-100	120
	Bollinger Road to Prospect Road	San Jose	6	70-100	120
12.	Schallenger Road				
	S.R. 17 to Oakland Highway	San Jose	4	40	94
13.	Scheller Avenue				
	San Bruno Canyon Road to Monterey Highway	San Jose	4	0-50	106
	Monterey Highway to Bailey Avenue	San Jose	6	0	130
14.	Seeley Road (Montague Exp)				
	Montague Road to Trimble Road	County	6	0-40	134
15.	Senter Road				
	Story Road to Phelan Avenue	San Jose	6	60	116
	Phelan Avenue to Capitol Expressway	San Jose	6	60	126
	Capitol Expressway to Monterey HWY	San Jose	4	60	90
16.	Shaw Lane				
	Oakland Highway to ± 600' East of Oakland Highway	San Jose	4	90	90
	± 600' East of Oakland Highway to Coyote Creek	San Jose	4	0	80
	Coyote Creek to Morrill Road	San Jose	4	0	106
17.	Sierra Road				
	Morrill Road to Piedmont Road	San Jose	4	80	90
18.	Silver Creek Road				
	Capitol Expressway to Silver Creek	San Jose	6	50	130
	Silver Creek to San Felipe Road	San Jose	4	50	106

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
S. 19.	Snell Road				
	Branham Lane to Avenida Del Roble	San Jose	6	60	130
	Avenida Del Roble to S.R. 85	San Jose	6	70	130
	S.R. 85 to Mindy Way	San Jose	4	80	90
20.	Snell-Hillsdale Connection				
	Hillsdale Avenue (ext.) to Monterey Highway	San Jose	4	0-90	90
21.	South Bascom Avenue				
	West San Carlos Street to Hamilton Avenue	San Jose	6	80-100	120
	Hamilton Avenue to Dry Creek Road	Campbell	6	60	120
	Dry Creek Road to N. Union Avenue	San Jose	6	100	123
	N. Union Avenue to Curtner Avenue	San Jose	6	100	127
	Curtner Avenue to S.R. 85	San Jose	6	50-100	120
	S.R. 85 to Blossom Hill Road	Los Gatos	6	50-70	120
	Blossom Hill Road to Roberts Road	Los Gatos	6	70	120
	Roberts Road to Mitchell Avenue	Los Gatos	4	70	100
	Mitchell Avenue to Caldwell Avenue	Los Gatos	4	70	90
	Caldwell Avenue to Charles Street	Los Gatos	4	70	80
22.	Southwest Expressway				
	Meridian Avenue to S. Bascom Avenue	San Jose	6	118	125
23.	Stelling Road				
	McClellan Road to Waterford Drive	San Jose	4	50-90	90
	Waterford Drive to Prospect Road	San Jose	4	80	80
24.	Stevens Creek Boulevard				
	Foothill Boulevard to Lawrence Exp.	Cupertino	6	90	120
	Lawrence Expressway to S.R. 17	San Jose	6	120	120
25.	Stierlin Road				
	Central Expressway to Crittenden Lane Mt. View		4	60-80	90
26.	Story Road				
	Senter Road to Capitol Expressway	San Jose	6	0-110	120
	Capitol Expressway to White Road	San Jose	6	70	130
	White Road to Bedford Street	San Jose	6	118	118
	Bedford Street to Clayton Road	San Jose	4	80	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>T.</u>					
1.	Tennant Avenue Monterey Highway to Foothill Road	Morgan Hill	4	60	90
2.	Tennant Road Coyote Creek to Piercy Road	San Jose	4	40	106
3.	Tenth Street Santa Teresa Expressway to U.S. 101	Gilroy	4	66	102
4.	Terminal Boulevard (ext.) Terminal Boulevard to Moffett Blvd.	Mt. View	4	0	90
5.	Third Avenue Santa Teresa Expressway to Tilton Avenue	San Jose	4	0-60	106
6.	Trimble Road U. S. 101 to Zanker Road	San Jose	6	140	140
	Zanker Road to S.R. 17	San Jose	6	60	140
	S.R. 17 to Landess Avenue	San Jose	4	40	110±
	Landess Avenue to Capitol Avenue	San Jose	4	0-40	106
7.	Trinidad Drive Camden Avenue to Almaden Expressway	San Jose	4	0-90	90
8.	Tully Road S. 7th Street to S. 10th Street	San Jose	4	110	110
	S. 10th Street to McLaughlin	San Jose	6	100	120
	McLaughlin to Ruby Avenue	San Jose	6	60	130
	Ruby Avenue to Villa Vista Road	San Jose	4	0	106
	Villa Vista Road to ± 1 mile from San Felipe Road	San Jose	4	0	80
	±1 mile from San Felipe Road to San Felipe Road	San Jose	4	0	106
<u>U.</u>					
1.	Umbarger Road Monterey Highway to Senter Road	San Jose	4	45-65	90
	Senter Road to Quimby Road	San Jose	4	0	106
2.	Union Avenue S. Bascom Avenue to Camden Avenue	San Jose	4	50-90	90
	Camden Avenue to Stratford Avenue	San Jose	4	81	81
	Stratford Avenue to S.R. 85	San Jose	4	113	113
	S.R. 85 to Heather Drive	San Jose	4	60	88
	Heather Drive to Blossom Hill Road	San Jose	4	60	90

