

BEFORE THE
FEDERAL AVIATION ADMINISTRATION
WASHINGTON, D.C.

JEFF BODIN and
GARLIC CITY SKYDIVING,

Complainant,

v.

THE COUNTY OF SANTA CLARA,
CALIFORNIA,

Respondent.

FAA Docket No. 16-11-06

**MOTION BY THE COUNTY OF SANTA CLARA TO RELIEVE PALO ALTO
AIRPORT (PAO) AND REID-HILLVIEW (RHV) AIRPORT FROM THE GRANT
FUNDING SUSPENSION IMPOSED BY THE DECEMBER 19, 2011 DIRECTOR'S
DECISION REGARDING SAN MARTIN AIRPORT (E16)**

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I.

INTRODUCTION

The County of Santa Clara (the "County") operates three airports: the South County airport located in San Martin (E16), Palo Alto Airport (PAO), and Reid-Hillview Airport (RHV).

On December 19, 2011, in the above-captioned docket, the Director of the Office of Airport Compliance and Management Analysis (the "Director") issued a determination that the County violated Grant Assurance 5 (*Preserving Rights and Powers*) and Grant Assurance 22 (*Economic Nondiscrimination*) by not permitting an on-airport skydiving drop zone at the South County Airport ("E16"). The County's appeal is currently pending before the Federal Aviation Administration ("FAA"). That appeal seeks reversal of the Director's determination on grounds that it is not supported by the administrative record, regulations, law or policy.

The Director's determination regarding the alleged grant assurance violation at E16 also ordered a suspension of FAA grant funding. That order not only suspends grant funding for the subject E16, but also suspends grant funding for Palo Alto Airport ("PAO") and Reid-Hillview Airport ("RHV").

By this motion, the County respectfully requests that the grant funding suspension be lifted for PAO and RHV. These two airports are not the subject of the Part 16 Complaint at issue and these two airports are being unnecessarily penalized. Furthermore, there has been no request to conduct skydiving activities at either PAO or RHV. Unless the grant funding suspension is lifted with respect to these two airports, the County will be unable to undertake necessary pavement rehabilitation projects there, which projects were to be funded by FAA grants in 2012 from the accrued entitlement funds for these two airports. Moreover, if the pavement

rehabilitation projects are not undertaken this year as planned, then the entitlement funds that have been saved since 2009 for use on these important projects will no longer be available.

PAO, RHV, and the members of the public that use and rely on these airports, should not be burdened and penalized by the grant funding suspension imposed as part of the Director's determination on the Part 16 Complaint regarding the County's denial of a request for an on-airport skydiving drop zone at E16. This is especially true since that determination is currently under appeal. The County respectfully requests that the grant funding suspension be lifted with respect to PAO and RHV, so that necessary pavement improvement and maintenance can be funded and performed at these airports.

II.

APPLICABLE FEDERAL LAW AND FAA POLICY

The FAA Airport Compliance Program is designed to ensure the availability of a national system of safe and properly maintained public-use airports operated in a manner consistent with the airport owner's federal obligations and the public's investment in civil aviation. The Airport Compliance Program monitors the administration of rights pledged by airport sponsors to the people of the United States in exchange for monetary grants and donations of federal property to ensure that the public interest is being served. The federal role in encouraging and developing civil aviation has been augmented by various legislative actions, which authorize programs for providing funds and other assistance to local communities for the development of airport facilities. In each such program, the airport owner or sponsor assumes certain obligations to maintain and operate its airport facilities safely, efficiently, and in accordance with specified conditions. These commitments are important factors in maintaining safety and efficiency in

airport design, construction, operation and maintenance, as well as ensuring the public reasonable access to the airport.

Title 49 U.S.C. § 47101, *et seq.* provides for federal airport financial assistance for the development of public-use airports under the Airport Improvement Program established by the Airport and Airway Improvement Act of 1982, as amended. Title 49 U.S.C. § 47107 sets forth assurances to which an airport sponsor agrees as a condition of receiving federal financial assistance. Upon acceptance of an Airport Improvement Program grant, the assurances become a binding contractual obligation between the airport sponsor and the federal government. The assurances made by airport sponsors in Airport Improvement Program grant agreements are important factors in maintaining a viable national airport system.

As a condition precedent to providing airport development assistance under the Airport Improvement Program, the Secretary of Transportation and, by extension, the FAA, must receive certain assurances from the airport sponsor. Title 49 U.S.C. 47107(a) sets forth the statutory sponsorship requirements to which an airport sponsor receiving federal financial assistance must agree. FAA Order 5190.6B, *FAA Airport Compliance Manual* (Order) issued on September 30, 2009, provides guidance to be followed by the FAA in carrying out its legislatively mandated functions related to compliance with federal obligations of airport sponsors. The FAA considers it inappropriate to provide federal assistance for improvements to airports where the benefits of such improvements will not be fully realized due to inherent restrictions on aeronautical activities. 12/19/11 Director's Determination at p. 12.

III.

THE PART 16 COMPLAINT REGARDING E16 THAT LED TO SUSPENSION OF THE COUNTY'S FAA GRANT FUNDING

The appeal of the above-referenced docket matter is presently before the FAA and arises from the Director's decision on a formal complaint regarding E16 that was filed in accordance with the FAA Rules of Practice for Federally Assisted Airport Proceedings, Title 14 Code of Federal Regulations Part 16.

A. FACTUAL BACKGROUND

The Complainant proposes to use E16 to drop skydivers through the middle of a congested airway that is the main approach route to the Mineta San José International Airport. Skydivers are expected to land at E16 on a small landing zone located approximately 1,200 feet from a major freeway. The proposed site is approximately 300 feet from the nearest home, 700 feet from the active runway at E16, and 1,200 feet from highway 101. The proposal creates a hazard to air traffic and to persons and property on the surface that cannot be mitigated.

The FAA initially determined that the proposed skydiving operation would be operated in the safest manner if relocated elsewhere, but later concluded that neither safety nor efficiency would be compromised by the skydiving operations as proposed at E16. After rejecting the County's offer to use E16 for business operations and for the take-off and landing of jump aircraft, but to locate the drop zone at an off-site location where the various hazards would be substantially mitigated, Complainant filed a Part 16 complaint contending that the County is not complying with its grant assurances and obligations to the FAA at E16.

B. THE DIRECTOR'S DETERMINATION

A Director's Determination is an initial non-final decision. In a Director's Determination issued on December 19, 2011, the Director found that the County is currently in violation of its

federal obligations with respect to Grant Assurance 5 (*Preserving Economic Rights and Powers*) and Grant Assurance 22 (*Economic Nondiscrimination*).

The Director's Determination further ordered that future grant applications for Airport Improvement Program discretionary grants under 49 U.S.C. § 47115 and general aviation grants under 49 U.S.C. §47114(d) requested by the County are suspended until further notice. This suspension of grant funding applies not only to E16 that was the subject of the Part 16 Complaint, but also to PAO and RHV.

C. THE COUNTY'S APPEAL

The County timely appealed from the Director's determination and it is presently anticipated that the Associate Administrator will render a final agency decision by June 30, 2012. Even though the Director's decision is a non-final decision that is presently on appeal, the suspension of grant funding ordered by that decision presently remains in place, thus preventing grant funding and pavement maintenance repairs at PAO and RHV, which were not involved in the Part 16 Complaint regarding proposed skydiving operations at E16.

Further, because the law provides the parties a right to appeal the FAA's final agency decision and order in the United States Court of Appeals, regardless of how the Associate Administrator rules in issuing the FAA's final agency decision, it is possible that this matter may not be settled for quite some time. Thus, the order suspending grant funding imposes a harsh, unnecessary, and potentially lengthy, burden and penalty on two airports that are entirely unrelated to the Part 16 Complaint – PAO and RHV – and on the members of the public who rely on those two airports.

IV.

GRANT FUNDING AND PAVEMENT IMPROVEMENT AT PAO AND RHV

A. PAO

The County operates PAO under a lease from the City of Palo Alto, which lease expires in 2017. Grant assurances cover a period of 20 years. However, only a few years still remain on the PAO lease. Accordingly, for the last decade or more, the grant assurances for PAO have been signed not by the County, but rather by the City of Palo Alto.

B. FAA FUNDING FOR PAO AND RHV

PAO and RHV are both reliever airports under the FAA's National Program of Integrated Airport Systems. As such, each fiscal year, the PAO and RHV qualify for \$150,000 for their entitlement grant programs. Each of those airports is allowed to roll-over entitlement grant funds for a maximum four-year period in order to build-up a maximum \$600,000 entitlement accrual to fund necessary projects under FAA-issued grants.

Since 2009, PAO and RHV have been accruing their entitlement funds in preparation for their 2012 pavement rehabilitation and improvement projects.

The Director's decision arising from the Part 16 Complaint regarding proposed skydiving operations at E16 suspended grant funding for all three County Airports, thus preventing PAO and RHV from obtaining the grant approvals necessary to move forward and use their accrued entitlement funds for the planned and necessary pavement rehabilitation and improvement projects further described below. Moreover, as a result of the grant fund suspension, at the end of the federal government's fiscal year, the fiscal year 2009 portion of the funding-accrual for PAO and RHV will become permanently lost for project use at those airports.

C. THE 2012 PROJECTS AT PAO AND RHV

Unless the grant funding suspension is lifted with respect to PAO and RHV – which were not involved in the E16 Part 16 Complaint that gave rise to the grant funding suspension order – those airports will be unable to obtain the entitlement funding grants necessary to move forward with their 2012 projects. The purpose of these projects is to fix the various airport pavement, marking, and signage issues discovered by the FAA's Runway Safety Action Team.

1. PAO Project

The pavement rehabilitation design project at the Palo Alto Airport (PAO) is funded under Airport Improvement Program No. 3-06-0182-08.¹ The runway and taxiway pavement surfaces at Palo Alto Airport (PAO) are exhibiting medium severity weathering and isolated low severity edge cracking as a result of aging. If left unattended, the severity of the pavement weathering will increase and the airport may be more prone to foreign object debris issues. The cracks will also continue to expand and widen, creating foreign object debris issues as well.

There are also multiple areas where dips have formed on the runway perpendicular to the runway centerline. The dips, which are in locations where utilities have been previously installed, appear to have formed over time due to differential settlement of the utility backfill. Unless addressed, the severity of the dips may increase over time, which could result in more serious operational issues including potential loss of control, propeller strikes or other aircraft damage.

Under the Airport Improvement Program, the improvements at Palo Alto Airport (PAO) will include micro-surfacing of taxiway and pavements and rehabilitation of the asphalt runway surface to correct the dips and pavement cracks in the runway surface. Larger cracks in the

¹ A copy of the Engineer's Design Report for the PAO Pavement Rehabilitation Project is attached hereto as Exhibit 1.

southern edge of pavement on Taxiway C will be addressed with a full asphalt removal and replacement, while less severe cracks along the remaining taxiways will be addressed by the asphalt micro-surfacing process. Additionally, under the Airport Improvement Program, the last two tie-downs for each row along Taxiway Z will be removed to prevent parked aircraft from potentially penetrating the taxiway object-free area and the pavement in the removal areas will be repaired. When all pavement rehabilitation is complete, all pavement markings within the improvement limits will be reapplied.

Updates to FAA AC 150/534-1K, result in several marking changes since the current markings were placed. The threshold bar remains 10 feet wide but the spacing to the beginning of the threshold marking of the runway designator is only placed 10 feet past the marking edge instead of 20 feet. This changes the locations of the other runway markings by 10 feet for the entire length of the runway. With the removal of the existing tie-downs and markings that violate the taxiway object free area, new double yellow taxiway edge markings will be applied through the tie-down rows connecting to the non-movement markings across the taxilanes. The runway hold position markings will be 12-inch markings with 12-inch spaces, outlined in black paint as directed in the AC. See, PAO Engineer's Design Report at p. 3.

2. *RHV Project*

At RHV, the pavement rehabilitation design project to Runway 13L-31R and taxiways A,B,C, D, E, Y and Z is funded by Airport Improvement Program No. 3-06-0225-14.² On Runway 13L-31R, the asphalt surface is exhibiting low severity weathering as a result of aging. There are medium severity longitudinal cracks (along paving joints) and low to medium severity transverse cracks through the limits of the runway pavement. Isolated areas, primarily at the

² A copy of the Engineer's Design Report for the RHV Pavement Rehabilitation Project is attached hereto as Exhibit 2.

intersections of the taxiway connectors, are exhibiting signs of low to medium severity alligator-cracking and medium severity block cracking. Weed growth is occurring through the open cracks. Taxiways A, B, C, D, E, Y and the southern portion of taxiway Z have similar pavement conditions.

The planned improvements at RHV include pavement micro-surfacing of taxiway and runway pavements and installation of airfield signage, including replacing select sign legends to meet FAA standards as well as removing non-frangible mounted signs from within the safety areas.

New signs are also needed to clearly direct pilots around the airfield. Realignment of Taxiway C geometry at the intersection with Runway 13L-31R and the installation of new edge lights are to be addressed by this project as well. When all pavement rehabilitation is complete, new pavement markings will be applied per the FAA AC 150/5340-1K "Standards for Airport Markings" and as recommended in the January 23, 2007, Reid-Hillview Airport Runway Safety Action Plan update. See, Exhibit 2 - RHV Engineer's Design Report at pp. 1-2.

V.

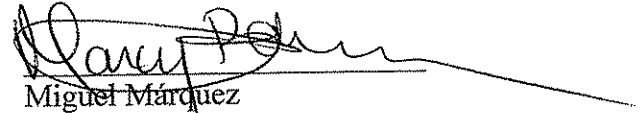
CONCLUSION

For the reasons set forth above, the County respectfully requests that the FAA grant its motion to relieve the County's PAO and RHV from the funding suspension imposed upon the

County as a result of the Director's Determination regarding the Part 16 Complaint pertaining to proposed skydiving operations at E16.

Dated: June 4, 2012

Respectfully submitted,



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Counsel for Respondent
County of Santa Clara

CERTIFICATE OF SERVICE

I hereby certify in accordance with 14 CFR § 16.15(a) that today I served the foregoing **MOTION BY THE COUNTY OF SANTA CLARA TO RELIEVE PALO ALTO AIRPORT (PAO) AND REID-HILLVIEW (RHV) AIRPORT FROM THE GRANT FUNDING SUSPENSION IMPOSED BY THE DECEMBER 19, 2011 DIRECTOR'S DECISION REGARDING SAN MARTIN AIRPORT (E16)**, on the following persons at the following address by Federal Express:

| | |
|---|---|
| Richard J. Durden Attorney at Law 27987 Richmond Hill Road Conifer, CO 80433 | Office of the Chief Counsel Attn: Docket Clerk FAA Part 16 Airport Proceedings Docket AGC 610 Federal Aviation Administration U.S. Department of Transportation 800 Independence Avenue, S.W., Room 923 Washington, D.C. 20591 |
| Randall S. Fiertz, Director Office of Airport Compliance and Management Analysis Federal Aviation Administration U.S. Department of Transportation 800 Independence Avenue, S.W., Room 615 Washington, D.C. 20591 | Associate Administrator of Airports Attn: Catherine M. Lang ARP-1 Federal Aviation Administration U.S. Department of Transportation 800 Independence Avenue, S.W., Room 600 Washington, D.C. 20591 |

Dated this 4th day of June 2012

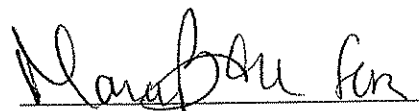
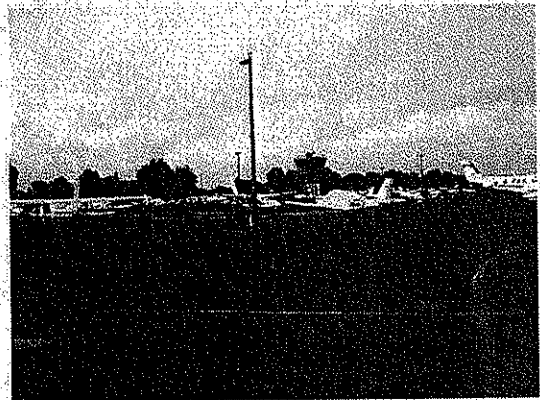

Elizabeth G. Pianca
for the Respondent

EXHIBIT 1



Palo Alto Airport

Pavement Rehabilitation

AIP No. 3-06-0182-08

Prepared for
County of Santa Clara



and

Federal Aviation Administration



Prepared by

**Mead
& Hunt**

DRAFT

MAY 2012

I. PROJECT SCOPE

This report documents the design of improvements to Runway 13-31, Taxiways A,B,C,E,and Z at the Palo Alto Airport (PAO) being funded under Airport Improvement Program (AIP) No. 3-06-0182-08. These improvements include pavement microsurfacing of taxiway and runway pavements and rehabilitation of the asphalt runway surface to correct depressions perpendicular to the runway centerline resulting from settlement of backfill in utility line trenches that have caused grade dips and pavement cracks in the runway surface. Larger cracks in the southern edge of pavement on Taxiway C will be addressed with a full asphalt removal and replacement, while less severe cracks along the remaining taxiways will be addressed by the asphalt microsurfacing process. The removal of the last two tie-downs in each row, along Taxiway Z, will be made to prevent parked aircraft from potentially penetrating the Taxiway Object Free Area and the repair of the pavement in the removal areas. When all pavement rehabilitation is complete, new pavement markings will be applied per the FAA AC 150/5340-1K.

Specifically, the improvements included in this Project are:

- Cleaning the existing asphalt surface in preparation for microsurfacing.
- Removal of paint markings and rubber build-up on the pavement.
- Cleaning pavement cracks and treating with herbicide.
- Removal of existing tie-down anchors.
- Isolated asphalt pavement removal and replacement with new asphalt pavement section.
- Application of an asphalt microsurfacing treatment.
- Applying new pavement markings.

II. DESIGN STANDARDS

The methodologies used in developing designs for this Project are in conformance with applicable FAA standards. The following Advisory Circulars have been reviewed during the design of the Project:

| | |
|-----------------|---|
| AC 150/5300-13 | <i>Airport Design</i> |
| AC 150/5320-6E | <i>Airport Pavement Design and Evaluation</i> |
| AC 150/5340-1K | <i>Standards for Airport Markings</i> |
| AC 150/5370-2F | <i>Operational Safety on Airports During Construction</i> |
| AC 150/5370-10F | <i>Standards for Specifying Construction of Airports</i> |
| AC 150/5370-12A | <i>Quality Control of Construction for Airport Grant Projects</i> |

III. TOPOGRAPHIC SURVEYS

No topographical survey was performed.

IV. GEOTECHNICAL INVESTIGATION

No geotechnical investigation was performed.

III. ANALYSIS AND DESIGN

A. PRELIMINARY DESIGN REPORT

A visual survey of the current pavement conditions was performed February 29, 2012, for Runway 13-31 and Taxiways A, B, C, E, and Z. With the exception of the run-up apron at Runway 31, the results of the survey indicated the conditions of pavement areas are uniform with respect to the observed distresses. The asphalt surfaces are exhibiting medium severity weathering and isolated low severity edge cracking, as a result of aging. The largest cracks appear to be along the southern edge of Taxiway C. This pavement should be saw-cut and removed for a 10-foot width from the edge of the runway pavement to the edge of the parallel Taxiway Z pavement. Due to the severity of the weathering on the asphalt pavement surface, and the frequency of smaller cracks forming along the pavement edges, a slurry micro-seal application is recommended.

As defined in the FAA's current AC 150/5340-1K, several marking changes have taken place since the current markings were placed. The threshold bar remains 10 feet wide but the spacing to the beginning of the threshold marking of the runway designator is only placed 10 feet past the marking edge instead of 20 feet. This changes the locations of the other runway markings by 10 feet for the entire length of the runway. A non-movement area boundary marking is recommended across the opening of the adjacent apron to delineate aircraft movement limits on the apron adjacent to Taxiway Z. With the removal of the existing tie-downs and markings that violate the taxiway object free area, new double yellow taxiway edge markings will be applied through the tie-down rows connecting to the non-movement markings across the taxilanes. Taxiway edge markings, arrowheads, and taxiway shoulder markings are shown on the lead-in taxiway at the 31 end of the runway to provide pilots direction for the taxiway alignment up to the runway threshold. All hold position markings on the taxiways will be 12-inch markings with 12-inch spaces.

B. DESIGN AIRCRAFT AND LOADING

PAO is a general aviation Airport with an FAA Aircraft Approach Category B and FAA Airplane Design Group I. The Airport Layout Plan indicates that the Airport design standards pertain to facilities for small airplanes exclusively. The current runway configuration is 13-31 and is 2,443 feet in length and 68 feet wide, with a parallel taxiway and four connector taxiways. The runway is supported by an apron that is located to the south, adjacent to Runway 31. The critical aircraft for design at this Airport is a light twin engine with the pavement loading rated for 12,500 lbs., single wheel gear.

C. SEALCOAT ALTERNATIVES

The recommendation of the engineer for pavement rehabilitation, based on the visual inspection, was a slurry seal. The County pointed out that microsurfacing has been very successful on other projects undertaken by the County, and that many local contractors can perform microsurfacing to meet the design standards so bid prices would be competitive. The engineer has reviewed the County's specification for microsurfacing and has confirmed this process will provide the level of pavement rehabilitation needed for PAO.