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>U.</u>					
3.	Uvas Creek Parkway Thomas Road to Santa Teresa Expressway	Gilroy	4	0	100
4.	Uvas Road Bailey Avenue to Croy Road Croy Road to 12,000' Southerly of Croy Road	San Jose County	4 4	60 60	Varies N.A.
<u>V.</u>					
1.	Vine Avenue Cochran Road to Middle Avenue	Morgan Hill	4	66	UNK
2.	Vista Park Drive Capitol Expressway to Viewpark Circle Viewpark Circle to Branham Lane Branham Lane to Chynoweth Avenue	San Jose San Jose San Jose	4 4 4	106 45 0	106 90 106
<u>W.</u>					
1.	Watsonville Road Monterey Highway to Uvas Road Monterey Highway to Tennant Avenue	Morgan Hill Morgan Hill	4 4	50-60 0	110 UNK
2.	West San Carlos Street S.R. 17 to Meridian Avenue Meridian Avenue to Race Street Race Street to Lincoln Avenue	San Jose San Jose San Jose	4 4 4	90 110 94	126 127 94
3.	Westmont Avenue Quito Road to Harriet Avenue	San Jose	4	50	90
4.	White Road Penitencia Creek Road to Mabury Road Mabury Road to Story Road Story Road to Markingdon Drive Markingdon Drive to Aborn Road	San Jose San Jose San Jose San Jose	4 4 6 6	0-40 40-60 100-118 40-60	106 90 118 130

STREET & SECTION		ADJOINING JURISDICTION	NO. OF LANES	EXIST'G R.O.W. (FEET)	CITY'S PROPOSED ROW (FT.)
<u>W.</u>					
5.	Williams Road				
	Moorpark Avenue to Saratoga Avenue	San Jose	4	90	90
	Saratoga Avenue to Cypress Avenue	San Jose	4	60	88
	Cypress Avenue to Opal Drive	San Jose	4	60	86
	Opal Drive to Winchester Boulevard	San Jose	4	80	90
6.	Willow Expressway	Palo Alto	UNK	0	UNK
7.	Willow Street				
	Hamilton Avenue to Leigh Avenue	San Jose	4	0	106
	Leigh Avenue to Meridian Avenue	San Jose	4	0-80	80
8.	Willow Springs Road				
	Hale Avenue to Oak Glen Avenue	San Jose	4	40	106
9.	Winchester Boulevard				
	Stevens Creek Blvd. to Hamilton Ave.	San Jose	6	90	120
	Hamilton Avenue to Camden Avenue	Campbell	4	40-60	90
	Camden Avenue to Division Street	Campbell	4	40-60	80
	Division Street to Vineland Avenue	Los Gatos	4	80	90
	Vineland Avenue to San Benito Avenue	Monte Sereno	4	80	104
	San Benito Avenue to Blossom Hill Road	Los Gatos	4	80	90
10.	Winfield Boulevard				
	Ogan Avenue to Coleman Road	San Jose	4	0	90
	Coleman Road to Almaden Expressway	San Jose	4	0	106
11.	Wolfe Road				
	Reed Avenue to Homestead Road	Sunnyvale	4	66	86
	Homestead Road to Stevens Creek Boulevard	Cupertino	4	80	80
	Stevens Creek Boulevard to Bollinger Road	Cupertino	4	60	108
	Bollinger Road to Prospect Road	San Jose	4	50	90
<u>Y.</u>					
1.	Yerba Buena Avenue				
	White Road to San Felipe Road	San Jose	4	0	106
2.	Yerba Buena Road				
	Sylvandale Ext. to San Felipe Rd.	San Jose	4	0	106
	San Felipe Road to Villa Vista Road	San Jose	4	60	94
	Villa Vista Road to Tully Road	San Jose	4	60	106

APPENDIX II

SUMMARY
OF
FUTURE HIGHWAY CAPITAL IMPROVEMENTS

SUMMARY OF FUTURE HIGHWAY CAPITAL IMPROVEMENTS

STREET & SECTION	JURIS- DICTION	70-71	71-72	72-73	73-74	74-75	75-76	76-77	77-78	78-79	UNK.
Abbott Ave. - Harriet Ave. Westmont Ave. to Pollard Rd.	Campbell						X				
Auzerais St. Race St. to Market St.	San Jose			X	X						
Blossom Hill Rd. S. Bascom Ave. to Harwood Road	Los Gatos										X
Blossom Hill Rd. Snell Rd. to Monterey Hwy	San Jose	X									
Bollinger Rd. S.R. 9 to Lawrence Exp.	San Jose	X									
Camden Ave. W. line Tract 295 to S.R.17	Campbell	X	X								
Chestnut St. Leavesley Rd. to Luchessa Rd.	Gilroy	X									
Cochran Rd. US-101 to Monterey Hwy	Morgan Hill	X									
Coffin Road SPRR to exist. Coffin Rd.	Santa Clara	X		X			X				
Cropley Ave. Capitol Ave. to Morrill Ave.	San Jose			X							
E. Dana St. Mt. View-Alviso Rd. to E. City Limits	Mt. View										X
Fremont Ave. Miramonte Ave. to E. City Limits	Los Altos	X	X	X							

SUMMARY OF FUTURE HIGHWAY CAPITAL IMPROVEMENTS

STREET & SECTION	JURIS- DICTION	70-71	71-72	72-73	73-74	74-75	75-76	76-77	77-78	78-79	Unk
Grant Road Fremont Ave. to N. City Limits	Los Altos			X	X						
Grant Road S.R. 82 to S. City Limits	Mt. View										X
Hacienda Ave. Burrows Rd. to Abbott Ave.	Campbell						X				
Hamilton Ave. San Tomas Exp. to Eden Ave. Winchester Blvd to SR 17	Campbell				X						
Hostetter Rd. At Interchange with I-680	San Jose	X									
Jacklin Rd. Dempsey Rd. to N. Park Victoria Dr.	Milpitas	X									
Jackson Ave. At I-680	San Jose	X									
Lafayette Street Comstock St. to S. of Aldo Montague Rd. to S.R. 237	Santa Clara			X				X			
Landess Ave. At I-680	San Jose	X									
Leigh Ave. Exist. Leigh Ave. to Moorpark Ave.	San Jose	X									
Lincoln Ave. San Carlos St. to The Alameda	San Jose		X	X							

SUMMARY OF FUTURE HIGHWAY CAPITAL IMPROVEMENTS

STREET & SECTION	JURIS- DICTION	70-71	71-72	72-73	73-74	74-75	75-76	76-77	77-78	78-79	Unk.
McKee Road At I-680	San Jose	X									
Meridian Ave. San Carlos St. to Park Ave.	San Jose				X						
N. Park Victoria Dr. Jacklin Rd. to Calle Oriente	Milpitas	X									
Park Ave. Montgomery St. to SR 87	San Jose	X									
Penitencia Creek Rd. Realignment of Exist. Rd.	San Jose	X									
Phelan Ave. Monterey Hwy to McLaughlin Ave.	San Jose	X			X						
Piedmont Rd. Calaveras Rd. to Landess Ave.	Milpitas	X									
Pollard Rd. San Tomas Aquino Cr. to SPRR	Campbell						X				
Prospect Rd. Stelling Rd. to Saratoga Ave.	Saratoga			X	X	X					
Quito Road Saratoga Ave. to Pollard Rd.	Saratoga								X		
Race St. At Inter. w/Moorpark Ave.	San Jose	X									
San Antonio St. Jackson Ave. to San Carlos St.	San Jose	X	X								

SUMMARY OF FUTURE HIGHWAY CAPITAL IMPROVEMENTS

STREET & SECTION	JURIS- DICTION	70-71	71-72	72-73	73-74	74-75	75-76	76-77	77-78	78-79	Unl
San Tomas Aquino Rd. Campbell Ave. to Bucknall Rd.	Campbell						X				
S. Bascom Ave. San Carlos St. to Union Ave.	San Jose	X									
S. Bascom Ave. Los Gatos Cr. to 400' S of Apricot Ave.	Campbell	X									
S. Bascom Ave. S.R. 9 to Shannon Rd.	Los Gatos										X
Stevens Cr. Blvd. Foothill Blvd. to SR 85	Cupertino					X					
Stierlin Rd. Bailey Ave. to Shoreline Pk	Mt. View										X
Story Rd. U.S. 101 to Senter Rd.	San Jose	X									
Terminal Blvd. (Ext) Broderick Wy to Stierlin Rd	Mt. View										X
Winchester Blvd. Sunnyoaks to Division St.	Campbell									X	
Winchester Blvd. Campbell City Limits to Blossom Hill Rd.	Los Gatos										X
Wolfe Rd. At Reed Ave. Inter.	Sunnyvale			X	X						

APPENDIX III

EXISTING
OFFICIAL PLAN LINES

EXISTING OFFICIAL PLAN LINES

	<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>A</u>				
1.	AGNEW Road Bet. Lafayette St. & San Tomas Creek	Santa Clara	7	48-49
2.	ALAMEDA, The Bet. Pershing Avenue & Alameda Way	San Jose	1	24-25
3.	ALAMEDA, The Bet. Harrison & Bellomy Streets	Santa Clara	3	19
4.	ALLEY (30 feet wide) - In vicinity of Phyllis Avenue & Camille Court	Mt. View	7	50
5.	ALMA Avenue Bet. S. First Street & City Limits Line; Portion of Almaden Avenue	San Jose	1	60-61
6.	ALMA AVENUE Bet. S. First Street & City Limits Line; Portion of Almaden Avenue (as amended)	San Jose	1	82-83
7.	ALMADEN Avenue Bet. Santa Clara St. & City Limits	San Jose	1	26-27
8.	ALUM ROCK Avenue Bet. Eastgate Avenue & City Limits Line	San Jose	1	73
9.	ANDREWS Avenue Bet. Rose Avenue & Los Gatos Town Limits Line	Monte Sereno	7	13
10.	ANNIE LAURIE Avenue (Ext.) Easterly to Devonshire Avenue	Mt. View	3	45
11.	ARQUES Avenue Bet. 603 rd W. of Mathilda & Sunnyvale Avenue	Sunnyvale	2	86-87
12.	ASHCROFT Way Bet. Barton & Barton Drives	Sunnyvale	2	80
13.	AUSTIN Way Bet. Saratoga-Los Gatos & Quito Roads	Monte Sereno	7	11
14.	AUZERAIS Avenue Bet. Market Street & Lincoln Avenue	San Jose	1	78-79
<u>B</u>				
1.	BENTON Street Bet. Lincoln Street & The Alameda	Santa Clara	2	118
2.	BERNARDO Avenue Bet. El Camino Real & Evelyn Ave.	Sunnyvale	7	27
3.	BICKNELL Road Bet. Quito Road & Albright Subdivision	Monte Sereno	3	54
4.	BIRD Avenue Bet. Minnesota Avenue & Willow Glen Way	San Jose	1	23