D. PAVEMENT MARKING

All pavement markings within the improvement limits will be reapplied. Due to updates to FAA AC 150/5340-1K, new offsets starting at the runway threshold bar result in changes from the existing

layout. The runway hold position markings will be 12-inch markings with 12-inch spaces, outlined in black paint as directed in the AC.

E. SIGNAGE AND LIGHTING

No existing changes are proposed to the airfield signage and lighting in this Project. Potential issues with ground squirrels have been identified to the Airport and changes may be proposed on a future Project.

IV. AIRPORT SAFETY AND OPERATIONS DURING CONSTRUCTION**1. Runway and Taxiway Closure.**

The runway will be closed and existing lights, signs and NAVAIDs decommissioned during periods of construction. Low barricades will be placed along the boundary edge of the proposed work area and the tie-downs on the apron to minimize the possibility of an aircraft or vehicle accessing an active construction zone. NOTAMS will be issued by the COUNTY and notice will be given to based aircraft operators about the pending closures and construction schedule.

2. Instrument Operations.

No impact will be made on operations during construction since the runway and associated taxiways will be closed to traffic.

3. Construction Safety and Phasing Plan (CSPP). A preliminary Construction Safety and Phasing Plan (CSPP) has been prepared and will be submitted along with this EDR to the County for review. Upon completion of the County's review, the CSPP will be forwarded to the FAA for acceptance prior to advertising for competitive bids.**V. ENGINEER'S COST ESTIMATE**

The Palo Alto Airport Pavement Rehabilitation Project is one of three Santa Clara County airport projects of similar design that the FAA has allowed to be bid together. Each airport has a separate bid schedule. Bid Schedule 1 is the Palo Alto Airport Project and the estimated construction cost for the Bid Schedule 1 Base Bid is \$399,025.

A detailed breakdown of construction costs for the Project is presented in *Appendix B*.

VI. CONSTRUCTION PHASING AND TIME OF COMPLETION

This Project will be completed in two distinct elements: Mobilization and Construction. The overall time for completion of this Project is eighteen (18) working days.

A. MOBILIZATION Notice to Proceed with Mobilization will be given immediately after the award of the Contract. All work included in the Mobilization phase shall be completed within ten (10) working days. During this phase of the Project, no work shall be conducted that in any way restricts Airport operations. Mobilization work shall include, but not be limited to, the following:

1. Processing of required submittals, including the Contractor's work schedule.

2. Preparation and submission of the Safety Plan Compliance Document (a sample document is included in the CSPP).
3. All prequalification testing, review, and approval.
4. Mix design preparation, review, and approval.
5. Airfield Safety Devices delivered to site (construction flags, runway closed markers, Airport radios).
6. All miscellaneous Mobilization efforts required to commence construction.
7. Materials and equipment delivered to site, as applicable.

It is the County's intent that all preliminary work required to pursue construction to completion be finalized during the Mobilization phase to minimize delays during construction.

B. CONSTRUCTION

Notice to Proceed with the Construction phase shall be issued at the County's discretion after the start of Mobilization. All work included in the Construction phase shall be completed within eight (8) working days. The work included in this Project is all contained on the Airport. There are mandatory requirements and associated liquidated damages, if the specified timeframes are not met.

The following Phases are established for this Project:

1. **Construction Phase 1:** Phase 1 work will be performed during four (4) night only closures. The COUNTY will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from 6 pm until 7 am for four (4) consecutive nights and direct the Contractor when to place and remove lighted runway closure markers on each runway designation. The work in Construction Phase 1 includes the following:
 - Cleaning pavement surface.
 - Cleaning cracks and treating with herbicide, and filling cracks.
 - Repairing sections of pavement by grinding and paving.
 - Removal of rubber build-up and pavement markings to 60% removal.
2. **Construction Phase 2:** Phase 2 work will be performed during three (3) days of full-time closure beginning on a Monday at 6 pm through Friday at 7 am. The COUNTY will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from Monday 6 pm until Friday 7 am for three (3) days and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction Phase 2 includes the following:
 - Removal of remaining markings where 50% is required.
 - Rehabilitation of runway and taxiway pavements with the microsurfacing process.
 - Application of first coat of paint for pavement markings.

3. **Construction Phase 3.** Phase 3 work shall be performed during one (1) night closure (after 30 days of cure time), at which time the final application of pavement markings shall be applied. The COUNTY will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from 6 pm until 7 am for one night and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction phase 3 includes the following:

- Final application of pavement markings.

C. LIQUIDATED DAMAGES.

If the overall time limitation for any element of work not be met, liquidated damages of \$2,500 per calendar day will be assessed. Additionally, if the runway is not reopened at the specified date and time allowed for closure period(s), liquidated damages of \$1,000 per hour will be assessed for each hour or portion of hour until the runway is opened to aircraft.

VII. CONTRACT DOCUMENTS

A. BID DOCUMENTS AND GENERAL PROVISIONS

Bid Documents and General Provisions for this Project are adapted from County of Santa Clara Standard Specifications, modified to reflect FAA standards and specific Project requirements. The FAA-required "Standard Federal Contract Clauses and Requirements for Construction Contracts" are included as Division II of the Project Specifications.

B. TECHNICAL SPECIFICATIONS

Technical Specifications for this Project are all adapted and modified from FAA Standard Specifications. Where references to a Standard Specification are made, the standards are referred to for information purposes only. All of the information the Contractor needs to complete the work is contained within the Project Plans and Specifications.

VIII. CONSTRUCTION INSPECTION AND TESTING

Construction inspection will be provided by the County of Santa Clara. Quality Control and Quality Assurance testing will be provided by the Contractor. The COUNTY shall review the test results for compliance with the Contract Documents.

IX. SPONSOR CERTIFICATION FOR PROJECT PLANS AND SPECIFICATIONS

A copy of the Sponsor Certification for Project Plans and Specifications is included as *Appendix C*.

Respectfully submitted by,

MEAD & HUNT, Inc.

Robert A. Casagrande
Project Manager

Jeffrey T. Leonard, P.E.
Project Engineer

Appendices: *Appendix A* *Plan Sheets*
 Appendix B *Engineer's Cost Estimate*
 Appendix C *Sponsor Certification for Project Plans and Specifications*

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Appendix A

90% Palo Alto Airport Plan Sheets





These documents shall not be used for any purpose or project for which it is not intended. Mead & Hunt shall be indemnified by the client and shall be liable for all claims, damages, losses, costs, and expenses, including attorney's fees and costs, arising out of such misuse or reuse of the documents. In addition, unauthorized reproduction of these documents, in part or as a whole, is prohibited.

PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA

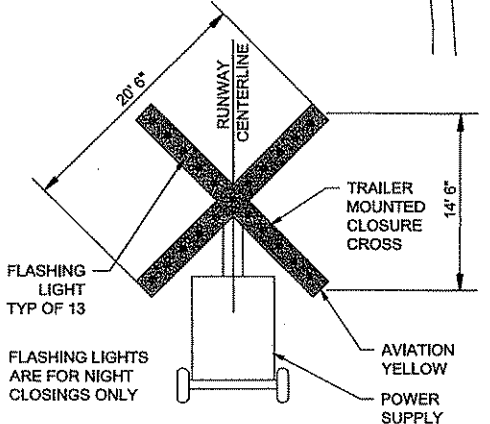
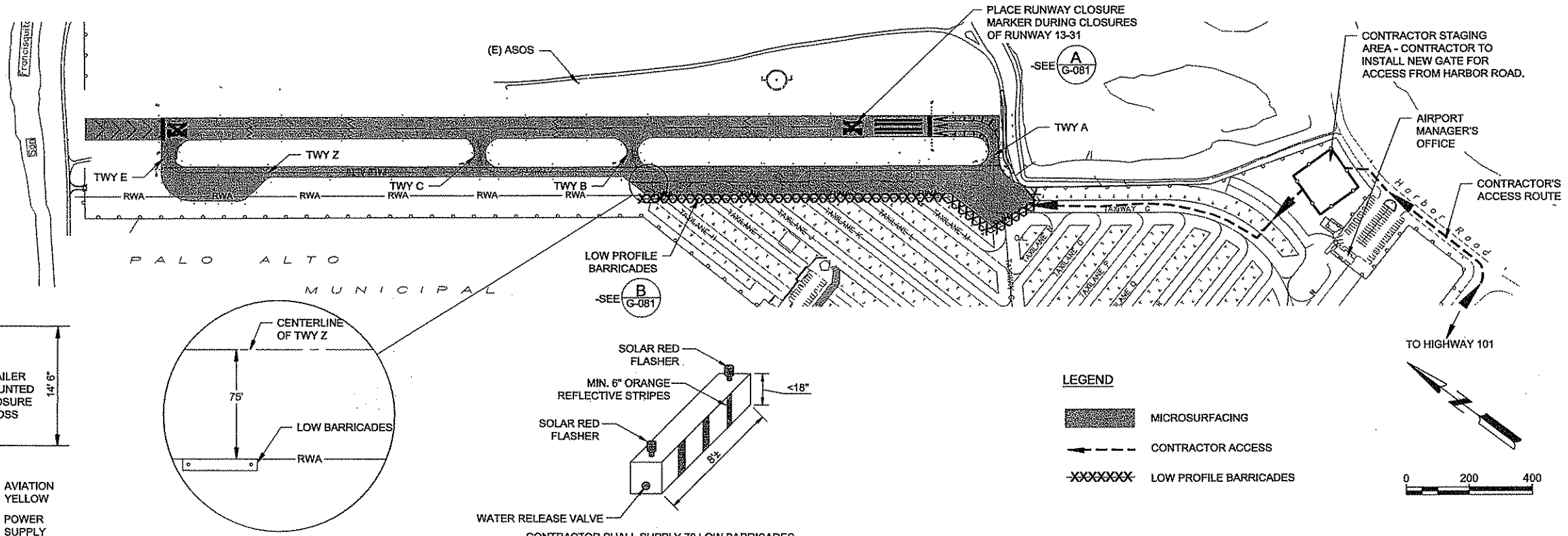
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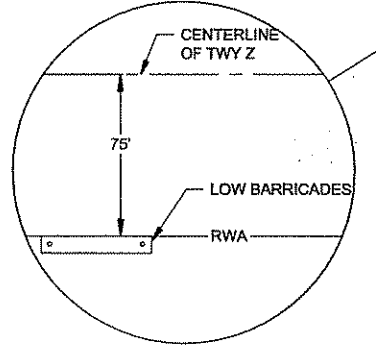
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| APP | 3-06-0182-08 |
| MAP NO. | R4058100-115099.01 |
| DATE | MAY 2012 |
| DESIGNED BY | RAC |
| DRAWN BY | BE |
| CHECKED BY | LBM |
| DO NOT SCALE DRAWINGS | |

SHEET CONTENTS
SAFETY AND PHASING PLAN



A **RUNWAY CLOSURE MARKER DETAIL**
 NO SCALE



B **LOW PROFILE BARRICADE DETAIL**
 NO SCALE

NOTE:
 TEMPORARY CLOSED RUNWAY MARKERS SUPPLIED BY THE CONTRACTOR SHALL BE INSTALLED AND REMOVED BY THE CONTRACTOR DURING CONSTRUCTION PER THE SCHEDULE APPROVED BY THE ENGINEER. SEE CONSTRUCTION SAFETY AND PHASING PLANS FOR DETAILS.

CONTRACTOR SHALL SUPPLY 70 LOW BARRICADES WHICH SHALL BE PLACED 10 FEET O.C. WITH A DELINEATOR PLACED BETWEEN AND SHALL BE TURNED OVER TO AIRPORT AT END OF PROJECT IN GOOD WORKING CONDITION. ADDITIONAL DELINEATORS CAN BE PLACED BETWEEN BARRICADES AS NEEDED.

LEGEND

| | |
|--|------------------------|
| | MICROSURFACING |
| | CONTRACTOR ACCESS |
| | LOW PROFILE BARRICADES |

Phasing and Time Limitations

The Project has been divided into two distinct phases: Mobilization; and Construction. Construction will be divided into three subphases to minimize impact to operations at the Airport and define the sequence of the work associated with the pavement rehabilitation. A separate Notice to Proceed shall be issued for the Mobilization phase and the Construction phase. The work efforts and affected airfield areas within the AOA are detailed below and all work will be coordinated with the ATO. The overall time of completion for the Project is eighteen (18) working days; Mobilization shall be completed within ten (10) working days; Construction (all phases) shall be completed within eight (8) working days as detailed below.

- Mobilization Phase (Ten (10) working days).** Notice to Proceed with Mobilization will be issued immediately after award of Contract. During this phase of the Project, no work shall be conducted that in any way restricts Airport operations. Mobilization work shall include, but not be limited to, the following:
 - Processing of required submittals, including the Contractor's work schedule.
 - Preparation and submission of the SFCD.
 - All prequalification testing, review, and approval.
 - Mix design preparation, review, and approval.
 - Airfield Safety Devices delivered to site (construction flags, runway closure markers, Airport radios).
 - All miscellaneous Mobilization efforts required to commence construction.
 - Materials and equipment delivered to site, as applicable.

All preliminary work required to pursue construction to completion shall be finalized during the Mobilization phase to minimize delays during construction.

- Construction Phase (Eight (8) working days).** Notice to Proceed with Construction will be issued at the County's discretion at any time after the start of Mobilization. The Construction Phase consists of three subphases as described below.
 - Construction Phase 1.** Phase 1 work will be performed during four (4) night only closures. The County will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from 6 pm until 7 am for four (4) consecutive nights and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction Phase 1 includes the following:
 - Cleaning pavement surface.
 - Cleaning cracks, treating cracks with herbicide, and filling cracks.
 - Repairing sections of pavement by grinding and paving.
 - Removal of rubber bulk-up and pavement markings to 60%.

Construction Phase 1 Summary.

- Scope of Work - Preparation of runway and taxiway asphalt pavements for application of asphalt microsurfacing rehabilitation.
- Area closed to aircraft operations - Runway 13-31, Taxiways A, B, C, E, and Z.
- Duration of closure - Four (4) nights from 6 pm to 7 am.

Construction Phase 2 Summary.

- Scope of Work - Application of asphalt microsurfacing material to rehabilitate the pavement and final application of pavement markings.
- Area closed to aircraft operations - Runway 13-31, Taxiways A, B, C, E, and Z.
- Duration of closure - Three (3) working days
- Alternate taxi route - None, Airport will be closed.
- Emergency access routes - Community fire and rescue services enter through existing gate off of Harbor Road to the Airport apron. Construction operations will not hinder access to areas of Airport.
- Construction staging area - Contractor's staging area is off of Harbor Road on the north east end of the Airport. This area is off of Airport pavement and away from moving or parked aircraft.
- Construction access and haul route - access will be through the gate adjacent to the Airport Manager's Office on Harbor Road and the haul route will be along Taxilane G.

- Alternate taxi route - None - Airport will be closed.
- Emergency access routes - Community fire and rescue services enter through existing gate off of Harbor Road to the Airport apron. Construction operations will not hinder access to areas of Airport.
- Construction staging area - Contractor's staging area is off of Harbor Road on the north east end of the Airport. This area is off of Airport pavement and away from moving or parked aircraft.
- Construction access and haul route - Access will be through the gate adjacent to the Airport Manager's Office on Harbor Road and the haul route will be along Taxilane G.
- Impacts to NAVAIDs - None
- Lighting and marking changes - None.
- Required hazard marking and lighting - Low profile barricades and 42-inch high plastic delineators will be placed along the boundary of Taxiway Z and the adjacent apron prior to beginning construction. The boundary for the Restricted Work Area (RWA) limit is 75 feet from the centerline of Taxiway Z and is greater than the Taxiway Object Free Area of a BI taxiway. This offset is required due to the limits of construction associated with this Project. This line of barricades will block entry into any work area on the Airport for based aircraft. All airfield lights, navigational and visual aids will be decommissioned each night during construction hours.
- Lead times for required notification - five (5) working days

Construction Phase 2. Phase 2 work will be performed during three (3) days of full-time closure beginning on a Monday at 6 pm through Friday at 7 am. The County will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from Monday 6 pm until Friday 7 am for three (3) days and direct the Contractor when to place and remove runway closure markers on each runway designation marker. The work in Construction Phase 2 includes the following:

- Removal of remaining pavement markings on Runway.
- Rehabilitation of runway and taxiway pavements with the microsurfacing process.
- Application of first coat of paint for pavement markings.

Construction Phase 3 Summary.

- Scope of Work - Final application of pavement markings.
- Area closed to aircraft operations - Runway 13-31, Taxiways A, B, C, E, and Z.
- Duration of closure - One (1) night
- Alternate taxi route - None, Airport will be closed.
- Emergency access routes - Community fire and rescue services enter through existing gate off of Harbor Road to the Airport apron. Construction operations will not hinder access to areas of Airport.
- Construction staging area - Contractor's staging area is off of Harbor Road on the north east end of the Airport. This area is off of Airport pavement and away from moving or parked aircraft.
- Construction access and haul route - access will be through the gate adjacent to the Airport Manager's Office on Harbor Road and the haul route will be along Taxilane G.
- Impacts to NAVAIDs - None
- Lighting and marking changes - None.
- Required hazard marking and lighting - Low profile barricades and 42-inch high plastic delineators will be placed along the boundary of Taxiway Z and the adjacent apron prior to beginning construction. The boundary for the Restricted Work Area (RWA) limit is 75 feet from the centerline of Taxiway Z and is greater than the Taxiway Object Free Area of a BI taxiway. This offset is required due to the limits of construction associated with this Project. This line of barricades will block entry into any work area on the Airport for based aircraft. All airfield lights, navigational and visual aids will be decommissioned each night during construction hours.
- Lead times for required notification - five (5) working days

- Impacts to NAVAIDs - None
- Lighting and marking changes - None.
- Required hazard marking and lighting - Low profile barricades and 42-inch high plastic delineators will be placed along the boundary of Taxiway Z and the adjacent apron prior to beginning construction. The boundary for the Restricted Work Area (RWA) limit is 75 feet from the centerline of Taxiway Z and is greater than the Taxiway Object Free Area of a BI taxiway. This offset is required due to the limits of construction associated with this Project. This line of barricades will block entry into any work area on the Airport for based aircraft. All airfield lights, navigational and visual aids will be decommissioned each night during construction hours.
- Lead times for required notification - five (5) working days

Construction Phase 3. Phase 3 work shall be performed during one (1) night closure (after 30 days of cure time), at which time the final application of pavement markings shall be applied. The County will issue a NOTAM that Runway 13-31 and Taxiways A, B, C, E, and Z will be closed from 6 pm until 7 am for one night and direct the Contractor when to place and remove runway closure markers on each runway designation marker. The work in Construction phase 3 includes the following:

- Final application of pavement markings.

Construction Phase 3 Summary.

- Scope of Work - Final application of pavement markings.
- Area closed to aircraft operations - Runway 13-31, Taxiways A, B, C, E, and Z.
- Duration of closure - One (1) night
- Alternate taxi route - None, Airport will be closed.
- Emergency access routes - Community fire and rescue services enter through existing gate off of Harbor Road to the Airport apron. Construction operations will not hinder access to areas of Airport.
- Construction staging area - Contractor's staging area is off of Harbor Road on the north east end of the Airport. This area is off of Airport pavement and away from moving or parked aircraft.
- Construction access and haul route - access will be through the gate adjacent to the Airport Manager's Office on Harbor Road and the haul route will be along Taxilane G.
- Impacts to NAVAIDs - None
- Lighting and marking changes - None.
- Required hazard marking and lighting - Low profile barricades and 42-inch high plastic delineators will be placed along the boundary of Taxiway Z and the adjacent apron prior to beginning construction. The boundary for the Restricted Work Area (RWA) limit is 75 feet from the centerline of Taxiway Z and is greater than the Taxiway Object Free Area of a BI taxiway. This offset is required due to the limits of construction associated with this Project. This line of barricades will block entry into any work area on the Airport for based aircraft. All airfield lights, navigational and visual aids will be decommissioned each night during construction hours.
- Lead times for required notification - five (5) working days

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SAN FRANCISCO BAY

SCENIC RECREATIONAL AREA

WETLANDS

City of Palo Alto Property

RUNWAY 13-31 PAVEMENT REHABILITATION SEE SHEETS: C-201 TO C-204

PAVEMENT REPAIR (TYPICAL 4 PLACES) -SEE A C-201

(E) PAPI TO BE PROTECTED IN PLACE

(E) PAPI TO BE PROTECTED IN PLACE

TAXIWAY PAVEMENT REHABILITATION SEE SHEETS: C-201 TO C-204

ASPHALT PATCH SEE SHEETS: C-202

PALO ALTO MUNICIPAL GOLF COURSE

DEMOLISH 12 TIE-DOWN SETS AND REMOVE TIE-DOWN MARKINGS SEE SHEETS: C-203 TO C-204

POND

CONTRACTOR STAGING AREA

(E) AIRPORT FENCE

CONTRACTOR TO INSTALL GATE (6' HIGH CHAIN LINK) AS NECESSARY TO ACCOMMODATE CONSTRUCTION INGRESS AND EGRESS.

AIRPORT MANAGER'S OFFICE

WASTEWATER/TREATMENT PLANT

CONTRACTOR'S ACCESS ROUTE




GOLF COURSE PARKING

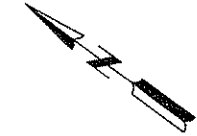
TO HIGHWAY 101

GENERAL NOTES:

1. WORK WITHIN THE RESTRICTED WORK AREA (RWA) REQUIRES CLOSURE OF THE RUNWAY AND TAXIWAY. SEE PROJECT SPECIFICATIONS, SPECIAL PROVISIONS SECTION 106 "FEDERAL REQUIREMENTS" AND SHEET G-081 FOR ADDITIONAL INFORMATION ON CLOSURE REQUIREMENTS, LIMITATIONS ON CONSTRUCTION, AND ACCESS TO THE VARIOUS WORK AREAS.
2. CONTRACTOR'S ACCESS TO THE AIRPORT SHALL BE VIA THE ROUTES SHOWN UNLESS OTHERWISE APPROVED BY THE COUNTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING DAMAGE TO PAVEMENTS OR OTHER EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITIES. CONSTRUCTION TRAFFIC IS LIMITED TO APPROVED HAUL AND ACCESS ROUTES. HAUL ROUTES ON PAVEMENTS SHALL BE CLEANED EACH DAY AND DUST CONTROL SHALL BE MAINTAINED BY THE CONTRACTOR, USING THE METHODS OUTLINED IN CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).

LEGEND

-  PROPOSED ASPHALT MICROSURFACING
-  CONTRACTOR ACCESS
-  TIE-DOWN DEMOLITION (TYPICAL FOR 12 SETS, 3 PER SET TOTAL)



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REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA

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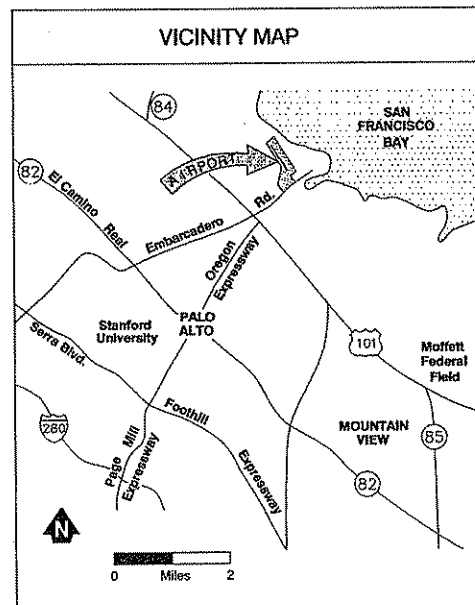
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SHEET CONTENTS
 PROJECT LAYOUT

SHEET NO. 2 of 12

G-021



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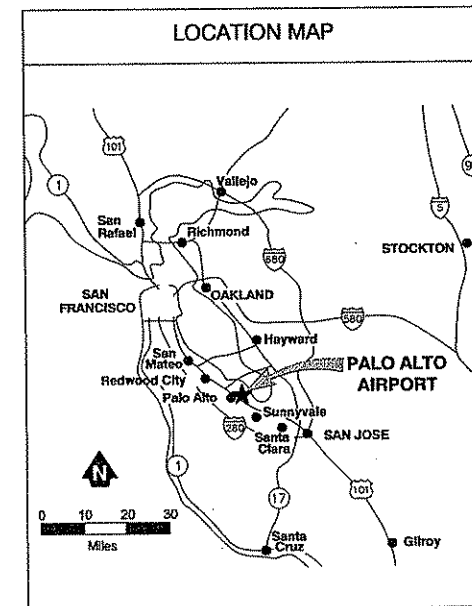
PALO ALTO AIRPORT

REHABILITATION OF RUNWAY 13-31 & TAXIWAYS

BID SCHEDULE 1

AIP 3-06-0182-08

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PROJECT DESCRIPTION

- REHABILITATION OF RUNWAY 13-31 & TAXIWAYS A, B, C, E, & Z
- PAVEMENT MARKINGS

Sheet List Table

| Sheet Number | Sheet Title |
|--------------|------------------------------|
| G-001 | COVER SHEET |
| G-021 | PROJECT LAYOUT |
| G-081 | SAFETY AND PHASING PLAN |
| C-201 | PAVEMENT MICROSURFACING PLAN |
| C-202 | PAVEMENT MICROSURFACING PLAN |
| C-203 | PAVEMENT MICROSURFACING PLAN |
| C-204 | PAVEMENT MICROSURFACING PLAN |
| C-651 | MARKING PLAN |
| C-652 | MARKING PLAN |
| C-653 | MARKING PLAN |
| C-654 | MARKING PLAN |
| C-671 | MARKING DETAILS |

DRAWING LEGEND

| | EXISTING | PROPOSED | | EXISTING | PROPOSED |
|-----------------------------|----------|----------|-------------------------------|----------|----------|
| AC PAVEMENT | | N/A | ELECTRIC (UNDERGROUND) | | N/A |
| SUBGRADE-SECTION | N/A | N/A | PULL BOX | | N/A |
| PCC PAVEMENT | N/A | N/A | GUIDANCE SIGN | | N/A |
| PAVEMENT REMOVAL | N/A | | RUNWAY EDGE LIGHT | * | N/A |
| AGGREGATE BASE | N/A | | THRESHOLD LIGHT | ⊙ | N/A |
| PAVEMENT MICROSURFACING | N/A | | PAPI | | N/A |
| SHOULDER BACKING/PLAN | N/A | N/A | WATER | | N/A |
| SUBGRADE-SECTION | N/A | | WATER VALVE | | N/A |
| ELEVATION | N/A | N/A | FIRE HYDRANT | | N/A |
| BENCHMARK/MONUMENT | N/A | N/A | SANITARY SEWER | | N/A |
| TOPOGRAPHIC CONTOUR | N/A | N/A | STORM DRAIN | | N/A |
| APPROXIMATE DAYLIGHT | N/A | N/A | CATCH BASIN | | N/A |
| PROPERTY | | N/A | FLOW LINE | | N/A |
| FENCE | | N/A | TELEPHONE | | N/A |
| GATE | | N/A | TAXIWAY EDGE LIGHT ELEVATED | • | N/A |
| CONSTRUCTION SAFETY FENCE | N/A | | TAXIWAY EDGE LIGHT SEMI-FLUSH | | N/A |
| LOW BARRICADES & DELINEATOR | N/A | | | | |

ABBREVIATIONS

| | | | | | |
|----------|---------------------------------|-------|-----------------------------------|------|-----------------------------|
| AB | AGGREGATE BASE | GALV | GALVANIZED | REQ | REQUIRED |
| AC | ASPHALT CONCRETE | GB | GRADE BREAK | RWA | RUNWAY WORK RESTRICTED AREA |
| ASB | AGGREGATE SUB-BASE | GFCI | GROUND FAULT CIRCUIT INTERRUPTER | RWY | RUNWAY |
| AWG | AMERICAN WIRE GAUGE | GS | GROUND SHOT | SD | STORM DRAIN |
| BC | BEGINNING OF CURVE | HDPE | HIGH DENSITY POLYETHYLENE | SDMH | STORM DRAIN MANHOLE |
| BLDG | BUILDING | HORIZ | HORIZONTAL | SG | STRAIGHT GRADE |
| BM | BENCHMARK | HP | HIGH POINT | SH | SHOULDER |
| BVC | BEGIN VERTICAL CURVE | IE | INVERT ELEVATION | SS | SANITARY SEWER |
| CL | CENTERLINE | L | LENGTH | SSMH | SANITARY SEWER MANHOLE |
| CLF | CHAIN LINK FENCE | LF | LINEAL FEET | STA | STATION |
| CB | CATCH BASIN | MAX | MAXIMUM | STD | STANDARD |
| CMP | CORRUGATED METAL PIPE | MID | MID POINT | T | TELEPHONE LINE |
| DB | DIRECT BURIAL | MIN | MINIMUM | TC | TOP OF CURB |
| DIA or Ø | DIAMETER | MIRL | MEDIUM INTENSITY RUNWAY LIGHTING | TG | TOP OF GRATE |
| (E) | EXISTING | MITL | MEDIUM INTENSITY TAXIWAY LIGHTING | T/L | TAXILANE |
| E | ELECTRICAL LINE | (N) | NEW | TOE | TOE OF BANK |
| EC | END OF CURVE | NIS | NOT IN SERVICE | TOP | TOP OF BANK |
| EG | EXISTING GRADE (OR GROUND) | OC | ON CENTER | TWY | TAXIWAY |
| EL | ELEVATION | PB | PULLBOX | TYP | TYPICAL |
| EP | EDGE OF PAVEMENT | PC | POINT OF CURVATURE | UCN | UNLESS OTHERWISE NOTED |
| ETR | EXISTING TO REMAIN | PCC | PORTLAND CEMENT CONCRETE | USA | UNDERGROUND SERVICE ALERT |
| FAA | FEDERAL AVIATION ADMINISTRATION | PI | POINT OF INTERSECTION | VERT | VERTICAL |
| FBO | FIXED BASE OPERATOR | PT | POINT OF TANGENCY | VG | VALLEY GUTTER |
| FF | FINISH FLOOR | PVC | POLY-VINYL CHLORIDE | W | WATER LINE |
| FG | FINISH GRADE | PVI | POINT OF VERTICAL INTERSECTION | W/ | WITH |
| FH | FIRE HYDRANT | R | REMOVE | W/O | WITHOUT |
| FL | FLOW LINE | R&R | REMOVE & REPLACE | WV | WATER VALVE |
| G | GAS LINE | RC | RELATIVE COMPACTION | WWM | WELDED WIRE MESH |
| | | RCP | REINFORCED CONCRETE PIPE | | |

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 REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
 PALO ALTO, CALIFORNIA

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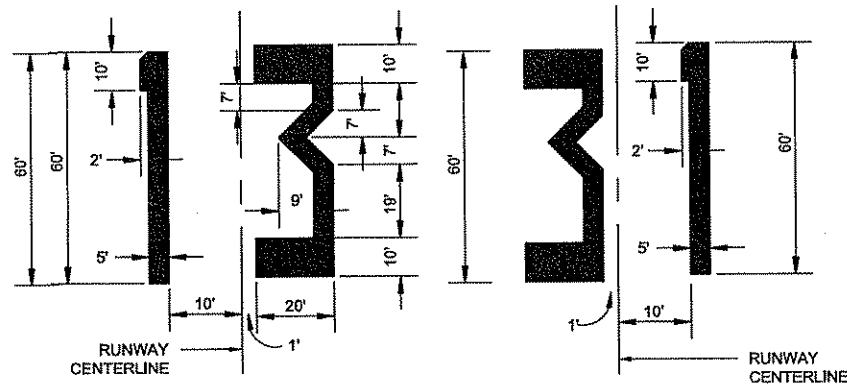
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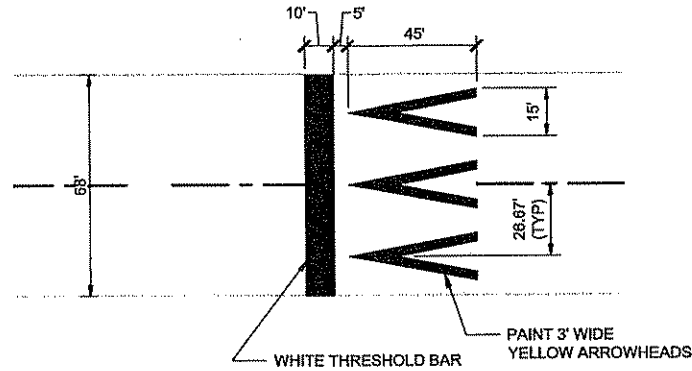
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SHEET NO. 1 of 12

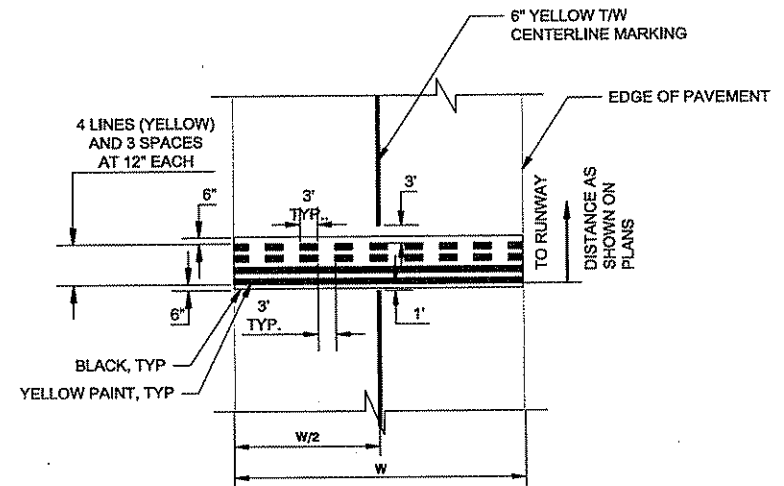
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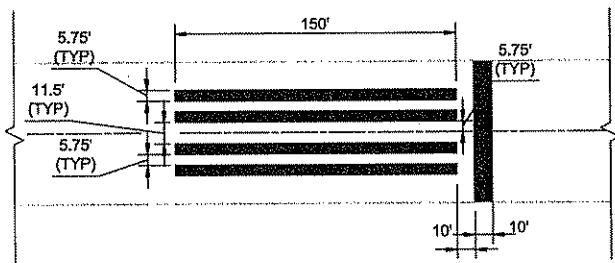
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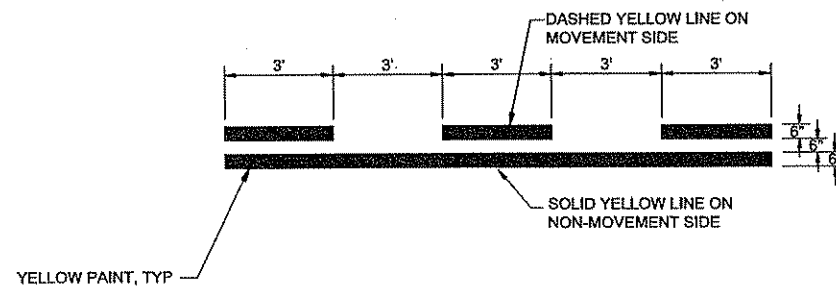
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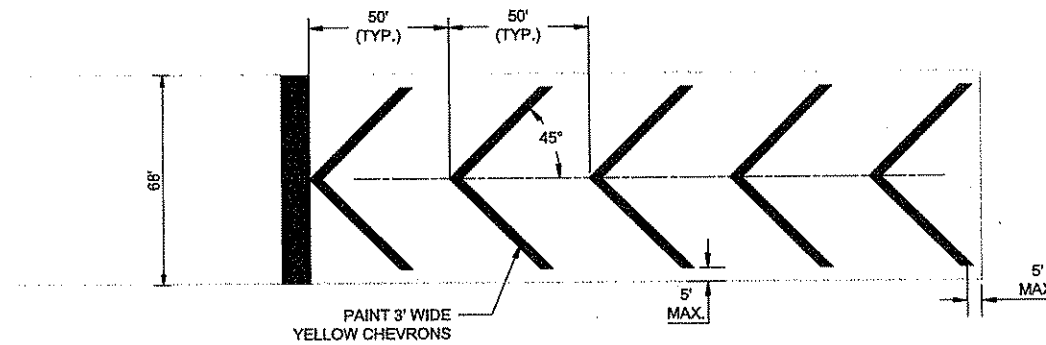
C TAXIWAY HOLD LINE DETAIL
NO SCALE M302 05/10



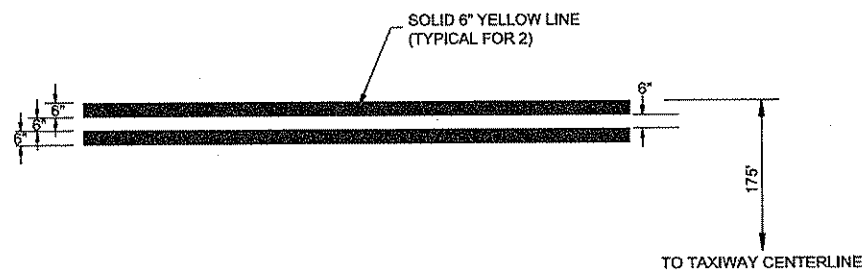
D THRESHOLD MARKING
NO SCALE M207A 05/10



E NON-MOVEMENT AREA MARKING
NO SCALE M306 04/10



F BLAST PAD MARKING
NO SCALE M305 04/10



G CONTINUOUS TAXIWAY EDGE MARKING
NO SCALE

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MARKING DETAILS

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C-671



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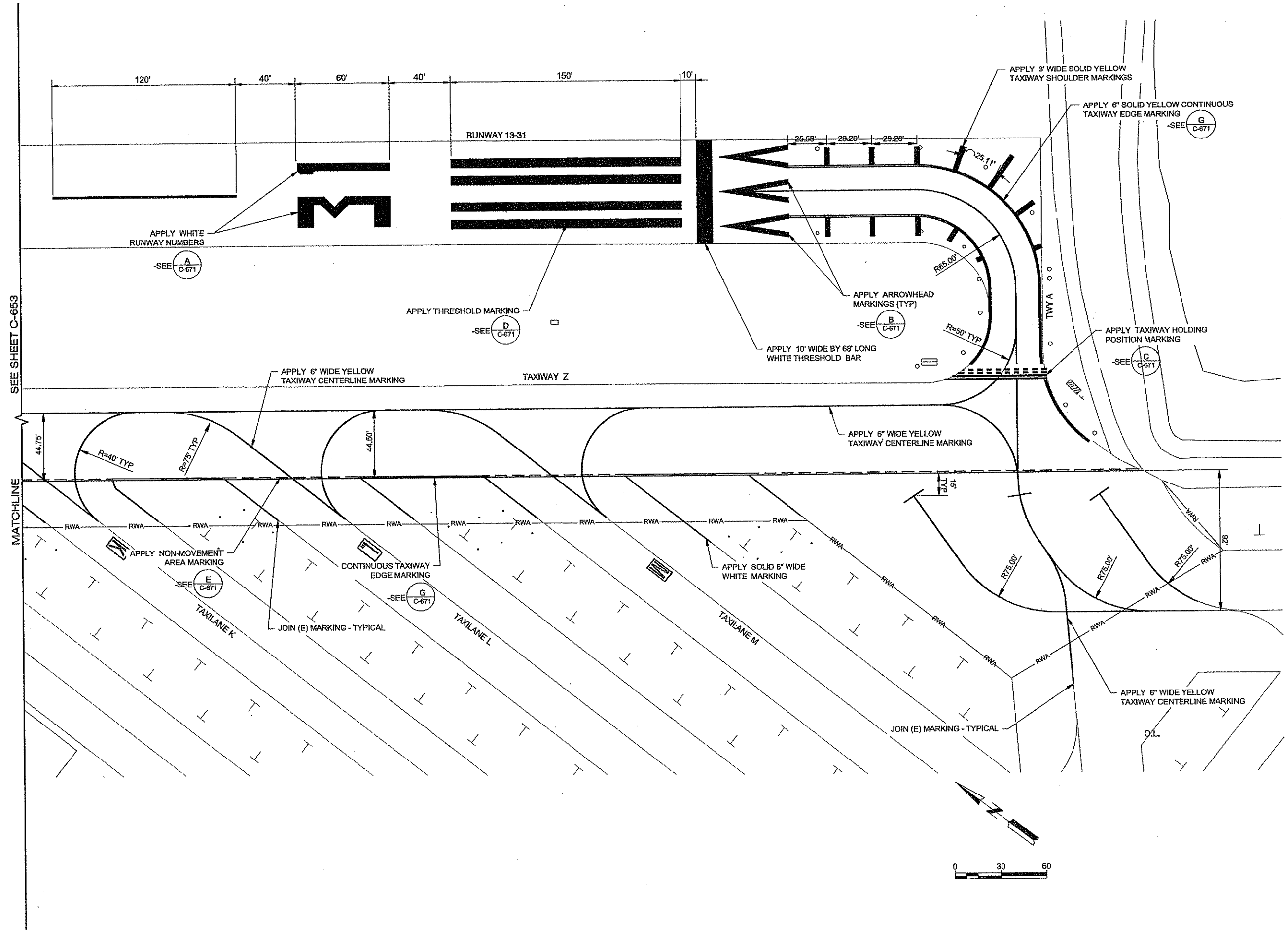
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SHEET CONTENT