<u>STREET AND SECTION</u>		<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
5.	BIRD Avenue-MONTGOMERY Street Bet. Julian Street & Minnesota Avenue	San Jose	1	35-38
6.	BLOSSOM HILL Road Bet. San Jose-Los Gatos & Harwood Road	Los Gatos	2	71-72
7.	BORREGAS Avenue Bet. Bayshore Hwy. & Mt. View-Alviso Road	Sunnyvale	2	85
8.	BRITTON Avenue Bet. Arques & Maude Avenues	Sunnyvale	3	30
<u>C</u>				
1.	CALABAZAS Boulevard Bet. Pomeroy Avenue & Existing Calabazas Boulevard North of El Camino Real	Santa Clara	3	26
2.	CALABAZAS Boulevard Bet. Pomeroy Avenue & Existing Calabazas Boulevard North of El Camino Real	Santa Clara	5	64
3.	CALAVERAS Road Bet. Dempsey Road & Piedmont Road	County	2	13
4.	CAMPBELL Avenue Campbell City Limits to Leigh Avenue	Campbell	2	35-36
5.	CAMPBELL Avenue (amended) Bet. Route 5 Freeway & Harrison Ave.	Campbell	2	38
6.	CENTRAL Expressway Mountain View City Limits to Santa Clara City Limits	Sunnyvale	2	47-55
7.	CENTRAL Expressway Mountain View City Limits to Santa Clara City Limits	Sunnyvale	5	39-48
8.	CHESTNUT Street Eighth Street to Luchessa Avenue	Gilroy	6	63-64
9.	CHESTNUT (North) Street Lewis Street to Leavesley Road	Gilroy	6	67-68
10.	CHURCH Street (Ext.) Liman Avenue to Welburn Avenue	Gilroy	3	8
11.	CHURCH Street Tenth Street to Thomas Road	Gilroy	3	37-38
12.	CHURCH Street Northerly Extension Bet. Welburn & Liman Avenues.	Gilroy	6	66

<u>STREET AND SECTION</u>		<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>C</u> cont.--				
13. CIVIC CENTER Drive Bet. Lincoln & Jackson Streets		Santa Clara	3	47
14. CIVIC CENTER Drive Bet. Monroe & Lafayette Streets		Santa Clara	7	42
15. COCHRAN Road Monterey Highway to 500'± Easterly of Peet Road		County	7	24-26
16. COE Avenue Bet. Lincoln & Bird Avenues		San Jose	1	67
17. COLEMAN Avenue Bet. Seymour & Newhall Streets		San Jose	1	51-52
 <u>D</u>				
1. DEL MEDIO Avenue (Ext.) Northeasterly from Del Medio Court		Mt. View	3	46
2. DEL MONTE Avenue (Ext.) Extension of Del Monte Avenue		Morgan Hill	2	127
3. DEWITT (South) Avenue & HALE Avenue Springs Avenue to Dunne Avenue; & Llagas Avenue to Warren Avenue		Morgan Hill	3	40
4. DUNFORD Avenue Bet. Quail Avenue & Marion Way		Sunnyvale	3	4
5. DUNNE (East) Avenue; WATSONVILLE Road; & DUNNE (West) Avenue Monterey Highway to Hill Road; Monterey to Bowden Avenue; Monterey Highway to 269'± S.W.		Morgan Hill	3	39
 <u>E</u>				
1. EL CAMINO Real Lawrence Station Road to Scott Boulevard		Santa Clara	2	103-105
2. EL CAMINO Real 1300'± West of Bernardo Avenue to 160'± East of Helen Avenue		Sunnyvale	2	128-138

<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>E</u> cont.--			
3. EL CAMINO Real Scott Boulevard to DeLaCruz Overpass	Santa Clara	2	141-142
4. EUNICE Avenue (ext.) Grant Road to Hospital Drive	Mt. View	5	30
5. EVELYN Avenue Bet. Bayview & Reed Avenues (except portions bet. Fair Oaks Avenue & PG&E R.O.W.	Sunnyvale	3	23-25
6. EVELYN Avenue Bet. Bayview & Reed Avenues (except portions bet. Fair Oaks Avenue and PG&E R.O.W.	Sunnyvale	4	58-60
<u>F</u>			
1. FAIR OAKS Avenue Bet. Bayshore Highway & Mt. View- Alviso Road	Sunnyvale	4	71-73
2. FAIR OAKS Avenue-WOLFE Road conn. Bet. Stewart Drive & Fair Oaks Avenue	Sunnyvale	6	20-21
3. FAIR OAKS Avenue Bayshore Highway & Persian Drive	Sunnyvale	7	1
4. FAIR OAKS Avenue-WOLFE Road conn. Bet. Duane & Britton Avenues	Sunnyvale	7	2
5. FAIR OAKS Avenue Bet. Duane & Ahwanhee Avenues	Sunnyvale	7	34
6. FIRST Street (South) Bet. Hollywood Avenue & City Limits Line	San Jose	1	17
7. FIRST Street (North) Bet. Route 5 Freeway & Bayshore Highway	San Jose	2	17-18
8. FIRST St. (North) Bet. Bassett Street & Route 5 Freeway	San Jose	2	22-23
9. FLOODWAY Area of UPPER LLAGAS Creek, Lower LLAGAS Creek & Tributaries	County Flood Control & Water District	7	29-33
10. FLOODWAY Area of UPPER LLAGAS Creek, Lower LLAGAS Creek & Tributaries	County Flood Control & Water District	7	35-39