MARKING PLAN

SHEET NO. 11 of 12

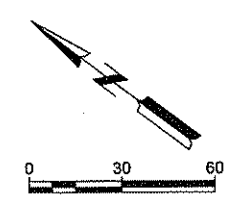
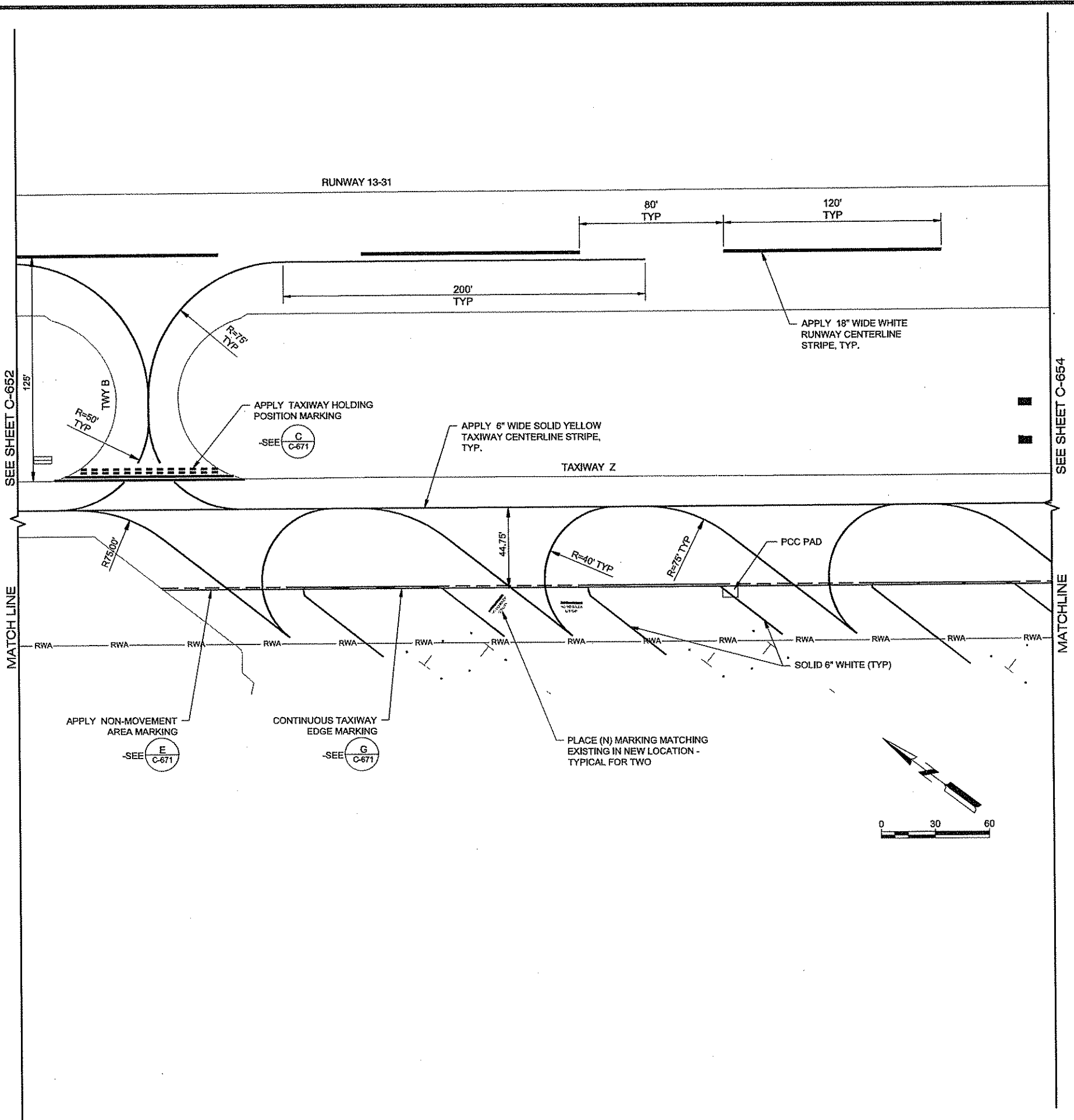
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SEE SHEET C-653

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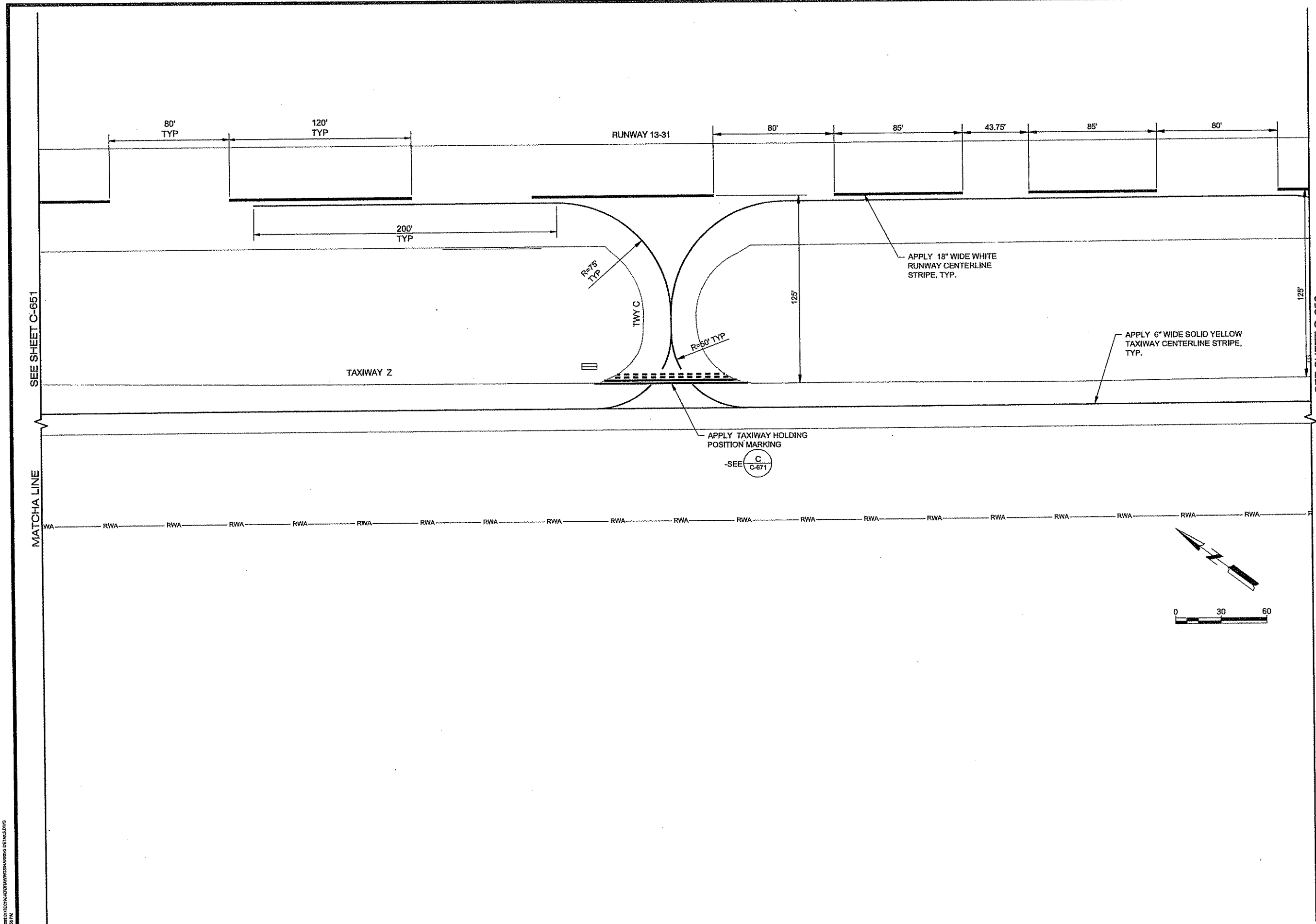
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MARKING PLAN

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PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA

| REVISION | |
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90% SUBMITTAL

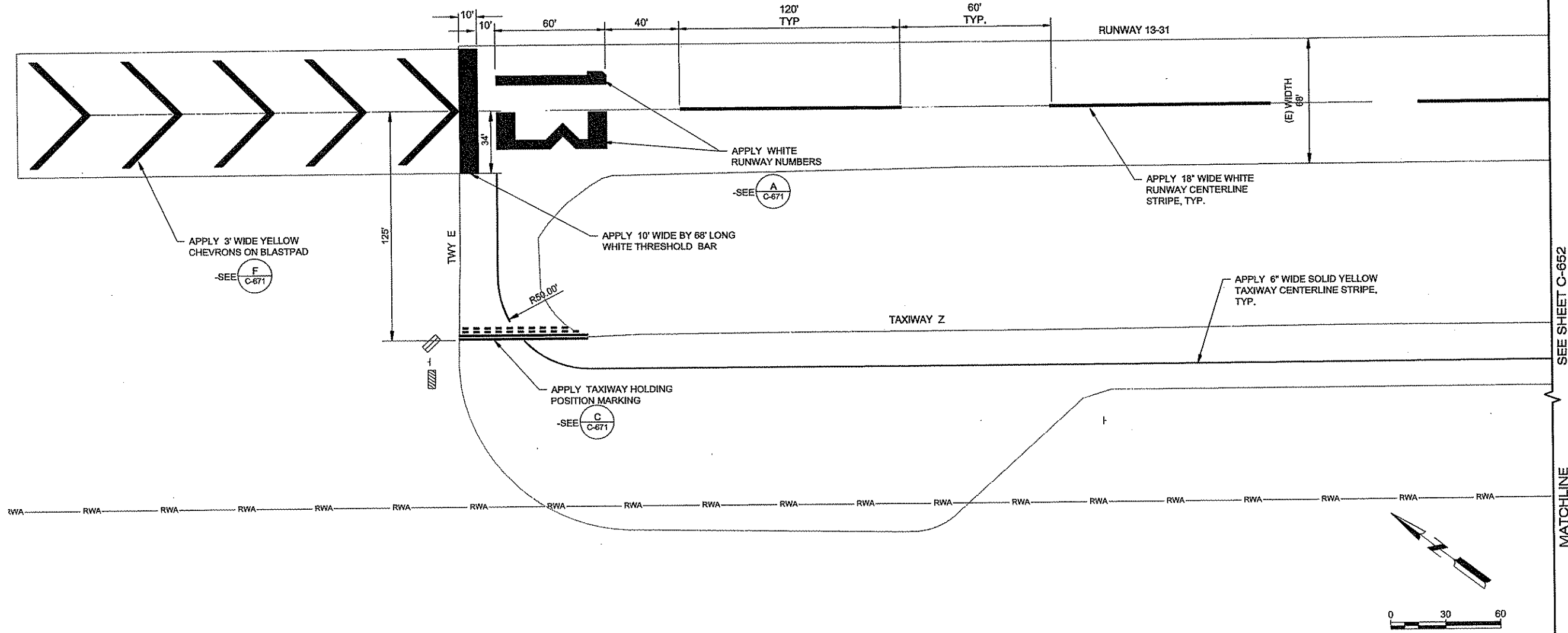
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| APP | 3-05-0182-08 |
| MAIN NO. | R4058100-115099.01 |
| DATE | MAY 2012 |
| DESIGNED BY | RAC |
| DRAWN BY | BE |
| CHECKED BY | LBM |

DO NOT SCALE DRAWINGS

SHEET CONTENTS
MARKING PLAN

SHEET NO. 8 of 12

C-651



APPLY 3' WIDE YELLOW
CHEVRONS ON BLASTPAD
-SEE (F)
C-671

APPLY WHITE
RUNWAY NUMBERS
-SEE (A)
C-671

APPLY 10' WIDE BY 68' LONG
WHITE THRESHOLD BAR

APPLY 18" WIDE WHITE
RUNWAY CENTERLINE
STRIPE, TYP.

APPLY 6" WIDE SOLID YELLOW
TAXIWAY CENTERLINE STRIPE,
TYP.

APPLY TAXIWAY HOLDING
POSITION MARKING
-SEE (C)
C-671

SEE SHEET C-652

MATCHLINE



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PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA

REVISION

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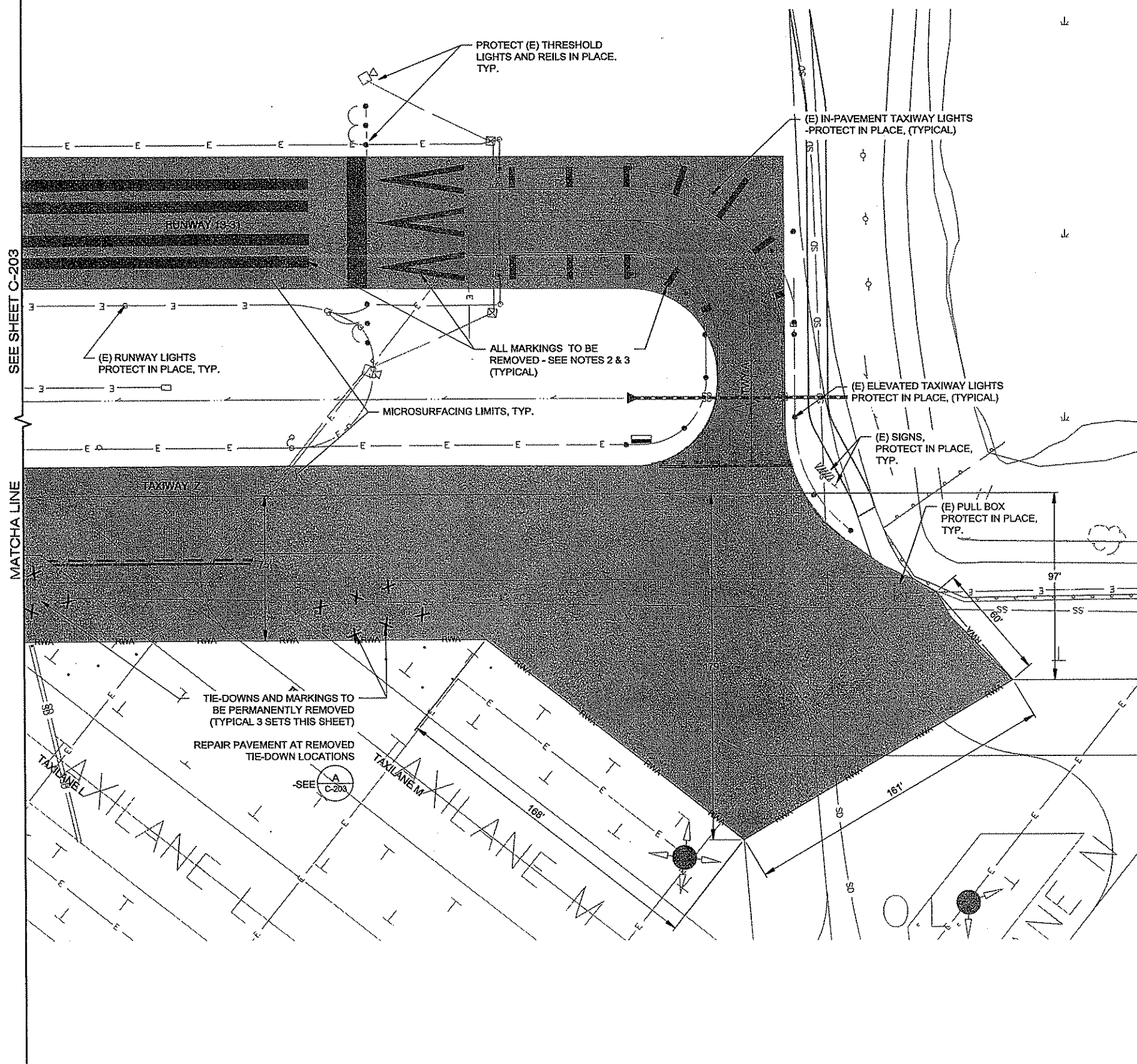
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AP: 3-06-0182-08
MAP NO.: R4058100-115099.01
DATE: MAY 2012
DESIGNED BY: RAC
DRAWN BY: BE
CHECKED BY: LBM
DO NOT SCALE DRAWINGS

SHEET CONTENTS
PAVEMENT
MICROSURFACING
PLAN

SHEET NO. 7 of 12

C-204



- NOTES:**
1. CRACK REPAIR OCCURS WITHIN MICROSURFACING AREA. ACTUAL LOCATIONS TO BE IDENTIFIED IN THE FIELD BY COUNTY.
 2. WHITE PAVEMENT MARKINGS TO BE REMOVED BY 95% UNLESS NOTED OTHERWISE.
 3. YELLOW AND BLACK PAVEMENT MARKINGS TO BE REMOVED BY 60% UNLESS NOTED OTHERWISE.

LEGEND

MICROSURFACING

SEE SHEET C-203

MATCHA LINE



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**PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA**

REVISION

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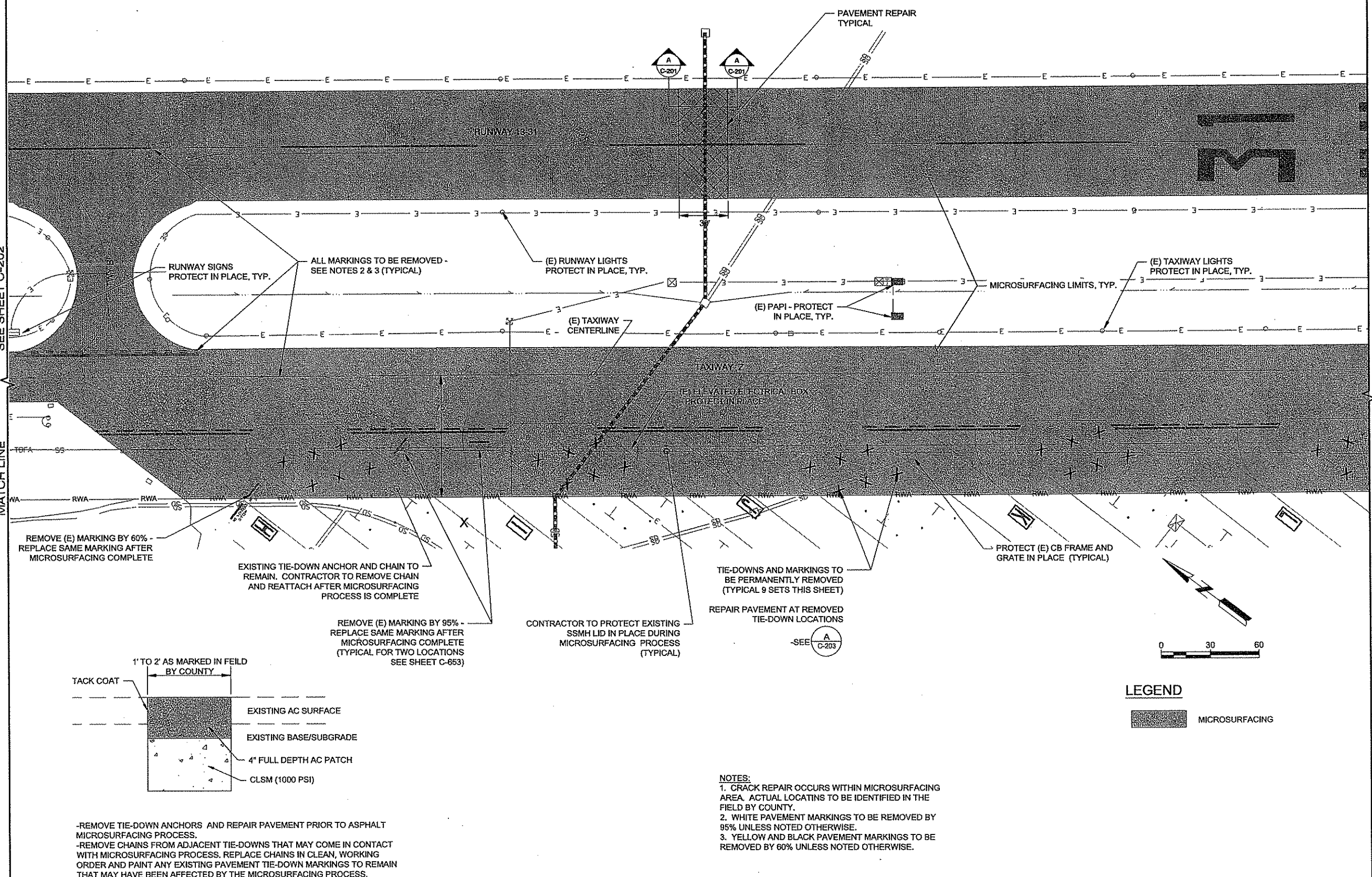
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| APP: | 3-06-0182-08 |
| MSH NO: | R4058100-115099.01 |
| DATE: | MAY 2012 |
| DESIGNED BY: | RAC |
| DRAWN BY: | BE |
| CHECKED BY: | LBM |
| DO NOT SCALE DRAWINGS | |

SHEET CONTENTS
PAVEMENT
MICROSURFACING
PLAN

SHEET NO. 6 of 12

C-203



SEE SHEET C-202

SEE SHEET C-204

MATCH LINE

MATCHLINE

REMOVE (E) MARKING BY 60% -
REPLACE SAME MARKING AFTER
MICROSURFACING COMPLETE

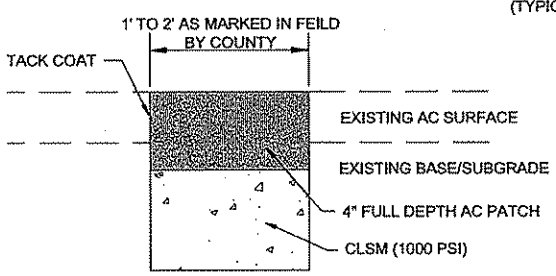
EXISTING TIE-DOWN ANCHOR AND CHAIN TO
REMAIN. CONTRACTOR TO REMOVE CHAIN
AND REATTACH AFTER MICROSURFACING
PROCESS IS COMPLETE

REMOVE (E) MARKING BY 95% -
REPLACE SAME MARKING AFTER
MICROSURFACING COMPLETE
(TYPICAL FOR TWO LOCATIONS
SEE SHEET C-653)

CONTRACTOR TO PROTECT EXISTING
SSMH LID IN PLACE DURING
MICROSURFACING PROCESS
(TYPICAL)

TIE-DOWNS AND MARKINGS TO
BE PERMANENTLY REMOVED
(TYPICAL 9 SETS THIS SHEET)
REPAIR PAVEMENT AT REMOVED
TIE-DOWN LOCATIONS

PROTECT (E) CB FRAME AND
GRATE IN PLACE (TYPICAL)



-REMOVE TIE-DOWN ANCHORS AND REPAIR PAVEMENT PRIOR TO ASPHALT
MICROSURFACING PROCESS.
-REMOVE CHAINS FROM ADJACENT TIE-DOWNS THAT MAY COME IN CONTACT
WITH MICROSURFACING PROCESS. REPLACE CHAINS IN CLEAN, WORKING
ORDER AND PAINT ANY EXISTING PAVEMENT TIE-DOWN MARKINGS TO REMAIN
THAT MAY HAVE BEEN AFFECTED BY THE MICROSURFACING PROCESS.