	<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
11.	FLORA VISTA Avenue Bet. El Camino Real & Granada Avenue	Santa Clara	7	15
12.	FLYNN Avenue Whisman Road to Tyrella Avenue	Mt. View	4	74
13.	FONTANA Drive Bet. De Guigne Drive & San Xavier Avenue	Sunnyvale	3	5
14.	FOURTH Street Bet. City Limits Line & Reed Street	San Jose	1	53-55
15.	FOURTH Street (Amended) Bet. City Limits Line & Reed Street	San Jose	1	92-94
16.	FOURTH Street (North) Bet. Rosa Street & Bayshore Highway	San Jose	2	24-25
17.	FOXTAIL Drive Bet. future Iris Avenue & Golden Oak Drive	Sunnyvale	2	81

G

1.	GAIL Avenue Bet. Iris Avenue & Old San Francisco Road	Sunnyvale	2	82
2.	GOODYEAR Street Bet. Mastic & First Streets	San Jose	1	45
3.	GRANADA Drive Alma Street to North of Wright Avenue	Mt. View	3	58
4.	GREENWOOD Avenue Saratoga-Los Gatos Road to Withey Road	Monte Sereno	6	62
5.	GUADALUPE Freeway Bayshore Highway to Rosa Street	San Jose	2	43-44

H

1.	HAMILTON Avenue Bet. Santa Clara-Los Gatos Road & City Limits of San Jose	County	1	101
2.	HAMILTON Avenue Llewellyn Avenue to Nottingham Way	Campbell	2	34

<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>H</u> cont.--			
3. HAMILTON Avenue Llewellyn Avenue to San Tomas Aquino Road	Campbell	2	39
4. HECKER PASS Highway Hanna Street to Morey Avenue	Gilroy	5	27-28
5. HOMESTEAD Road In City of Santa Clara	Santa Clara	2	106-109
6. HOMESTEAD Road Bet. Mary Avenue & Calabazas Creek	County	6	2-19
7. HOMESTEAD Road Bet. Mary Avenue & Calabazas Creek	Sunnyvale	6	22-39
8. HOMESTEAD Road W. City Limits to Lawrence Expressway	Santa Clara	6	57
<u>I</u>			
1. IOWA Avenue Mathilda Avenue to Carroll Street	Sunnyvale	2	119
2. IRIS Avenue Bet. Fair Oaks Avenue to Golden Oak Drive	Sunnyvale	2	83
<u>J</u>			
1. JULIAN Street Bet. Stockton Avenue & First Street	San Jose	1	80-81
2. JULIAN Street - MCKEE Road Bet. First Street & Thirty-Fourth St.	San Jose	1	74-76

<u>STREET AND SECTION</u>		<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>K</u>				
1.	KARL Avenue Vineland Avenue to 2000'± Northerly thereof	Monte Sereno	6	40
2.	KEYES Street Bet. South First Street & City Limits Line	San Jose	1	33-34
3.	KIFER Road Bet. Sunnyvale City Limits & Santa Clara-Alviso Road	County	2	2-3
4.	KIFER Road Bet. Coffin Road & West City Limits	Santa Clara	2	125-126
5.	KIFER Road Bet. Lawrence Express. & most Easterly City Limits	Sunnyvale	3	20-21
6.	KING Road Bet. San Antonio Street & McKee Road; a portion of Alum Rock Avenue	San Jose	1	39-40
<u>L</u>				
1.	LAFAYETTE Street-Cut Through-Washington Street City Limits North to Civic Center Drive	Santa Clara	2	88-90
2.	LAFAYETTE Street Bet. Civic Center Drive & Bayshore Overpass	Santa Clara	3	56-57
3.	LARK Avenue Bet. San Jose-Los Gatos Road & Santa Clara-Los Gatos Road	Los Gatos	2	78
4.	LAS PALMAS Drive Extending North from Madera Avenue to El Camino Real	Santa Clara	3	28
5.	LATIMER Avenue First Street to Central Avenue	Campbell	6	58
6.	LAWRENCE Expressway Stevens Creek Road to Prospect Avenue	San Jose	2	40
7.	LAWRENCE Expressway Bet. City Limits Line & Lassen Avenue	San Jose	2	56-57
8.	LAWRENCE Expressway Bet. Stevens Creek Road & 150'± South of Lapa Drive	San Jose	2	70