A PAVEMENT REPAIR AT TIE-DOWN DEMO
NO SCALE

- NOTES:**
1. CRACK REPAIR OCCURS WITHIN MICROSURFACING AREA. ACTUAL LOCATIONS TO BE IDENTIFIED IN THE FIELD BY COUNTY.
 2. WHITE PAVEMENT MARKINGS TO BE REMOVED BY 95% UNLESS NOTED OTHERWISE.
 3. YELLOW AND BLACK PAVEMENT MARKINGS TO BE REMOVED BY 60% UNLESS NOTED OTHERWISE.

LEGEND

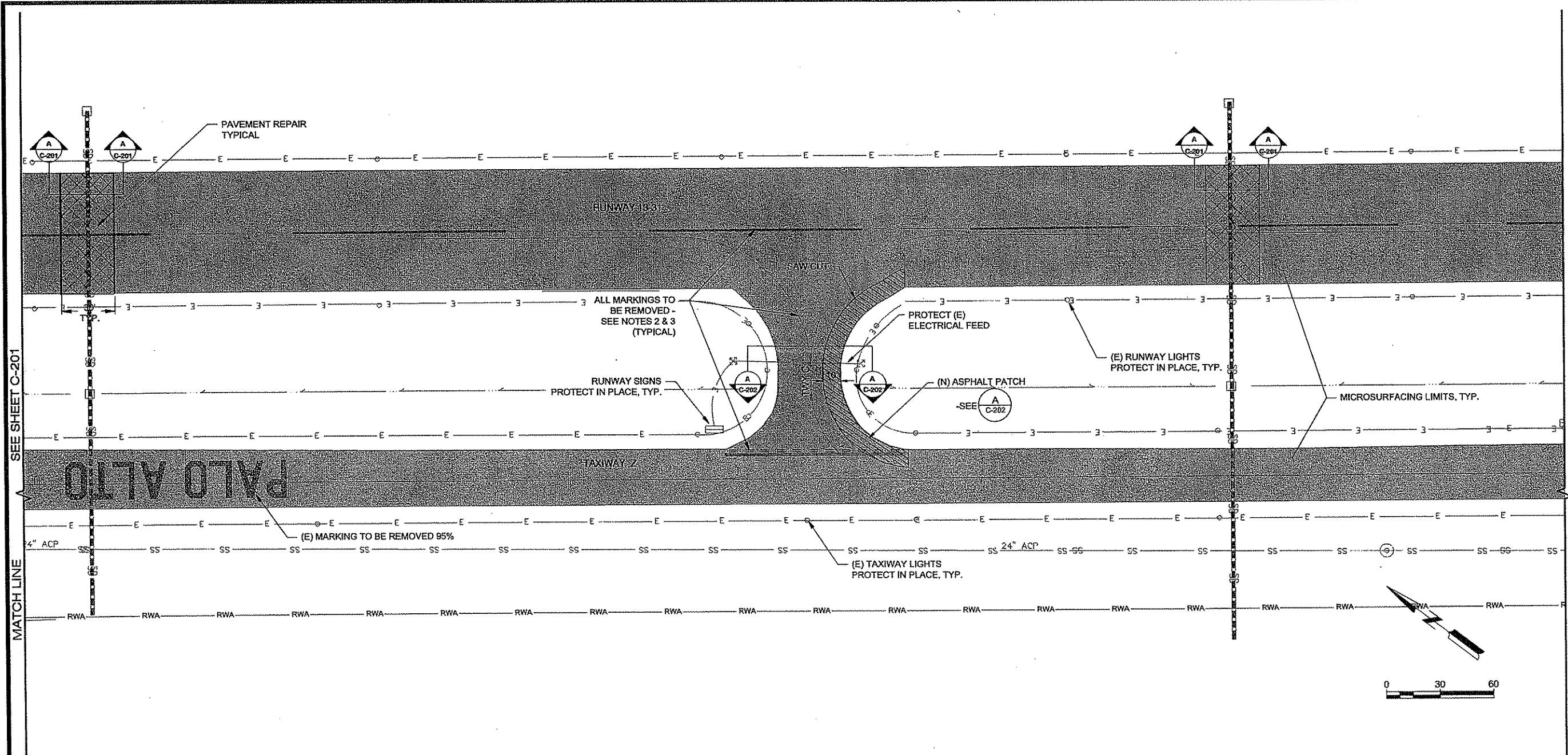


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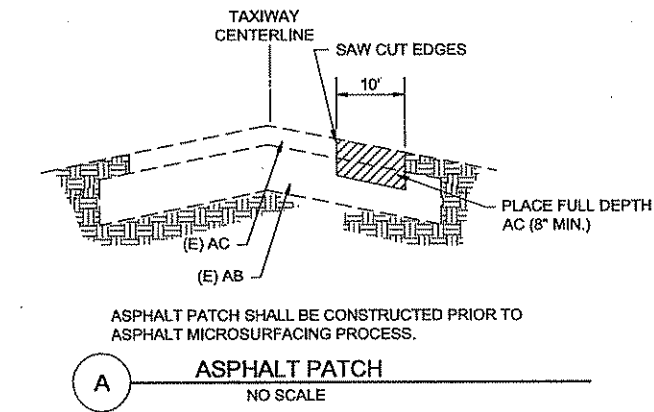
PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA



SEE SHEET C-201
MATCH LINE

SEE SHEET C-203
MATCHLINE

PALO ALTO



- NOTES:**
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 2. WHITE PAVEMENT MARKINGS TO BE REMOVED BY 95% UNLESS NOTED OTHERWISE.
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LEGEND



REVISION

90% SUBMITTAL

AP: 3-06-0182-08
ASH#NO: R4058100-115099.01
DATE: MAY 2012
DESIGNED BY: JTL
DRAWN BY: BE
CHECKED BY: LBM
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SHEET CONTENTS
PAVEMENT
MICROSURFACING
PLAN

SHEET NO. 5 of 12

C-202

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PALO ALTO AIRPORT
REHABILITATION OF RUNWAY 13-31 & TAXIWAYS
PALO ALTO, CALIFORNIA

REVISION

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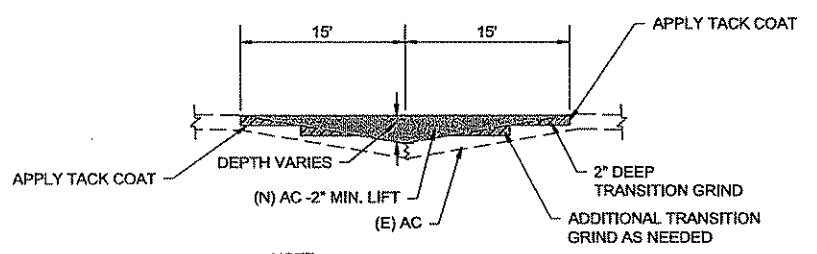
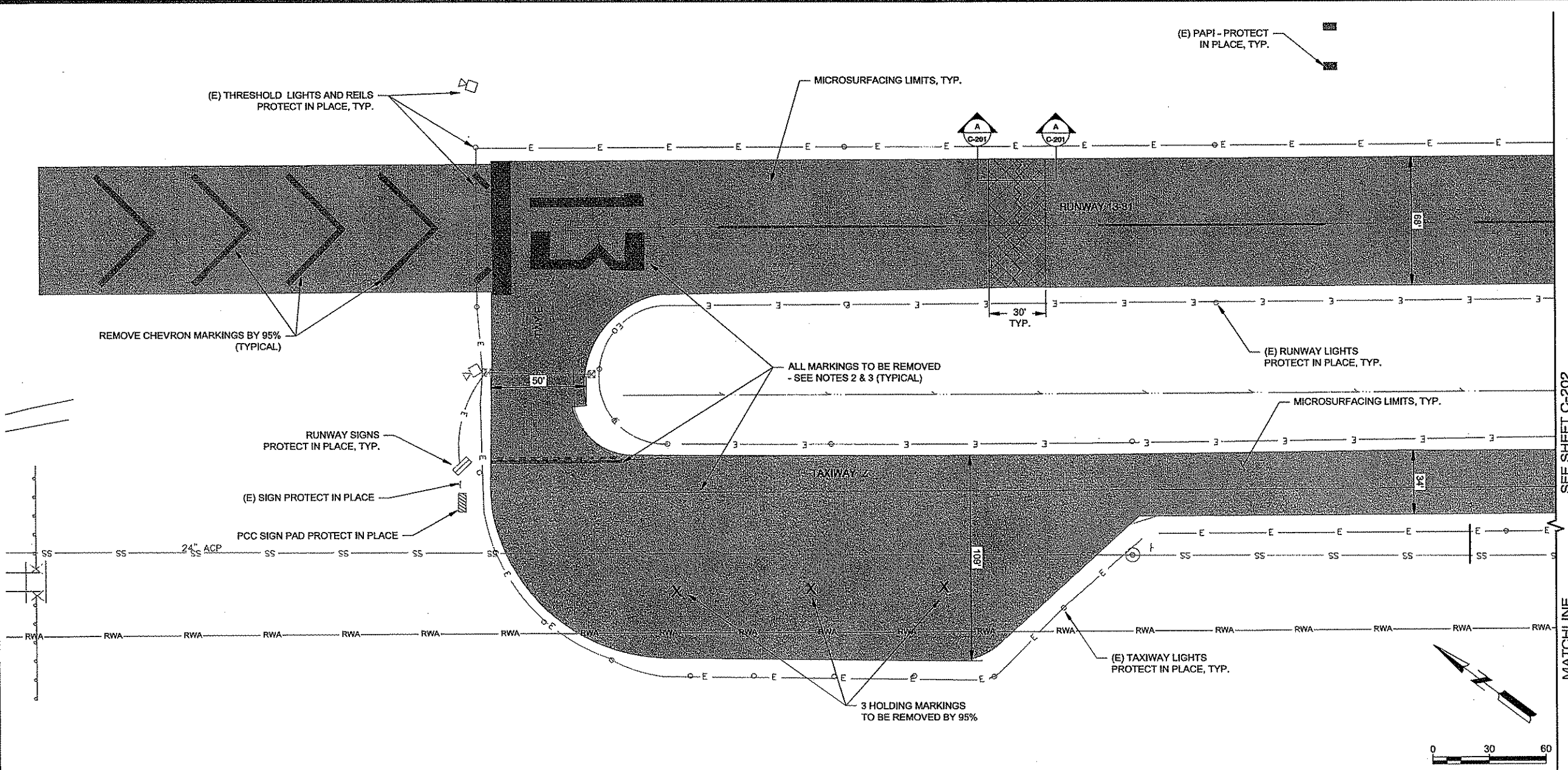
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AP: 3-06-0182-06
MAN NO: R4058100-115099.01
DATE: MAY 2012
DESIGNED BY: RAC
DRAWN BY: BE
CHECKED BY: LBM
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SHEET CONTENTS
PAVEMENT
MICROSURFACING
PLAN

SHEET NO. 4 of 12

C-201



A PAVEMENT REPAIR DETAIL
NO SCALE

- NOTES:**
1. CRACK REPAIR OCCURS WITHIN MICROSURFACING AREA. ACTUAL LOCATIONS TO BE IDENTIFIED IN THE FIELD BY COUNTY.
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LEGEND

MICROSURFACING

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Appendix B
Engineer's Construction Cost Estimate

ENGINEER'S CONSTRUCTION COST ESTIMATE

BID SCHEDULE 1

Palo Alto Airport

AIP NO. 3-06-0182-08

| | | ENGINEER'S ESTIMATE | | | |
|--------------------------|-------------------------------------|----------------------------|--------------------|--------------|----------------------|
| Item No. | Description | Unit | Estimated Quantity | Unit Cost | Extension |
| 1 | MOBILIZATION | LS | 1 | \$ 28,000.00 | \$ 28,000.00 |
| 2 | AIRFIELD SAFETY AND TRAFFIC CONTROL | LS | 1 | \$ 49,765.00 | \$ 49,765.00 |
| 3 | SURFACE PREPARATION - 95% | SY | 21,920 | \$ 2.10 | \$ 46,032.00 |
| 4 | SURFACE PREPARATION - 60% | SY | 23,670 | \$ 1.90 | \$ 44,973.00 |
| 5 | CRACK SEALING | LF | 1,500 | \$ 3.00 | \$ 4,500.00 |
| 6 | MICROSURFACING | SY | 45,590 | \$ 3.00 | \$ 136,770.00 |
| 7 | ASPHALT CONCRETE (SURFACE REPAIR) | TN | 290 | \$ 115.00 | \$ 33,350.00 |
| 8 | PAVEMENT MARKING WHITE | SF | 8,600 | \$ 2.10 | \$ 18,060.00 |
| 9 | PAVEMENT MARKING BLACK | SF | 3,000 | \$ 2.00 | \$ 6,000.00 |
| 10 | PAVEMENT MARKING YELLOW | SF | 12,900 | \$ 1.75 | \$ 22,575.00 |
| 11 | TIE-DOWN DEMO | EA | 36 | \$ 250.00 | \$ 9,000.00 |
| | | | | \$ - | \$ - |
| CONSTRUCTION COST | | | | \$ | \$ 399,025.00 |

Appendix C

Sponsor Certification for Project Plans and Specifications



U.S. DEPARTMENT OF TRANSPORTATION
 FEDERAL AVIATION ADMINISTRATION
 AIRPORT IMPROVEMENT PROGRAM
 SPONSOR CERTIFICATION
 PROJECT PLANS AND SPECIFICATIONS

County of Santa Clara
 (Sponsor)

Palo Alto Airport
 (Airport)

AIP 3-06-0182-08
 (Project Number)

Pavement Rehabilitation

(Project Description)

Title 49, United States Code, section 47105(d), authorizes the Secretary to require certification from the sponsor that it will comply with the statutory and administrative requirements in carrying out a project under the Airport Improvement Program (AIP). AIP standards are generally described in FAA Advisory Circular (AC) 150/5100-6, Labor Requirements for the Airport Improvement Program, AC 150/5100-15, Civil Rights Requirements for the Airport Improvement Program, and AC 150/5100-16, Airport Improvement Program Grant Assurance One--General Federal Requirements. A list of current advisory circulars with specific standards for design or construction of airports as well as procurement/installation of equipment and facilities is referenced in standard airport sponsor Grant Assurance 34 contained in the grant agreement.

Except for the certified items below marked not applicable (N/A), the list includes major requirements for this aspect of project implementation, although it is not comprehensive, nor does it relieve the sponsor from fully complying with all applicable statutory and administrative standards.

| | Yes | No | N/A |
|---|-------------------------------------|--------------------------|-------------------------------------|
| 1. The plans and specifications were (will be) prepared in accordance with applicable Federal standards and requirements, so no deviation or modification to standards set forth in the advisory circulars, or State standard, is necessary other than those previously approved by the FAA. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 2. Specifications for the procurement of equipment are not (will not be) proprietary or written so as to restrict competition. At least two manufacturers can meet the specification. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 3. The development included (to be included) in the plans is depicted on the airport layout plan approved by the FAA. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 4. Development that is ineligible for AIP funding has been (will be) omitted from the plans and specifications. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 5. The process control and acceptance tests required for the project by standards contained in Advisory Circular 150/5370-10 are (will be) included in the project specifications. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 6. If a value engineering clause is incorporated into the contract, concurrence was (will be) obtained from the FAA. | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 7. The plans and specifications incorporate (will incorporate) applicable requirements and recommendations set forth in the federally approved environmental finding. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

- | | Yes | No | N/A |
|--|-------------------------------------|--------------------------|--------------------------|
| 8. For construction activities within or near aircraft operational areas, the requirements contained in Advisory Circular 150/5370-2 have been (will be) discussed with the FAA as well as incorporated into the specifications, and a safety/phasing plan has FAA's concurrence, if required. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 9. The project was (will be) physically completed without Federal participation in costs due to errors and omissions in the plans and specifications that were foreseeable at the time of project design. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

I certify, for the project identified herein, responses to the forgoing items are accurate as marked and have prepared documentation attached hereto for any item marked "no" that is correct and complete.

County of Santa Clara

(Name of Sponsor)

(Signature of Sponsor's Designated Official Representative)

Carl Honaker

(Typed Name of Sponsor's Designated Official Representative)

Director of Airports

(Typed Title of Sponsor's Designated Official Representative)

(Date)

Mead & Hunt, Inc.

(Name of Consultant)

(Signature of Mead & Hunt's Official Representative)

Jeffrey T. Leonard, P.E.

(Typed Name of Mead & Hunt's Official Representative)

Project Manager

(Typed Title of Mead & Hunt's Official Representative)

(Date)

EXHIBIT 2

Reid-Hillview Airport

Pavement Rehabilitation

AIP No. 3-06-0225-14

Prepared for
County of Santa Clara



and

Federal Aviation Administration



Prepared by

**Mead
& Hunt**



MAY 2012

I. PROJECT SCOPE

This report documents the design of pavement rehabilitation to Runway 13L-31R, Taxiways A,B,C, D, E, Y, and Z at the Reid-Hillview Airport (RHV) being funded under Airport Improvement Program (AIP) No. 3-06-0225-14. These improvements include pavement microsurfacing of taxiway and runway pavements and installation of airfield signage, including replacing select exiting sign legends to meet FAA standards as well as removing non-frangible mounted signs from within the safety areas. New signs are also needed to clearly direct pilots around the airfield. Realignment of Taxiway C geometry at the intersection with Runway 13L-31R and the installation of new edge lights are addressed on this project as well. When all pavement rehabilitation is complete, new pavement markings will be applied per the FAA AC 150/5340-1K.

Specifically, the improvements included in this Project are:

- Cleaning the existing asphalt surface in preparation for microsurfacing.
- Removal of paint markings and rubber build-up on the pavement.
- Cleaning pavement cracks and treating with herbicide.
- Installation of new airfield signs along Taxiways Y and Z.
- Removal of existing airfield signs that do not meet FAA standards.
- Realignment of Taxiway C and Runway 13L-31R geometry.
- Application of an asphalt microsurfacing treatment.
- Applying new pavement markings.

II. DESIGN STANDARDS

The methodologies used in developing designs for this Project are in conformance with applicable FAA standards. The following Advisory Circulars have been reviewed during the design of the Project:

| | |
|-----------------|---|
| AC 150/5300-13 | <i>Airport Design</i> |
| AC 150/5320-6E | <i>Airport Pavement Design and Evaluation</i> |
| AC 150/5340-1K | <i>Standards for Airport Markings</i> |
| AC 150/5340-18F | <i>Standards for Airport Sign Systems</i> |
| AC 150/5340-30F | <i>Design and Installation Details for Airport Visual Aids</i> |
| AC 150/5345-26D | <i>FAA Specification for L-823 Plug and Receptacle, Cable Connectors</i> |
| AC 150/5345-42F | <i>Specification for Airport Light Bases, Transformer Housings, Junction Boxes, and Accessories</i> |
| AC 150/5345-44J | <i>Specification for Runway and Taxiway Signs</i> |
| AC 150/5345-46D | <i>Specification for Runway and Taxiway Light Fixtures</i> |
| AC 150/5370-2F | <i>Operational Safety on Airports During Construction</i> |
| AC 150/5370-10F | <i>Standards for Specifying Construction of Airports</i> |
| AC 150/5370-12A | <i>Quality Control of Construction for Airport Grant Projects</i> |

III. TOPOGRAPHIC SURVEYS

No topographical survey was performed.

IV. GEOTECHNICAL INVESTIGATION

No geotechnical investigation was performed.

III. ANALYSIS AND DESIGN

A. PRELIMINARY DESIGN REPORT

A visual survey of the current pavement condition was performed February 29, 2012, for Runway 13L-31R and Taxiways A, B, C, D, E, Y, and Z. The following summarizes the visual condition survey:

Runway 13L-31R. The asphalt surface is exhibiting low severity weathering as a result of aging. There are medium severity longitudinal cracks (along paving joints) and low to medium severity transverse cracks through the limits of the runway pavement. Isolated areas, primarily at the intersections of the taxiway connectors, are exhibiting signs of low to medium severity alligator cracking and medium severity block cracking. Weed growth is occurring through the open cracks.

Taxiways A, B, C, D, E, and Y. These taxiways have similar pavement conditions as Runway 13L-31R.

Taxiway Z. The pavement condition of Taxiway Z is divided into two regions separated by the nested/portable T-hangars. The northerly section of Taxiway Z is in good condition, as the asphalt concrete has received a surface treatment as part of the recent adjacent ramp project. The pavement exhibits low severity and isolated cracking, less than a 1/4-inch wide. The southerly portion of the Taxiway Z pavement is similar to the other taxiway pavements mentioned above.

The cracks described herein range from 1/4-inch to 1/2-inch. Due to the severity of the weathering on the asphalt pavement surface and the frequency of the cracks, a slurry seal application is recommended.

All pavement markings in the affected area shall be removed as required for the pavement rehabilitation with some changes and additions for the replacement of the markings after treatment of the pavement due to updates to the FAA AC 150/5340-1K "Standards for Airport Markings" and as recommended in the Reid-Hillview Airport Runway Safety Action Plan (RSAP) update of January 23, 2007.

Due to the recommendations of the RSAP, reconfiguration of the intersection at Taxiway C and Runway 13L-31R will require the relocation of existing sign arrays, as well as the removal and reconfiguration of lights. Non-standard placards, both post mounted and those mounted to lighted sign arrays, need to be removed. Non-standard information signs need to be replaced and the RSAP identified several issues with other airfield signage.

B. DESIGN AIRCRAFT AND LOADING

RHV is a general aviation Airport with an FAA Aircraft Approach Category B and FAA Airplane Design Group I. The Airport Layout Plan lists the critical design aircraft as the Beech Baron 58.

The runway configuration of the parallel pavements are oriented to 13-31 and are approximately 3,100 feet in length and 75 feet wide, with a two parallel taxiway and five connector taxiways. The runway is supported by an apron that is located to the north east, and serviced by Taxiway Z. The pavement loading is rated for 17,000 lbs., single wheel gear.

C. SEALCOAT ALTERNATIVES

The recommendation of the Engineer for pavement rehabilitation, based on the visual inspection, was a slurry seal. The County pointed out that microsurfacing has been very successful on other projects undertaken by the County, and that many local contractors can perform microsurfacing to meet the design standards so bid prices would be competitive. The Engineer has reviewed the County's specification for microsurfacing and has confirmed this process will provide the level of pavement rehabilitation needed for RHV.

D. PAVEMENT MARKING

All pavement markings within the improvement limits will be reapplied. Due to updates to FAA AC 150/5340-1K, new offsets starting at the runway threshold bar result in changes from the existing layout. The runway holding position markers will be 12-inch markings with 12-inch spaces, outlined in black paint as directed in the AC.

E. SIGNAGE AND LIGHTING

As part of the Airport's compliance with the 2007 Airport Runway Safety Action Plan, issued by the FAA ATO Safety Field Operations Group, Runway Safety Program Office, changes to the existing airfield signage are presented on the design documents. Removal of non-standard directional signs and information signs (blue lettering on a white background), installation of new taxiway direction and location signs, and the modification of existing mandatory holding position signs that have direction signs incorrectly co-located on them are proposed on the plans. Painted directional signs are proposed in locations where sign arrays cannot be installed to meet FAA design standards.

Realignment of Taxiway C geometry at the intersection of Runway 13L-31R, as requested in the same FAA RSAP document, requires the removal of existing mandatory holding position signs and taxiway lighting. New mandatory holding signs and new in-pavement lights are shown to be installed in the appropriate locations to meet standards.

IV. AIRPORT SAFETY AND OPERATIONS DURING CONSTRUCTION

1. Runway and Taxiway Closure.

Construction for this project is separated into four construction phases to promote a safe work environment, minimize impact to airport activities and support deliberate, organized, construction activity. Closure of connector and parallel taxiways are coordinated in each construction phase to maintain air operations on Runway 13R-31L during daylight hours. Runway 13R-31L is not a lighted runway and is therefore closed to traffic at night.

While work on Runway 13L-31R is being performed, the runway will be closed and existing lights, signs and NAVAIDs decommissioned during the construction periods. Low profile barricades will be placed along the boundary edge of the proposed work area to minimize the possibility of an aircraft or vehicle accessing an active construction zone. NOTAMs will be

issued by the COUNTY and notice will be given to based aircraft operators about the pending closures and construction schedule.

2. Instrument Operations.

Runway 13L-31R will be closed for a total of six night only closures and ten day/night closures during which time Runway 13R-31L will be open for daytime operations.

- 3. Construction Safety and Phasing Plan (CSPP).** A preliminary Construction Safety and Phasing Plan (CSPP) has been prepared and will be submitted along with this EDR to the County for review. Upon completion of the County's review, the CSPP will be forwarded to the FAA for acceptance prior to advertising for competitive bids.

V. ENGINEER'S COST ESTIMATE

The Reid-Hillview Airport Pavement Rehabilitation Project is one of three Santa Clara County airports projects of similar design that the FAA has allowed to be bid together. Each airport has a separate bid schedule. Bid Schedule 2 is the Reid-Hillview Airport Project and the estimated construction cost for the Bid Schedule 2 Base Bid is \$788,159.

A detailed breakdown of construction costs for the Project is presented in *Appendix B*.

VI. CONSTRUCTION PHASING AND TIME OF COMPLETION

This Project will be completed in two distinct elements: Mobilization and Construction. The overall time for completion of this Project is twenty-six (26) working days.

A. MOBILIZATION

Notice to Proceed with Mobilization will be given immediately after the award of the Contract. All work included in the Mobilization phase shall be completed within ten (10) working days. During this phase of the Project, no work shall be conducted that in any way restricts Airport operations. Mobilization work shall include, but not be limited to, the following:

1. Processing of required submittals, including the Contractor's work schedule.
2. Preparation and submission of the Safety Plan Compliance Document (a sample document is included in the CSPP).
3. All prequalification testing, review, and approval.
4. Mix design preparation, review, and approval.
5. Airfield Safety Devices delivered to site (construction flags, runway closure markers, Airport radios).
6. All miscellaneous Mobilization efforts required to commence construction.
7. Materials and equipment delivered to site, as applicable.

It is the County's intent that all preliminary work required to pursue construction to completion be finalized during the Mobilization phase to minimize delays during construction.

B. CONSTRUCTION

Notice to Proceed with the Construction phase shall be issued at the County's discretion after the start of Mobilization. All work included in the Construction phase shall be completed within sixteen (16) working days. The work included in this Project is all contained on the Airport. There are mandatory requirements and associated liquidated damages, if the specified timeframes are not met.

The following Phases are established for this Project:

1. **Construction Phase 1.** Phase 1 work will be performed during four (4) night-only closures. The County will issue a NOTAM that Runway 13L-31R and Taxiways A, B, C, E, Y, and Z will be closed from 9 p.m. until 5 a.m. for four (4) consecutive nights and direct the Contractor when to place and remove lighted runway closure markers on each runway designation. The work in Construction Phase 1 includes the following:
 - Cleaning pavement surface.
 - Cleaning cracks and treating with herbicide, and filling cracks.
 - Removal of rubber build-up and pavement markings to 60% removal.
 - Installation of new airfield signs with associated electrical conduit and cable.
2. **Construction Phase 2.** Phase 2 work will be performed during six (6) days of full-time closure of Runway 13L-31R. The County will issue a NOTAM that Runway 13L-31R and Taxiways A, B, D, Y, and Z will be closed and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction Phase 2 includes the following:
 - Removal of remaining markings where 95% is required.
 - Demolition of Taxiway C's existing edge lights and lighted signs.
 - Rehabilitation of runway and taxiway pavements with the microsurfacing process.
 - Application of first coat of paint for pavement markings.
3. **Construction Phase 3.** Phase 3 work will be performed during four (4) days of full-time closure. The County will issue a NOTAM that Runway 13L-31R and Taxiways C, E, and parts of Taxiways Y and Z will be closed and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction Phase 3 includes the following:
 - Removal of remaining markings where 95% is required.
 - Rehabilitation of runway and taxiway pavements with the microsurfacing process.
 - Completion of new signing and lighting tasks.
 - Installation of Taxiway C's new edge lights and lighted signs.
 - Application of first coat of paint for pavement markings.

4. **Construction Phase 4.** Phase 4 work shall be performed during two (2) night closures (after 30 days of cure time), at which time the final application of pavement markings shall be applied. The County will issue a NOTAM that both Runways 13L-31R and 13R-31L and Taxiways A, B, C, D, E, Y, and Z will be closed from 6 p.m. until 7 a.m. for two nights and direct the Contractor when to place and remove lighted runway closure markers on each runway designation marker. The work in Construction phase 4 includes the following:

- Final application of pavement markings.

C. LIQUIDATED DAMAGES.

If the overall time limitation for any element of work not be met, liquidated damages of \$2,500 per calendar day will be assessed. Additionally, if the runway is not reopened at the specified date and time allowed for closure period(s), liquidated damages of \$1,000 per hour will be assessed for each hour or portion of hour until the runway is opened to aircraft.

VII. CONTRACT DOCUMENTS

A. BID DOCUMENTS AND GENERAL PROVISIONS

Bid Documents and General Provisions for this Project are adapted from County of Santa Clara Standard Specifications, modified to reflect FAA standards and specific Project requirements. The FAA-required "Standard Federal Contract Clauses and Requirements for Construction Contracts" are included in the Project Specifications.

B. TECHNICAL SPECIFICATIONS

Technical Specifications for this Project are all adapted and modified from FAA Standard Specifications. Where references to a Standard Specification are made, the standards are referred to for information purposes only. All of the information the Contractor needs to complete the work is contained within the Project Plans and Specifications. All additions and modifications to FAA Standard Technical Specifications are italicized and detailed as *Appendix C*

VIII. CONSTRUCTION INSPECTION AND TESTING

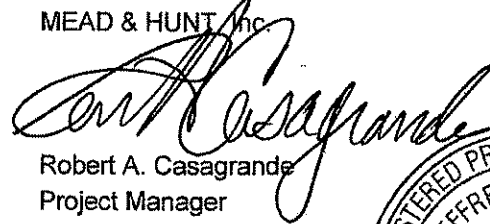
Construction inspection will be provided by the County of Santa Clara. Quality Control and Quality Assurance testing will be provided by the Contractor. The County shall review the test results for compliance with the Contract Documents.

IX. SPONSOR CERTIFICATION FOR PROJECT PLANS AND SPECIFICATIONS

A copy of the Sponsor Certification for Project Plans and Specifications is included as *Appendix C*.

Respectfully submitted by,

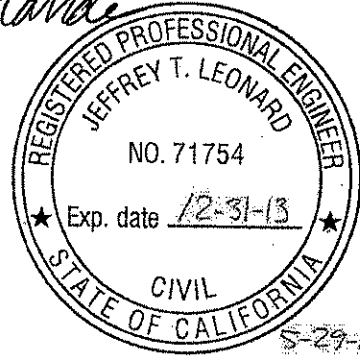
MEAD & HUNT, Inc.



Robert A. Casagrande
Project Manager



Jeffrey T. Leonard, P.E.
Project Engineer

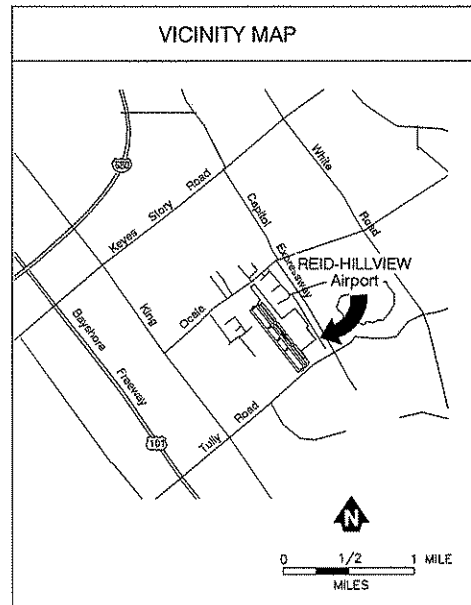


- Appendices:
- Appendix A 90% Plan Sheets
 - Appendix B Engineer's Construction Cost Estimate
 - Appendix C Additions and Modifications to FAA Standard Technical
 - Appendix D Sponsor Certification for Project Plans and Specifications

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Appendix A

90% Reid-Hillview Airport Plan Sheets



COUNTY OF SANTA CLARA

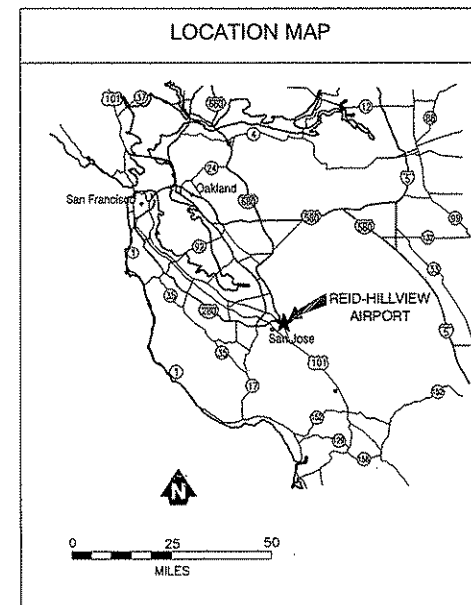
REID-HILLVIEW AIRPORT

REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

BID SCHEDULE 2

AIP NO.: 3-06-0225-14

MAY 2012



Sheet List Table

| SHEET NUMBER | SHEET TITLE |
|--------------|--|
| G-001 | COVER SHEET |
| G-021 | PROJECT LAYOUT PLAN |
| G-081 | CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 1 |
| G-082 | CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 2 |
| G-083 | CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 3 |
| G-084 | CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 4 |
| C-201 | ASPHALT PAVEMENT REHABILITATION PLAN |
| C-202 | ASPHALT PAVEMENT REHABILITATION PLAN |
| C-203 | ASPHALT PAVEMENT REHABILITATION PLAN |
| C-204 | ASPHALT PAVEMENT REHABILITATION PLAN |
| E-301 | SIGNING AND MARKING PLAN |
| E-302 | SIGNING AND MARKING PLAN |
| E-303 | SIGNING AND MARKING PLAN |
| E-304 | SIGNING AND MARKING PLAN |
| E-601 | MARKING DETAILS |
| E-602 | ELECTRICAL AND SIGNING DETAILS |
| E-603 | ELECTRICAL AND SIGNING DETAILS |
| E-604 | ELECTRICAL AND SIGNING DETAILS |

PROJECT DESCRIPTION

- REHABILITATION OF RUNWAY 13-31 & TAXIWAYS A, B, C, D, E, Y & Z
- PAVEMENT MARKINGS
- INSTALLATION AND UPGRADE OF AIRFIELD LED SIGN ARRAYS
- INSTALLATION OF TAXIWAY LED SEMI-FLUSH AND ELEVATED EDGE LIGHTS

DRAWING LEGEND

| | EXISTING | PROPOSED | | EXISTING | PROPOSED |
|----------------------------|----------|----------|---------------------------------|----------|----------|
| AC PAVEMENT | [Symbol] | N/A | REMOVE UG ELECTRIC | [Symbol] | [Symbol] |
| AC PAVEMENT MICROSURFACING | N/A | [Symbol] | RUNWAY EDGE LIGHT | [Symbol] | N/A |
| AGGREGATE BASE | N/A | [Symbol] | TAXIWAY EDGE LIGHT (SEMI-FLUSH) | [Symbol] | [Symbol] |
| PAVEMENT REMOVAL | N/A | [Symbol] | TAXIWAY EDGE LIGHT (ELEVATED) | [Symbol] | [Symbol] |
| PCC PAVEMENT | N/A | N/A | THRESHOLD LIGHT | [Symbol] | N/A |
| SHOULDER BACKING/PLAN | N/A | [Symbol] | FIRE HYDRANT | [Symbol] | N/A |
| SUBGRADE-SECTION | N/A | [Symbol] | WATER | [Symbol] | N/A |
| PROPERTY | [Symbol] | N/A | WATER VALVE | [Symbol] | N/A |
| CONSTRUCTION SAFETY FENCE | N/A | [Symbol] | SANITARY SEWER | [Symbol] | N/A |
| FENCE | [Symbol] | N/A | CATCH BASIN | [Symbol] | N/A |
| GATE | [Symbol] | N/A | FLARED END | [Symbol] | N/A |
| VALLEY GUTTER DEMOLITION | N/A | N/A | FLOW LINE | [Symbol] | N/A |
| ELECTRIC (UNDERGROUND) | [Symbol] | [Symbol] | STORM DRAIN | [Symbol] | N/A |
| LED SIGN ARRAY | [Symbol] | [Symbol] | STORM DRAIN REMOVAL | N/A | N/A |
| PULL BOX | [Symbol] | N/A | TELEPHONE | [Symbol] | N/A |
| | | | LOW BARRICADES & DELINEATOR | N/A | [Symbol] |

ABBREVIATIONS

| | | | | | |
|----------|---------------------------------|-------|-----------------------------------|------|-----------------------------|
| AB | AGGREGATE BASE | GALV | GALVANIZED | REQ | REQUIRED |
| AC | ASPHALT CONCRETE | GB | GRADE BREAK | RWA | RUNWAY WORK RESTRICTED AREA |
| ACB | ASPHALT CONCRETE SUB-BASE | GFCI | GROUND FAULT CIRCUIT INTERRUPTER | RWY | RUNWAY |
| AWG | AMERICAN WIRE GAUGE | GS | GROUND SHOT | SD | STORM DRAIN |
| BC | BEGINNING OF CURVE | HDPE | HIGH DENSITY POLYETHYLENE | SDMH | STORM DRAIN MANHOLE |
| BLDG | BUILDING | HORIZ | HORIZONTAL | SG | STRAIGHT GRADE |
| BM | BENCHMARK | HP | HIGH POINT | SH | SHOULDER |
| BVC | BEGIN VERTICAL CURVE | IE | INVERT ELEVATION | SS | SANITARY SEWER |
| CL | CENTERLINE | L | LENGTH | SSMH | SANITARY SEWER MANHOLE |
| CLF | CHAIN LINK FENCE | LF | LINEAL FEET | STA | STATION |
| CB | CATCH BASIN | MAX | MAXIMUM | STD | STANDARD |
| CMP | CORRUGATED METAL PIPE | MID | MID POINT | T | TELEPHONE LINE |
| DB | DIRECT BURIAL | MIN | MINIMUM | TC | TOP OF CURB |
| DIA or Ø | DIAMETER | MIRL | MEDIUM INTENSITY RUNWAY LIGHTING | TG | TOP OF GRATE |
| (E) | EXISTING | MITL | MEDIUM INTENSITY TAXIWAY LIGHTING | T/L | TAXILANE |
| E | ELECTRICAL LINE | (N) | NEW | TOE | TOE OF BANK |
| EC | END OF CURVE | NIS | NOT IN SERVICE | TOP | TOP OF BANK |
| EG | EXISTING GRADE (OR GROUND) | OC | ON CENTER | TWY | TAXIWAY |
| EL | ELEVATION | PB | PULLBOX | TYP | TYPICAL |
| EP | EDGE OF PAVEMENT | PC | POINT OF CURVATURE | UON | UNLESS OTHERWISE NOTED |
| ETR | EXISTING TO REMAIN | PCC | PORTLAND CEMENT CONCRETE | USA | UNDERGROUND SERVICE ALERT |
| FAA | FEDERAL AVIATION ADMINISTRATION | PI | POINT OF INTERSECTION | VERT | VERTICAL |
| FBO | FIXED BASE OPERATOR | PT | POINT OF TANGENCY | VG | VALLEY GUTTER |
| FF | FINISH FLOOR | PVC | POLY-VINYL CHLORIDE | W | WATER LINE |
| FG | FINISH GRADE | PVI | POINT OF VERTICAL INTERSECTION | W/ | WITH |
| FH | FIRE HYDRANT | R | REMOVE | W/O | WITHOUT |
| FL | FLOW LINE | R&R | REMOVE & REPLACE | WV | WATER VALVE |
| G | GAS LINE | RC | RELATIVE COMPACTION | WWM | WELDED WIRE MESH |
| | | RCP | REINFORCED CONCRETE PIPE | | |

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 REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
 CONNECTOR TAXIWAYS