<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>L</u> cont. --			
9. LAWRENCE Station Road Homestead Road South to Stevens Creek Boulevard	Santa Clara	2	98-99
10. LEAVESLEY Road-FERGUSON Road Bet. Monterey Highway & Pacheco Pass Highway	County	6	49-53
11. LEXINGTON-LIBERTY Streets Connection Via Monastery Way	Santa Clara	4	76
12. LIBERTY Street Bet. Lincoln & Lafayette Streets	Santa Clara	2	124
13. LINCOLN Avenue Bet. Willow Street & Minnesota Avenue	San Jose	1	22
14. LINCOLN Avenue Bet. Coe Avenue & Willow Street	San Jose	1	28
15. LINCOLN Avenue From 150'± Northerly of Auzerais Avenue to 621'± Southerly of Savaker Avenue	San Jose	1	46
16. LINCOLN Avenue Bet. San Carlos Street & Los Gatos Creek	County	1	86
17. LINCOLN Avenue Bet. Minnesota Avenue & City Limits Line	San Jose	1	96-97
18. LINCOLN Avenue Bet. Los Gatos Creek & Garfield Avenue	San Jose	1	98
19. LINCOLN Street Bet. El Camino Real & Lexington Avenue	Santa Clara	3	27
20. LOS GATOS-ALMADEN Road Bet. San Jose-Los Gatos Road & Harwood Road	Los Gatos	2	79
<u>M</u>			
1. MADELEINE Drive From White Road to Kirk Avenue	County	1	16
2. MARTIN Avenue Bet. Scott & De La Cruz Boulevards	Santa Clara	3	42-43

<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
M cont.--			
3. MARY Avenue Homestead Road to Washington Avenue	Sunnyvale	2	60-69
4. MARY Avenue Bet. Washington Avenue and a point 766'± North of Evelyn Avenue	Sunnyvale	2	101-102
5. MATHILDA Avenue Mountain View-Alviso Road to Maude Avenue	County	1	1
6. MAUDE Avenue Mathilda Avenue to most Westerly City Limits	Sunnyvale	2	45-46
7. McGLINCY Lane; CRISTICH Lane; GRIFFITHS Lane In the City of Campbell	Campbell	2	140
8. McKEAN Road-Unit I Harry Road to Schillingsburg Avenue	County	4	46-57
9. McKEAN Road -Unit II Schillingsburg Avenue to Calero Reservoir	County	4	61-70
10. McKEE Road Bet. Coyote Creek & Easterly City Limits Line	San Jose	2	1
11. MERIDIAN Road Bet. San Carlos Street & Park Avenue	San Jose	2	9
12. MERIDIAN Road San Carlos Street to Dry Creek Road	County	2	26
13. MERIDIAN Road Bet. W. San Carlos Street & Dry Creek Road	County	2	27-29
14. MIDDLEFIELD Road Bet. Alvin Street & Sierra Vista Avenue	Mt. View	3	14-15
15. MIDDLEFIELD Road Moffett Boulevard to Sierra Vista Ave.	Mt. View	3	50-53
16. MINNESOTA Avenue Bet. Hicks Avenue & W.P.R.R.	San Jose	1	56-57
17. MONROE Street Bet. El Camino Real & Newhall Street	Santa Clara	2	116-117
18. MONROE Street Bet. El Camino Real & Stardust Ct.	Santa Clara	3	48
19. MONTAGUE Road Bet. Santa Clara-Alviso Road & San Jose-Alviso Road	County	2	10
20. MONTEBELLO Avenue Montecito Avenue to proposed Madera Avenue and from proposed Montebello to Sierra Vista Avenue	Mt. View	3	49

<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>M</u> cont. --			
21. MONTEBELLO Avenue (Amended) Montecito Avenue to proposed Madera Avenue and from proposed Montebello to Sierra Vista Avenue	Mt. View	5	12
22. MONTEREY Road - US 101 Gilroy City Limits to Llagas Creek	County	1	9-12
23. MONTEREY Road - US 101 San Jose City Limits to Lewis Road	County	1	13
24. MOORE Avenue Bet. Montclari Road & Bicknell Road	Los Gatos	5	11
25. MORGAN HILL Master Street Plan (Nov. 1962 - Amendments)	Morgan Hill	3	9-11
26. MORSE Avenue Bet. Morse Court & Ahwanee Avenue	Sunnyvale	3	22
<u>N</u>			
1. NAGLEE Avenue Bet. Bascom Avenue and The Alameda	San Jose	1	58-59
2. NEW BRUNSWICK Drive Bet. Homestead Road & a point 1449'± North of Homestead Road	Sunnyvale	2	84
3. NEWHALL Street Bet. Maria Street & N. Winchester Blvd.	Santa Clara	7	47
<u>O</u>			
1. OLD SAN FRANCISCO Road Bet. Sunnyvale Avenue & Wolfe Road	Sunnyvale	3	16-18
2. OLIVE Avenue Bet. Pastoria & Mary Avenues	Sunnyvale	4	75