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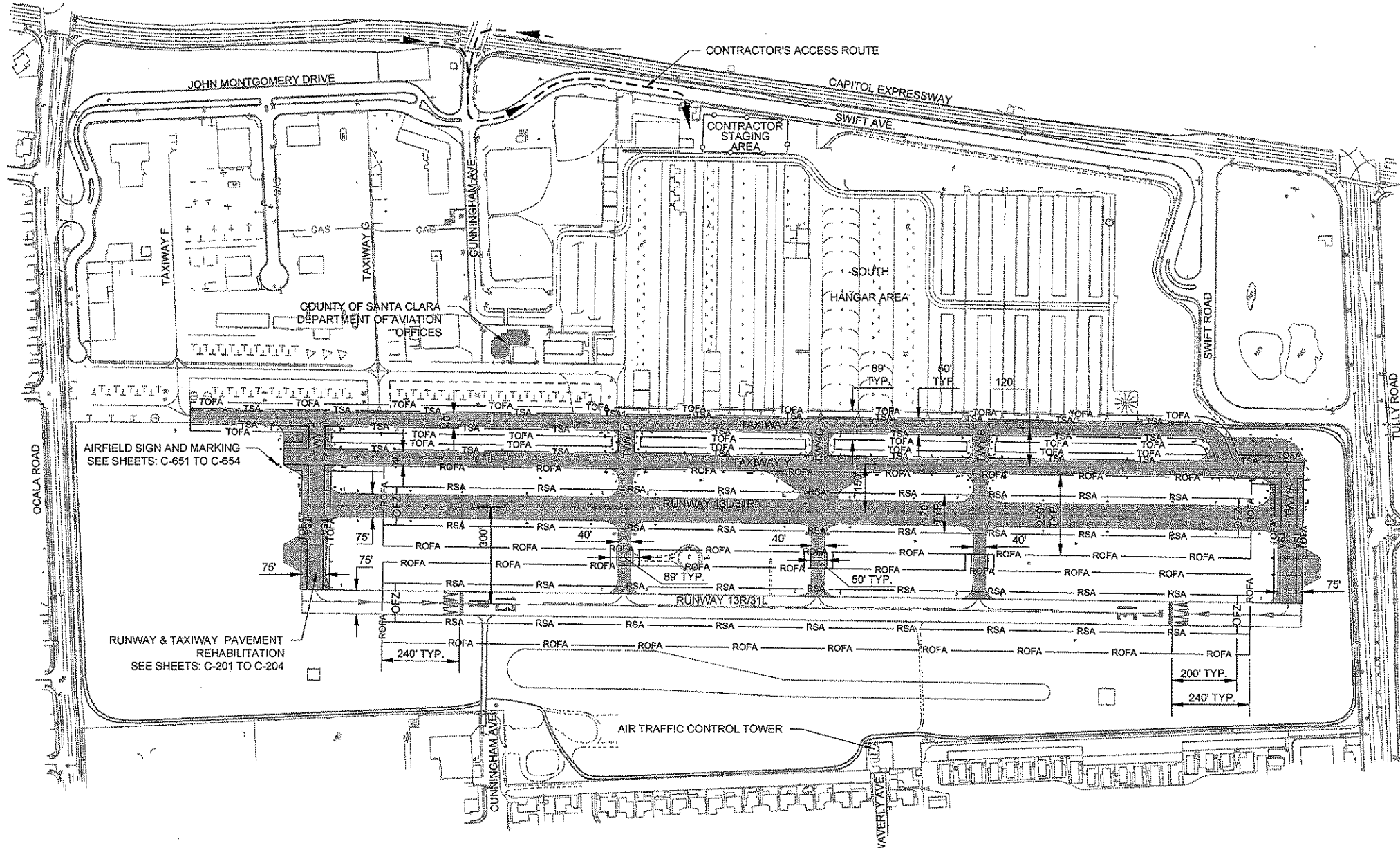
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SHEET CONTENTS
 COVER SHEET

SHEET NO. 1 of 15

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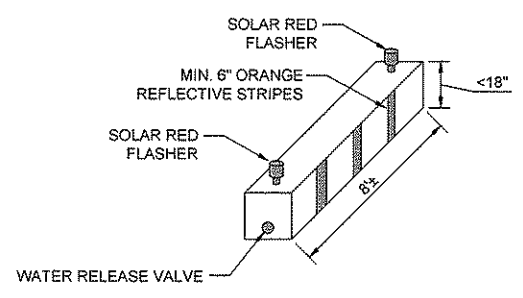
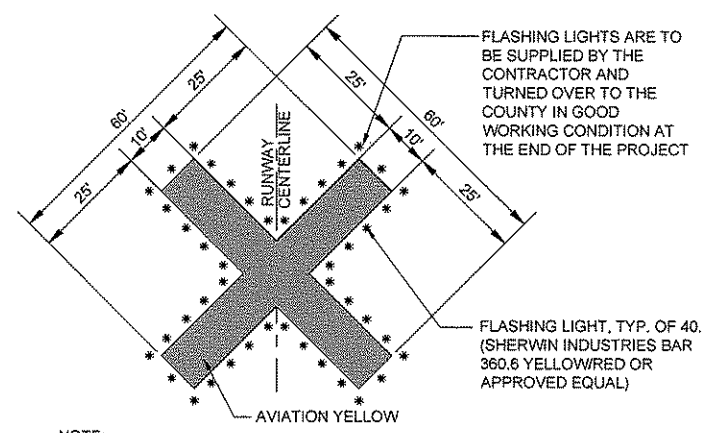
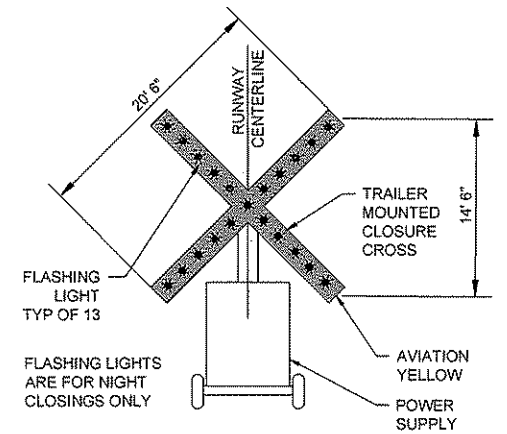
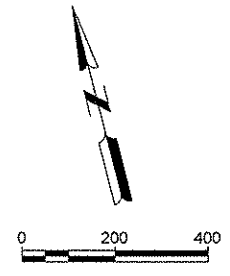


GENERAL NOTES:

1. WORK WITHIN THE RUNWAY RESTRICTED WORK AREA (RWA) REQUIRES CLOSURE OF THE RUNWAY. SEE PROJECT SPECIFICATIONS, SPECIAL PROVISIONS SECTION 1000 "GENERAL REQUIREMENTS" AND SHEET G-081 FOR ADDITIONAL INFORMATION ON CLOSURE REQUIREMENTS, LIMITATIONS ON CONSTRUCTION, AND ACCESS TO THE VARIOUS WORK AREAS.
2. CONTRACTOR'S ACCESS TO THE AIRPORT SHALL BE VIA THE ROUTES SHOWN UNLESS OTHERWISE APPROVED BY THE COUNTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING DAMAGE TO PAVEMENTS OR OTHER EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITIES. CONSTRUCTION TRAFFIC IS LIMITED TO APPROVED HAUL AND ACCESS ROUTES. HAUL ROUTES ON PAVEMENTS SHALL BE CLEANED EACH DAY AND DUST CONTROL SHALL BE MAINTAINED BY THE CONTRACTOR, USING THE METHODS OUTLINED IN SPECIFICATIONS, SPECIAL PROVISIONS SECTION 1002. "EARTHWORK AND SITE PREPARATION".

LEGEND

- MICROSURFACING
- CONTRACTOR ACCESS
- OBJECT FREE ZONE
- RUNWAY OBJECT FREE AREA
- RUNWAY SAFETY AREA
- RESTRICTED WORK AREA
- TAXIWAY OBJECT FREE AREA
- TAXIWAY SAFETY AREA



NOTE: TEMPORARY CLOSED RUNWAY MARKERS SUPPLIED BY THE CONTRACTOR SHALL BE INSTALLED AND REMOVED BY THE CONTRACTOR DURING CONSTRUCTION PER THE SCHEDULE APPROVED BY THE COUNTY. ONE SET OF CLOSURE MARKERS ARE TO BE TURNED OVER TO THE COUNTY IN GOOD WORKING ORDER AT COMPLETION OF PROJECT/

NOTE: TEMPORARY CLOSED RUNWAY MARKERS SHALL BE INSTALLED AND REMOVED BY THE CONTRACTOR DURING CONSTRUCTION PER THE SCHEDULE APPROVED BY THE COUNTY. CLOSED RUNWAY MARKERS AND FLASHERS SHALL BE TURNED OVER TO THE AIRPORT AT THE END OF THE PROJECT.

- CONTRACTOR SHALL SUPPLY 162 LOW PROFILE BARRICADES WHICH SHALL BE PLACED 20 FEET O.C. WITH A DELINEATOR PLACED BETWEEN AND SHALL BE TURNED OVER TO AIRPORT AT END OF PROJECT IN GOOD WORKING CONDITION.
- ADDITIONAL DELINEATORS CAN BE PLACED BETWEEN BARRICADES AS NEEDED

A TRAILER MOUNTED RUNWAY CLOSURE MARKER DETAIL
NO SCALE

B VINYL RUNWAY CLOSURE MARKER DETAIL
NO SCALE M101B 01/12

C LOW PROFILE BARRICADE DETAIL
NO SCALE R403 01/12



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REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

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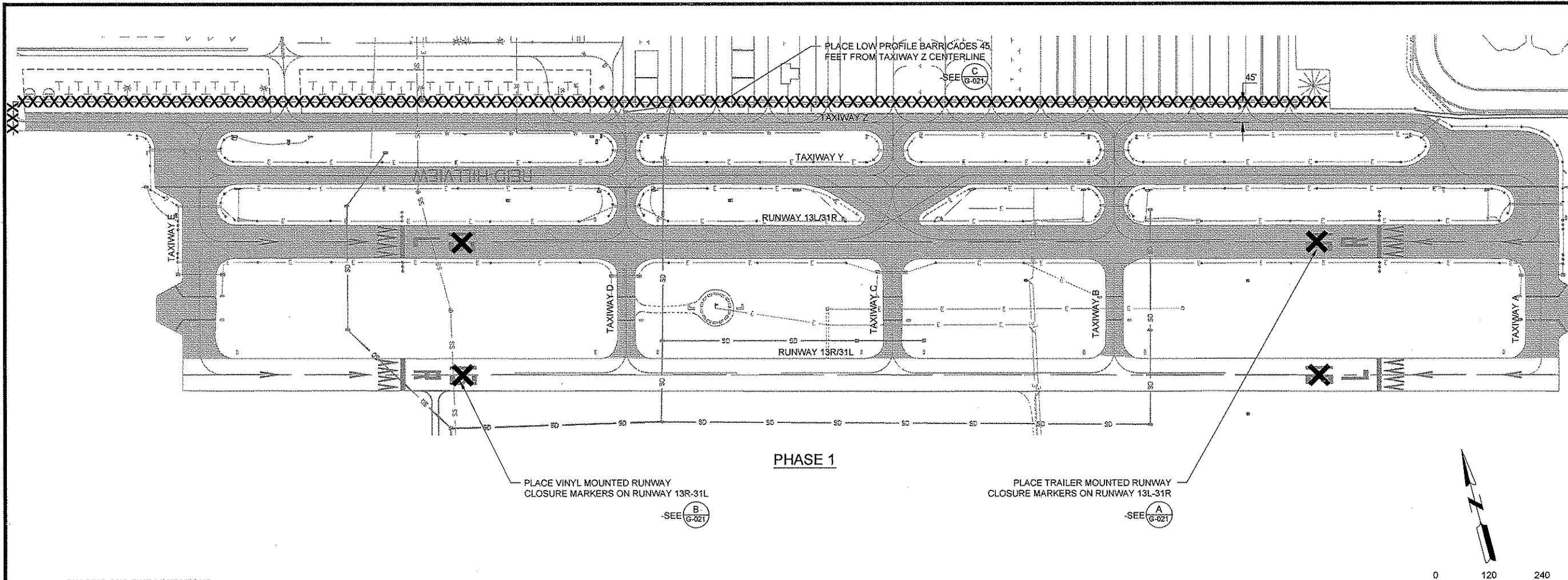
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SHEET CONTENTS
PROJECT LAYOUT PLAN

SHEET NO. 2 of 15

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**REID-HILLVIEW AIRPORT
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 CONNECTOR TAXIWAYS**

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| DRAWN BY: | BE |
| CHECKED BY: | JTL |
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PHASING AND TIME LIMITATIONS

THE PROJECT HAS BEEN DIVIDED INTO TWO DISTINCT PHASES: MOBILIZATION; AND CONSTRUCTION. CONSTRUCTION WILL BE DIVIDED INTO FOUR SUBPHASES TO MINIMIZE IMPACT TO OPERATIONS AT THE AIRPORT AND DEFINE THE SEQUENCE OF THE WORK ASSOCIATED WITH THE PAVEMENT REHABILITATION. A SEPARATE NOTICE TO PROCEED SHALL BE ISSUED FOR THE MOBILIZATION PHASE AND THE CONSTRUCTION PHASE. THE WORK EFFORTS AND AFFECTED AIRFIELD AREAS WITHIN THE AOA ARE DETAILED BELOW AND ALL WORK WILL BE COORDINATED WITH THE ATO. THE OVERALL TIME OF COMPLETION FOR THE PROJECT IS TWENTY-SIX (26) WORKING DAYS; MOBILIZATION SHALL BE COMPLETED WITHIN TEN (10) WORKING DAYS; CONSTRUCTION (ALL PHASES) SHALL BE COMPLETED WITHIN SIXTEEN (16) WORKING DAYS AS DETAILED BELOW.

1. MOBILIZATION PHASE

(TEN (10) WORKING DAYS). NOTICE TO PROCEED WITH MOBILIZATION WILL BE ISSUED IMMEDIATELY AFTER AWARD OF CONTRACT.

DURING THIS PHASE OF THE PROJECT, NO WORK SHALL BE CONDUCTED THAT IN ANY WAY RESTRICTS AIRPORT OPERATIONS. MOBILIZATION WORK SHALL INCLUDE, BUT NOT BE LIMITED TO, THE FOLLOWING:

- a) PROCESSING OF REQUIRED SUBMITTALS, INCLUDING THE CONTRACTOR'S WORK SCHEDULE.
- b) PREPARATION AND SUBMISSION OF THE SPCD.
- c) ALL PREQUALIFICATION TESTING, REVIEW, AND APPROVAL.
- d) MIX DESIGN PREPARATION, REVIEW, AND APPROVAL.
- e) AIRFIELD SAFETY DEVICES DELIVERED TO SITE (CONSTRUCTION FLAGS, RUNWAY CLOSURE MARKERS, AIRPORT RADIOS).
- f) ALL MISCELLANEOUS MOBILIZATION EFFORTS REQUIRED TO COMMENCE CONSTRUCTION.
- g) MATERIALS AND EQUIPMENT DELIVERED TO SITE, AS APPLICABLE.

ALL PRELIMINARY WORK REQUIRED TO PURSUE CONSTRUCTION TO COMPLETION SHALL BE FINALIZED DURING THE MOBILIZATION PHASE TO MINIMIZE DELAYS DURING CONSTRUCTION.

2. CONSTRUCTION PHASE

(SIXTEEN (16) WORKING DAYS). NOTICE TO PROCEED WITH CONSTRUCTION WILL BE ISSUED AT THE COUNTY'S DISCRETION AT ANY TIME AFTER THE START OF MOBILIZATION. THE CONSTRUCTION PHASE CONSISTS OF FOUR SUBPHASES AS DESCRIBED BELOW.

- a) **CONSTRUCTION PHASE 1.** PHASE 1 WORK WILL CONSIST OF CLEANING PAVEMENT SURFACE AND CRACKS, TREATING CRACKS WITH HERBICIDE, AND FILLING; CONSTRUCTION OF NEW SIGNS AND SIGN PADS; TRENCHING, PLACING CONDUIT, CABLE AND GROUNDING; AND REMOVAL OF RUBBER BUILD-UP AND 60% OF PAVEMENT MARKINGS. THIS WORK SHALL BE PERFORMED DURING FOUR (4) NIGHT ONLY CLOSURES. EXISTING SIGNS AND LIGHTS SHALL BE FUNCTIONAL EVERY DAY THE AIRPORT IS OPEN. THE COUNTY WILL ISSUE A NOTAM THAT RUNWAY 13L-31R AND TAXIWAYS A, B, C, D, E, Y, AND Z WILL BE CLOSED FROM 9 P.M. UNTIL 5 A.M. FOR FOUR (4) CONSECUTIVE NIGHTS AND DIRECT THE CONTRACTOR WHEN TO PLACE AND REMOVE LIGHTED RUNWAY CLOSURE MARKERS ON EACH RUNWAY DESIGNATION MARKER.

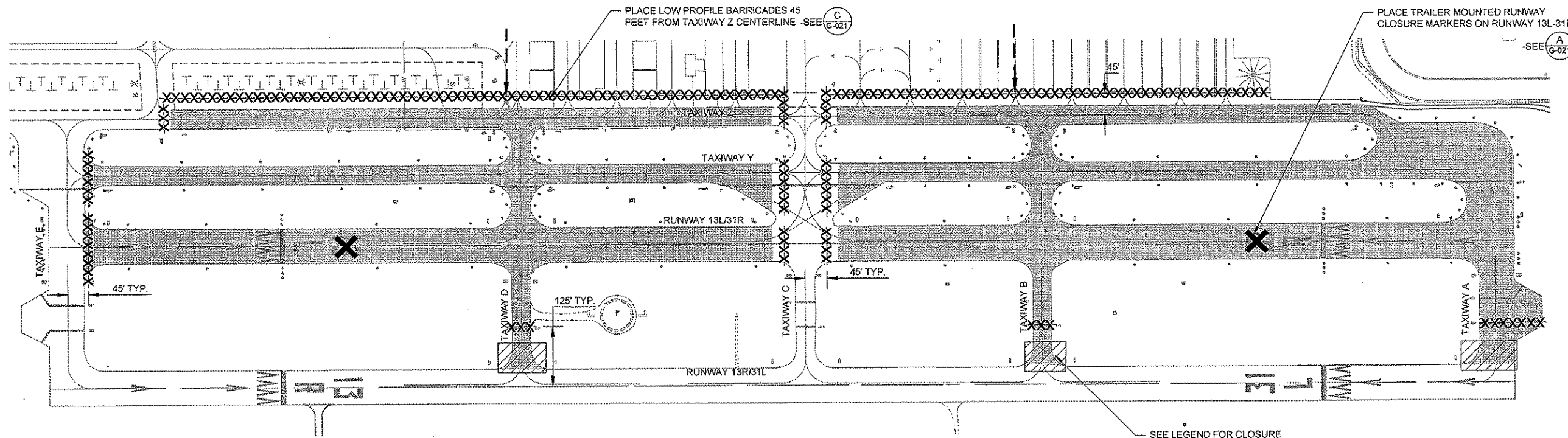
CONSTRUCTION PHASE 1 SUMMARY.

- SCOPE OF WORK - PREPARATION OF RUNWAY AND TAXIWAY ASPHALT PAVEMENTS FOR APPLICATION OF ASPHALT MICROSURFACING REHABILITATION AND CONSTRUCTION OF NEW SIGNS.
- AREA CLOSED TO AIRCRAFT OPERATIONS - ENTIRE AIRPORT IS CLOSED FROM 9 P.M. TO 5 A.M. EACH WORKING DAY.
- DURATION OF CLOSURE - FOUR (4) NIGHTS FROM 9 P.M. TO 5 A.M.
- ALTERNATE TAXI ROUTE - NONE. AIRPORT WILL BE CLOSED.
- EMERGENCY ACCESS ROUTES - COMMUNITY FIRE AND RESCUE SERVICES ENTER THROUGH EXISTING GATE "A" OFF OF CUNNINGHAM AVENUE TO THE AIRPORT APRON. CONSTRUCTION OPERATIONS SHALL NOT HINDER ACCESS TO AIRPORT APRON, RUNWAYS, OR TAXIWAYS.
- CONSTRUCTION STAGING AREA - CONTRACTOR'S STAGING AREA IS OFF OF SWIFT AVENUE, THROUGH AN EXISTING GATE, ON THE NORTH SIDE OF THE AIRPORT. THIS AREA IS OFF OF AIRPORT PAVEMENT AND AWAY FROM MOVING OR PARKED AIRCRAFT.
- CONSTRUCTION ACCESS AND HAUL ROUTE - ACCESS WILL BE THROUGH THE EXISTING GATE ADJACENT TO THE SJSU HANGAR ON SWIFT AVENUE AND THE HAUL ROUTE TO THE PROJECT SITE WILL BE ALONG TAXILANE H OR TAXILANE U AS PHASING REQUIRES.
- IMPACTS TO NAVAIDS - NONE.
- LIGHTING AND MARKING CHANGES - NONE.
- REQUIRED HAZARD MARKING AND LIGHTING - LOW PROFILE BARRICADES AND 42-INCH HIGH PLASTIC DELINEATORS WILL BE PLACED ALONG THE BOUNDARY OF TAXIWAY Z AND THE ADJACENT APRON/HANGAR AREA PRIOR TO BEGINNING CONSTRUCTION. THE BOUNDARY FOR THE RESTRICTED WORK AREA (RWA) LIMIT IS 44.5 FEET FROM THE CENTERLINE OF THE NORTH SIDE OF TAXIWAY Z. THIS LINE OF BARRICADES WILL BLOCK ENTRY INTO ANY WORK AREA ON THE AIRPORT FOR BASED AIRCRAFT. ALL AIRFIELD LIGHTS AND NAVIGATIONAL AND VISUAL AIDS WILL BE DECOMMISSIONED EACH NIGHT DURING CONSTRUCTION HOURS.
- LEAD TIMES FOR REQUIRED NOTIFICATION - FIVE (5) WORKING DAYS

LEGEND

- MICROSURFACING
- CONTRACTOR ACCESS
- LOW PROFILE BARRICADES

DRAWN BY: BE, CHECKED BY: JTL, DATE: MAY 2012, PROJECT: REID-HILLVIEW AIRPORT, SHEET: G-081



PHASE 2

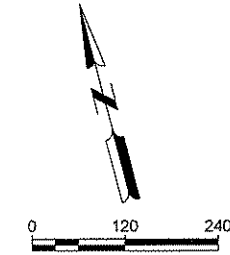
CONSTRUCTION PHASE 2

PHASE 2 WORK SHALL CONSIST OF CONNECTION OF NEW SIGN ARRAY PANELS TO ELECTRICAL SERVICE; DEMOLITION OF EXISTING LIGHTS AND SIGNS FOR REALIGNMENT OF TAXIWAY C; FINAL REMOVAL AND REPLACEMENT OF EXISTING SIGNS; REMOVAL OF FINAL PERCENTAGE OF RUNWAY MARKINGS; APPLICATION OF MICROSURFACING TO ENTIRE CLOSED PROJECT AREA; AND FIRST APPLICATION OF PAVEMENT MARKINGS. THIS WORK SHALL BE PERFORMED DURING SIX (6) CONSECUTIVE DAYS OF FULL-TIME CLOSURE BEGINNING ON A SUNDAY AT 9 P.M. THROUGH SATURDAY AT 5 A.M. THE COUNTY WILL ISSUE A NOTAM THAT RUNWAY 13L-31R AND TAXIWAYS A, B, D, Y, AND Z WILL BE CLOSED AND DIRECT THE CONTRACTOR WHEN TO PLACE AND REMOVE RUNWAY CLOSURE MARKERS ON EACH RUNWAY DESIGNATION MARKER. RUNWAY 13R-31L AND TAXIWAYS C AND E WILL BE OPEN TO TRAFFIC DURING PHASE 2.