	<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>P</u>				
1.	PARK Avenue Bet. Market Street & Meridian Road	San Jose	1	20-21
2.	PARK Avenue Bet. Meridian Road & City Limits Line	San Jose	1	88-89
3.	PHYLLIS Avenue In the vicinity of Grant Road	Mt. View	6	41
4.	POLHEMUS-WEST TAYLOR Street Bet. The Alameda & North First Street	San Jose	1	47-48
5.	POLLARD Road Bet. Winchester Boulevard & West Town Limits	Los Gatos	7	40-41
6.	PREVOST Street Bet. San Carlos & Willow Streets	San Jose	1	31-32
7.	PUBLIC Alley California Street to Dana Street	Mt. View	5	29
<u>Q</u>				
1.	QUITO Road Bet. Saratoga Avenue & SPRR	County	2	11-12
2.	QUITO Road Bet. Ravenwood Drive & Austin Way	Saratoga	6	69-73
3.	QUITO Road Bet. Bicknell Road & San Tomas Aquino Creek & Bet. San Tomas Aquino Creek & Oak Drive	Monte Sereno	7	12
<u>R</u>				
1.	RACE Street Bet. The Alameda & Park Avenue	San Jose	1	44
2.	RAILWAY Avenue & Kennedy Drive In the City of Campbell	Campbell	2	97
3.	REED Avenue Bet. Wolfe Road & Lawrence Expressway	Sunnyvale	4	21-23

<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>R</u> cont.--			
4. REMINGTON-FAIR OAKS Avenue Bet. Stevens Creek & Bayshore Highway	Sunnyvale	5	49-62
5. ROSE Avenue Bet. White Road & Capitol Avenue	County	1	95
6. ROSE Avenue Bet. Saratoga-Los Gatos Road & Los Gatos Town Limits Line	Monte Sereno	7	14
<u>S</u>			
1. SAN ANTONIO Street Bet. 17th Street & King Road	San Jose	1	99-100
2. SAN CARLOS Street Bet. Market Street & Royal Avenue	San Jose	1	18-19
3. SAN CARLOS Street Bet. 2nd Street & 10th Street	San Jose	1	29
4. SAN CARLOS Street Bet. 10th & 17th Streets & a portion of 17th Street	San Jose	2	19
5. SAN CARLOS Street Bet. Market & 2nd Streets	San Jose	2	42
6. SAN FERNANDO Street Bet. 4th Street & 10th Street	San Jose	1	87
7. SAN FERNANDO Street Bet. 10th & 17th Streets	San Jose	2	21
8. SAN JOSE Avenue Bet. Monterey Road & City Limits Line	San Jose	2	20
9. SAN JOSE-LOS GATOS Road Bet. White Oaks Avenue & Kennedy Road	Los Gatos	2	73-74
10. SAN JOSE-LOS GATOS Road Bet. Charles Street & Blossom Hill Road	Los Gatos	3	35
11. SAN TOMAS AQUINO Parkway City of Santa Clara to Camden Avenue	County	2	30
12. SAN TOMAS AQUINO Parkway City of Santa Clara to Camden Avenue	County	2	31-32
13. SAN TOMAS Boulevard Stevens Creek Road to Camden Avenue (existing)	Campbell	2	33
14. SAN TOMAS Expressway Bet. Cabrillo Avenue & S.P.R.R.	Santa Clara	2	100

	<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
S	cont.--			
15.	SAN TOMAS Expressway Kifer Road to Stevens Creek Blvd.	Santa Clara	2	110-115
16.	SANTA CLARA-LOS GATOS Road Bet. Latimer & Hacienda Avenues	County	1	90
17.	SANTA CLARA-LOS GATOS Road Bet. Hacienda Avenue & Town of Los Gatos	County	2	6-8
18.	SANTA CLARA-LOS GATOS Road Stevens Creek Road to Campbell City Limits	County	2	14-16
19.	SANTA CLARA-LOS GATOS Road Bet. Parr & Santa Cruz Avenues	Los Gatos	2	75-77
20.	SANTA TERESA Boulevard Bet. Spring Avenue & Hecker Pass Highway	County	3	59-73
21.	SANTA TERESA Boulevard Bet. Cottle Road & Palm Avenue	County	4	1-20
22.	SANTA TERESA Boulevard Hecker Pass Highway to Monterey Road-Bloomfield Intersection	County	5	32-38
23.	SANTA TERESA Boulevard W. Main Avenue to Spring Avenue + 690'± of Sunnyside Avenue	Morgan Hill	6	42-45
24.	SANTA TERESA Boulevard Palm Avenue to Llagas Avenue	County	6	46-48
25.	SARATOGA Avenue Stevens Creek Boulevard to Market Street	Santa Clara	3	1-3
26.	SARATOGA Avenue Bet. Fruitvale Avenue & Saratoga- Sunnyvale Road	Saratoga	7	43-46
27.	SARATOGA Avenue Intersection of Saratoga Avenue & Pruneridge Avenue	Santa Clara	7	51
28.	SARATOGA-SUNNYVALE Road Bollinger Road to Prospect Avenue	San Jose	2	41
29.	SARATOGA-SUNNYVALE Road Bet. a point 273'± North of center- line of that segment of Bollinger Road on the West side of Saratoga- Sunnyvale Road & Prospect Road	San Jose	2	58-59
30.	SCOTT Boulevard Bet. Kifer Road & Monroe Street	Santa Clara	3	12-13
31.	SCOTT Boulevard Bet. Monroe Street & El Camino Real	Santa Clara	3	41
32.	SOUTH Drive (Ext.) From present terminus to Miramonte Avenue	Mt. View	6	1

	<u>STREET AND SECTION</u>	<u>FILED BY</u> <u>(City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>S</u>	cont.--			
33.	STATE ROUTE 5 (South Bascom) Bet. San Jose & Los Gatos	County	1	2-7
34.	STATE ROUTE 5 (South Bascom) Bet. San Jose & Los Gatos	County	1	15
35.	STATE ROUTE 5 (South Bascom) Bet. San Jose & Alameda County Line	County	1	68-72
36.	STEVENS CREEK Road From Bascom Avenue to SPRR at Monte Vista	County	1	8
37.	STEVENS CREEK Boulevard In City of Santa Clara	Santa Clara	2	91-94
38.	STEVENS CREEK Boulevard Redwood Avenue to a point 150'± westerly of Stern Avenue	County	2	120-123
39.	STORY Road Bet. San Jose City Limits & Fleming Avenue	County	1	84-85
40.	SUNOL Street Bet. Park Avenue & The Alameda	San Jose	1	91
<u>T</u>				
1.	TASMAN Drive Morse Avenue & Calabazas Drainage Channel	Sunnyvale	7	3-4
2.	TAYLOR Avenue Bet. Fair Oaks & Britton Avenues	Sunnyvale	3	31
3.	TENTH Street (Circulation element of the general plan)	Gilroy	3	6-7
4.	TERRAINE Street-ALMADEN Avenue Bet. Julian & West Santa Clara Sts.	San Jose	1	64
5.	THIRD Street Santa Teresa Dr. to "Westwood Acres"	Gilroy	3	36
6.	THOMAS Road & NORMAN Ave. (Portions)	Santa Clara	3	29
7.	TULLY Road & SWIFT Lane Bet. Monterey Road & Easterly Terminus of Swift Lane	County	2	4-5
8.	TWENTY-FOURTH Street Bet. McKee Road & William Street	San Jose	1	65-66

	<u>STREET AND SECTION</u>	<u>FILED BY (City)</u>	<u>BOOK NO.</u>	<u>PAGES</u>
<u>U</u>				
1.	UNIVERSITY Avenue Bet. Main Street & Blossom Hill Road	Los Gatos	5	67-69
2.	URBAN Drive & extension of LOGUE & CLYDE Avenues	Mt. View	5	65
3.	UVAS Parkway Thomas Road at Monterey Highway to proposed extension of Santa Teresa Boulevard	Gilroy	4	44-45
4.	UVAS Road-Unit I Uvas Dam to Watsonville Road	County	5	1-10
5.	UVAS Road Croy Road to 5000'± Southerly	County	7	5-10
6.	UVAS Road 5000'± Southerly of Croy Road to 12,000'± Southerly	County	7	16-23
<u>V</u>				
1.	VERDANT Avenue Moffett Boulevard to Stevens Creek Freeway	Mt. View	6	54
2.	VINELAND Avenue Winchester Boulevard Westerly 3000'± to end of existing R.O.W.	Monte Sereno	3	55
3.	VIRGINIA-HOME Street Bet. Bird Avenue & 10th Street	San Jose	1	62-63
<u>W</u>				
1.	WALKER Drive Whisman Road to Tyrella Avenue	Mt. View	4	77
2.	WALSH Avenue Bet. San Tomas Expressway & Lafayette Street	Santa Clara	2	95-96
3.	WATSONVILLE Road-Unit I Uvas Road to Hecker Pass Highway	County	4	24-43

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W	cont.--			
4.	WATSONVILLE Road-Unit II Uvas Road to 200'± north of Sunnyside Avenue	County	5	13-26
5.	WATSONVILLE Road-Unit III From 200'± North of Sunnyside Avenue to Monterey Highway	County	5	31
6.	WILLIAM Street Bet. 1st & 24th Streets	San Jose	1	49-50
7.	WILLOW GLEN Way Bet. Bird Avenue & Guadalupe River	San Jose	1	30
8.	WILLOW Street Bet. South 1st Street & Meridian Road	San Jose	1	41-43
9.	WILLOW Street Bet. Dry Creek & Meridian Roads	San Jose	1	77
10.	WINCHESTER Road In City of Campbell	Campbell	2	37
11.	WINCHESTER (North) & UNIVERSITY Street Bet. Stevens Creek Boulevard & Market Street	Santa Clara	3	32-34
12.	WINCHESTER (North) Boulevard-LINCOLN Street Conn. Bet. Bellomy & Liberty Streets	Santa Clara	5	63
13.	WINCHESTER (North) Boulevard-LINCOLN Street Conn. Bet. Bellomy & Liberty Streets	Santa Clara	5	66
14.	WINCHESTER Boulevard Bet. Shelburne Way & Vineland Avenue	Monte Sereno	7	28
15.	WITHEY Road Beck Avenue to Greenwood Avenue	Monte Sereno	6	55
16.	WITHEY Road Beck Avenue to Greenwood Avenue	Monte Sereno	6	56
17.	WOLFE Road (Ext.) Arques Avenue to De Guigne Drive	Sunnyvale	2	139
18.	WOLFE Road Bet. Kifer Road & Britton Avenue; Bet. Reed Avenue & Northerly Boundary of Tract #3192	Sunnyvale	6	59
19.	WOLFE Road Bet. Kifer Road & Britton Avenue; Bet. Reed Avenue & Northerly Boundary of Tract #3192	Sunnyvale	6	60-61
20.	WRIGHT Avenue (Ext.) Westerly to Beatrice Street	Mt. View	3	44
X -	None			
Y				
T.	YARDIS Court West from Miramonte Ave.	Mt. View	6	65
Z -	NONE			