CONSTRUCTION PHASE 2 SUMMARY

- SCOPE OF WORK - FINAL ELECTRICAL WORK FOR SIGNS AND LIGHTS, APPLICATION OF ASPHALT MICROSURFACING MATERIAL, AND FIRST APPLICATION OF PAVEMENT MARKINGS.
- AREA CLOSED TO AIRCRAFT OPERATIONS - RUNWAY 13L-31R, TAXIWAYS A, B, D, Y, AND Z.
- DURATION OF CLOSURE - SIX (6) WORKING DAYS
- ALTERNATE TAXI ROUTE - ACCESS TO RUNWAY 13R WILL BE BY TAXIWAY E. ACCESS TO RUNWAY 31L WILL BE BY TAXIWAY C, THEN BACK TAXI TO RUNWAY 31L THRESHOLD. PLANES LANDING 13R WILL EXIT RUNWAY AT TAXIWAY C. PLANES LANDING RUNWAY 31L CAN EXIT TAXIWAY C OR E.
- EMERGENCY ACCESS ROUTES - COMMUNITY FIRE AND RESCUE SERVICES ENTER THROUGH EXISTING GATE 'A' IN THE PARKING LOT NEAR THE TERMINAL OFF OF CUNNINGHAM AVENUE TO THE AIRPORT APRON. CONSTRUCTION OPERATIONS WILL NOT HINDER ACCESS TO AREAS OF AIRPORT.
- CONTRACTOR'S STAGING AREA - CONTRACTOR'S STAGING AREA IS OFF OF SWIFT AVENUE, THROUGH AN EXISTING GATE, ON THE NORTH SIDE OF THE AIRPORT. THIS AREA IS OFF OF AIRPORT PAVEMENT AND AWAY FROM MOVING OR PARKED AIRCRAFT.
- CONSTRUCTION ACCESS AND HAUL ROUTE - ACCESS WILL BE THROUGH THE EXISTING GATE ADJACENT TO THE SJSU HANGAR ON SWIFT AVENUE AND THE HAUL ROUTE TO THE PROJECT SITE WILL BE ALONG TAXILANE H OR TAXILANE U AS PHASING REQUIRES.
- IMPACTS TO NAVAIDS - NONE.
- LIGHTING AND MARKING CHANGES - NONE.
- REQUIRED HAZARD MARKING AND LIGHTING - LOW PROFILE BARRICADES AND 42-INCH HIGH PLASTIC DELINEATORS WILL BE PLACED ALONG THE BOUNDARY OF TAXIWAY Z EXCEPT AT THE INTERSECTIONS OF TAXIWAYS C AND E AND THE ADJACENT APRONS PRIOR TO BEGINNING CONSTRUCTION. THE BOUNDARY FOR THE RESTRICTED WORK AREA (RWA) LIMIT IS 44.5 FEET NORTH OF THE CENTERLINE OF TAXIWAY Z. BARRICADES WILL ALSO BE PLACED ALONG TAXIWAY C AND E AT THEIR INTERSECTIONS WITH TAXIWAYS Y AND Z, AS WELL AS THE INTERSECTIONS WITH RUNWAY 13L-31R. THE DISTANCE FROM THE CENTERLINE OF TAXIWAYS C AND E TO THE BARRICADES ARE 25 FEET. LOW PROFILE BARRICADES WILL ALSO BE PLACED OUTSIDE THE RUNWAY SAFETY AREA (RSA) OF RUNWAY 13R-31L AND THE INTERSECTION OF TAXIWAYS A, B, AND D. THE BOUNDARY FOR THE RWA LIMIT IS 60 FEET NORTH OF THE CENTERLINE OF RUNWAY 13R-31L. THIS LINE OF BARRICADES WILL BLOCK ENTRY INTO ANY WORK AREA ON THE AIRPORT FOR TAXING AIRCRAFT. ALL AIRFIELD LIGHTS, NAVIGATIONAL AND VISUAL AIDS WILL BE DECOMMISSIONED DURING THIS CONSTRUCTION PHASE.
- LEAD TIMES FOR REQUIRED NOTIFICATION - FIVE (5) WORKING DAYS

SEE LEGEND FOR CLOSURE INFORMATION (TYPICAL)



NOTE:

IF AT ANYTIME THE CONTRACTOR WORKS INSIDE RUNWAY 13R-31L RSA, THE VINYL RUNWAY CLOSURE MARKER SHALL BE PLACED ON RUNWAY DESIGNATOR MARKERS AS SHOWN ON G-081, PHASE 1

LEGEND

- MICROSURFACING
- CONTRACTOR ACCESS
- LOW PROFILE BARRICADES
- WORK INSIDE "RSA" TO BE COMPLETED DURING NIGHTTIME AIRPORT CLOSURE AND COORDINATED WITH COUNTY (NIGHTTIME CLOSURE, 10PM TO 5AM)

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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

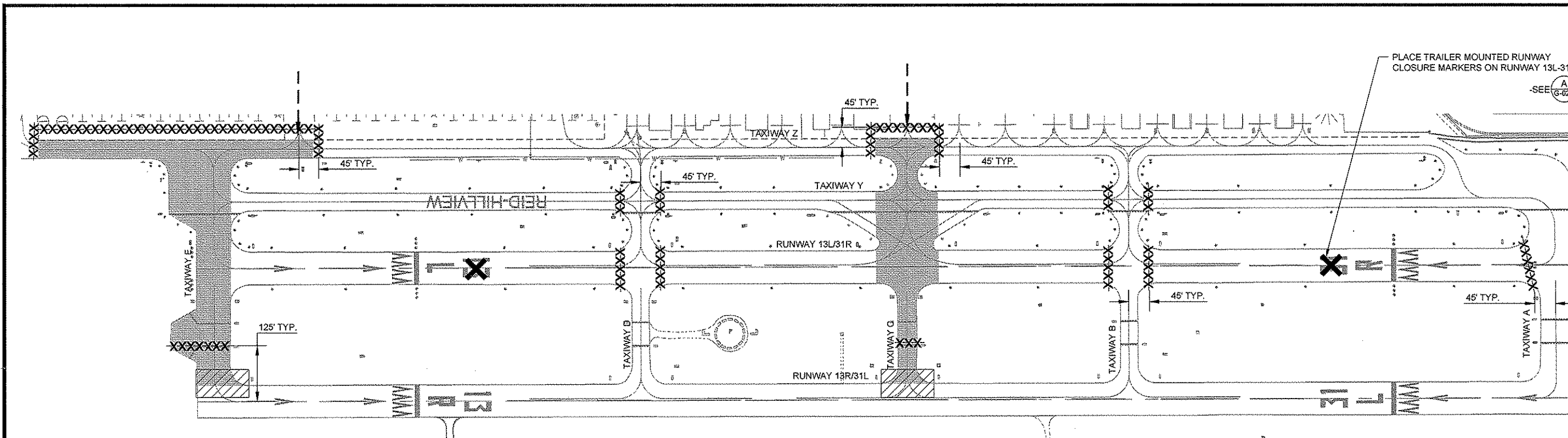
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SHEET COMMENTS
CONSTRUCTION
SAFETY AND PHASING
PLAN - PHASE 2

SHEET NO. 1 of 15

G-082



PHASE 3

CONSTRUCTION PHASE 3

PHASE 3 WORK SHALL CONSIST OF INSTALLATION OF TAXIWAY C EDGE LIGHTS AND SIGNS; CLEAN PAVEMENT SURFACES; APPLICATION OF MICROSURFACING TO ENTIRE CLOSED PROJECT AREA; APPLICATION OF FIRST COAT OF PAINT FOR PAVEMENT MARKINGS. THIS WORK SHALL BE PERFORMED DURING FOUR (4) CONSECUTIVE DAYS OF FULL-TIME CLOSURE BEGINNING ON A SUNDAY AT 9 P.M. THROUGH THURSDAY AT 9 P.M. THE COUNTY WILL ISSUE A NOTAM THAT RUNWAY 13L-31R AND TAXIWAYS C, E, AND PART OF Y, AND Z WILL BE CLOSED AND DIRECT THE CONTRACTOR WHEN TO PLACE AND REMOVE RUNWAY CLOSURE MARKERS ON EACH RUNWAY DESIGNATION MARKER. RUNWAY 13R-31L AND TAXIWAYS A, B, D, AND PARTS OF Y AND Z WILL BE OPEN TO TRAFFIC DURING PHASE 3.

CONSTRUCTION PHASE 3 SUMMARY

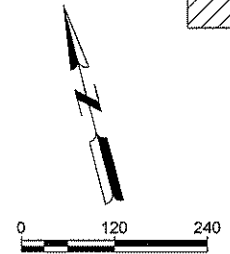
- SCOPE OF WORK - APPLICATION OF ASPHALT MICROSURFACING MATERIAL TO REHABILITATE THE PAVEMENT AND FIRST APPLICATION OF PAVEMENT MARKINGS.
- AREA CLOSED TO AIRCRAFT OPERATIONS - RUNWAY 13L-31R, TAXIWAYS C, E, AND PARTS OF Y AND Z.
- DURATION OF CLOSURE - FOUR (4) WORKING DAYS
- ALTERNATE TAXI ROUTE - ACCESS TO RUNWAY 13R WILL BE BY TAXIWAY D. ACCESS TO RUNWAY 31L WILL BE BY TAXIWAY A, TAXIWAY Z, OR EASTERN MOST PORTION OF TAXIWAY Y. PLANES LANDING RUNWAY 13R CAN EXIT TAXIWAY B. PLANES LANDING RUNWAY 31L CAN EXIT ON TAXIWAYS B OR D TO HANGARS AND TIE-DOWNS.
- EMERGENCY ACCESS ROUTES - COMMUNITY FIRE AND RESCUE SERVICES ENTER THROUGH EXISTING GATE "A" IN THE PARKING LOT NEAR THE TERMINAL OFF OF CUNNINGHAM AVENUE TO THE AIRPORT APRON. CONSTRUCTION OPERATIONS SHALL NOT HINDER ACCESS TO AIRPORT APRON, RUNWAYS, OR TAXIWAYS.
- CONTRACTOR'S STAGING AREA - CONTRACTOR'S STAGING AREA IS OFF OF SWIFT AVENUE, THROUGH AN EXISTING GATE, ON THE NORTH SIDE OF THE AIRPORT. THIS AREA IS OFF OF AIRPORT PAVEMENT AND AWAY FROM MOVING OR PARKED AIRCRAFT.
- CONSTRUCTION ACCESS AND HAUL ROUTE - ACCESS WILL BE THROUGH THE EXISTING GATE ADJACENT TO THE SJSU HANGAR ON SWIFT AVENUE AND THE HAUL ROUTE TO THE PROJECT SITE WILL BE ALONG TAXILANE H OR TAXILANE U AS PHASING REQUIRES.
- IMPACTS TO NAVAIDS - NONE
- LIGHTING AND MARKING CHANGES - NEW TAXIWAY C EDGE LIGHT AND MARKINGS.
- REQUIRED HAZARD MARKING AND LIGHTING - LOW PROFILE BARRICADES AND 42-INCH HIGH PLASTIC DELINEATORS WILL BE PLACED ALONG THE APRON BOUNDARY AT THE INTERSECTION OF TAXIWAYS E, C, AND Z PRIOR TO BEGINNING CONSTRUCTION. LOW PROFILE BARRICADES WILL ALSO BE PLACED AT THE INTERSECTIONS OF TAXIWAYS B, D, AND Y, AS WELL AS THE INTERSECTION WITH RUNWAY 13L-31R. THE BOUNDARY FOR THE RWA LIMIT IS 44.5 FEET NORTH OF THE CENTERLINE OF TAXIWAY Z AND 25 FEET FROM THE CENTERLINES OF TAXIWAYS B AND D. LOW PROFILE BARRICADES WILL ALSO BE PLACED OUTSIDE THE RSA OF RUNWAY 13R-31L AND THE INTERSECTION OF TAXIWAYS E AND C. THE BOUNDARY FOR THE RWA LIMIT IS 60 FEET FROM THE CENTERLINE OF RUNWAY 13R-31L. THIS LINE OF BARRICADES WILL BLOCK ENTRY INTO ANY WORK AREA ON THE AIRPORT FOR TAXIING AIRCRAFT. ALL AIRFIELD LIGHTS AND NAVIGATIONAL AND VISUAL AIDS WILL BE DECOMMISSIONED DURING THIS CONSTRUCTION PHASE.
- LEAD TIMES FOR REQUIRED NOTIFICATION - FIVE (5) WORKING DAYS

NOTE:

IF AT ANYTIME THE CONTRACTOR WORKS INSIDE RUNWAY 13R-31L RSA, THE VINYL RUNWAY CLOSURE MARKER SHALL BE PLACED ON RUNWAY DESIGNATOR MARKERS AS SHOWN ON G-081, PHASE 1

LEGEND

- MICROSURFACING
- CONTRACTOR ACCESS
- LOW PROFILE BARRICADES
- WORK INSIDE "RRWA" TO BE COMPLETED DURING NIGHTTIME AIRPORT CLOSURE AND COORDINATED WITH COUNTY (NIGHTTIME CLOSURE, 10PM TO 5AM)



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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

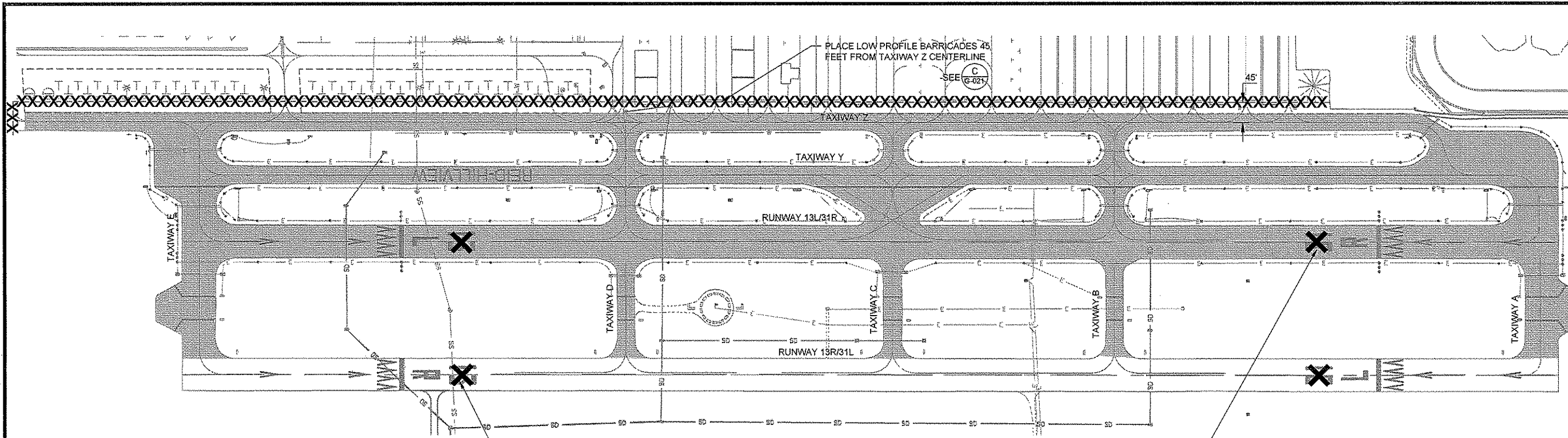
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SHEET CONTENTS
CONSTRUCTION
SAFETY AND PHASING
PLAN - PHASE 3

SHEET NO. 1 of 15

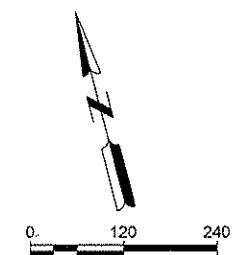
G-083



PHASE 4

PLACE VINYL MOUNTED RUNWAY CLOSURE MARKERS ON RUNWAY 13R-31L

PLACE TRAILER MOUNTED RUNWAY CLOSURE MARKERS ON RUNWAY 13L-31R



CONSTRUCTION PHASE 4

PHASE 4 WORK SHALL BE PERFORMED DURING TWO (2) NIGHT-TIME CLOSURES FROM 9 P.M. THROUGH 5 A.M. EACH NIGHT. THE COUNTY WILL ISSUE A NOTAM THAT THE AIRPORT WILL BE CLOSED AND DIRECT THE CONTRACTOR WHEN TO PLACE AND REMOVE RUNWAY CLOSURE MARKERS ON EACH RUNWAY DESIGNATION MARKER.

CONSTRUCTION PHASE 4 SUMMARY

- SCOPE OF WORK - APPLICATION OF SECOND COAT OF PAINT FOR PAVEMENT MARKINGS.
- AREA CLOSED TO AIRCRAFT OPERATIONS - ENTIRE AIRPORT IS CLOSED.
- DURATION OF CLOSURE - TWO (2) NIGHT CLOSURES FROM 9 P.M. THROUGH 5 A.M. EACH NIGHT.
- ALTERNATE TAXI ROUTE - NONE, AIRPORT WILL BE CLOSED.
- EMERGENCY ACCESS ROUTES - COMMUNITY FIRE AND RESCUE SERVICES ENTER THROUGH EXISTING GATE "A" IN THE PARKING LOT NEAR THE TERMINAL OFF OF CUNNINGHAM AVENUE TO THE AIRPORT APRON. CONSTRUCTION OPERATIONS WILL NOT HINDER ACCESS TO AIRPORT APRON, RUNWAYS, OR TAXIWAYS.
- CONSTRUCTION STAGING AREA - CONTRACTOR'S STAGING AREA IS OFF OF SWIFT AVENUE, THROUGH AN EXISTING GATE, ON THE NORTH SIDE OF THE AIRPORT. THIS AREA IS OFF OF AIRPORT PAVEMENT AND AWAY FROM MOVING OR PARKED AIRCRAFT.
- CONSTRUCTION ACCESS AND HAUL ROUTE - ACCESS WILL BE THROUGH THE EXISTING GATE ADJACENT TO THE SJSU HANGAR ON SWIFT AVENUE AND THE HAUL ROUTE WILL BE ALONG TAXILANE H OR TAXILANE U AS PHASING REQUIRES.
- IMPACTS TO NAVAIDS - NONE.
- LIGHTING AND MARKING CHANGES - NONE.
- REQUIRED HAZARD MARKING AND LIGHTING - LOW PROFILE BARRICADES AND 42-INCH HIGH PLASTIC DELINEATORS WILL BE PLACED ALONG THE NORTH BOUNDARY OF TAXIWAY Z AND THE ADJACENT APRON PRIOR TO BEGINNING CONSTRUCTION. THE BOUNDARY FOR THE RWA LIMIT IS 44.5 FEET FROM THE CENTERLINE OF TAXIWAY Z. THIS LINE OF BARRICADES WILL BLOCK ENTRY INTO ANY WORK AREA ON THE AIRPORT FOR BASED AIRCRAFT. ALL AIRFIELD LIGHTS AND NAVIGATIONAL AND VISUAL AIDS WILL BE DECOMMISSIONED EACH NIGHT DURING CONSTRUCTION HOURS.
- LEAD TIMES FOR REQUIRED NOTIFICATION - FIVE (5) WORKING DAYS

LEGEND

- MICROSURFACING
- CONTRACTOR ACCESS
- LOW PROFILE BARRICADES

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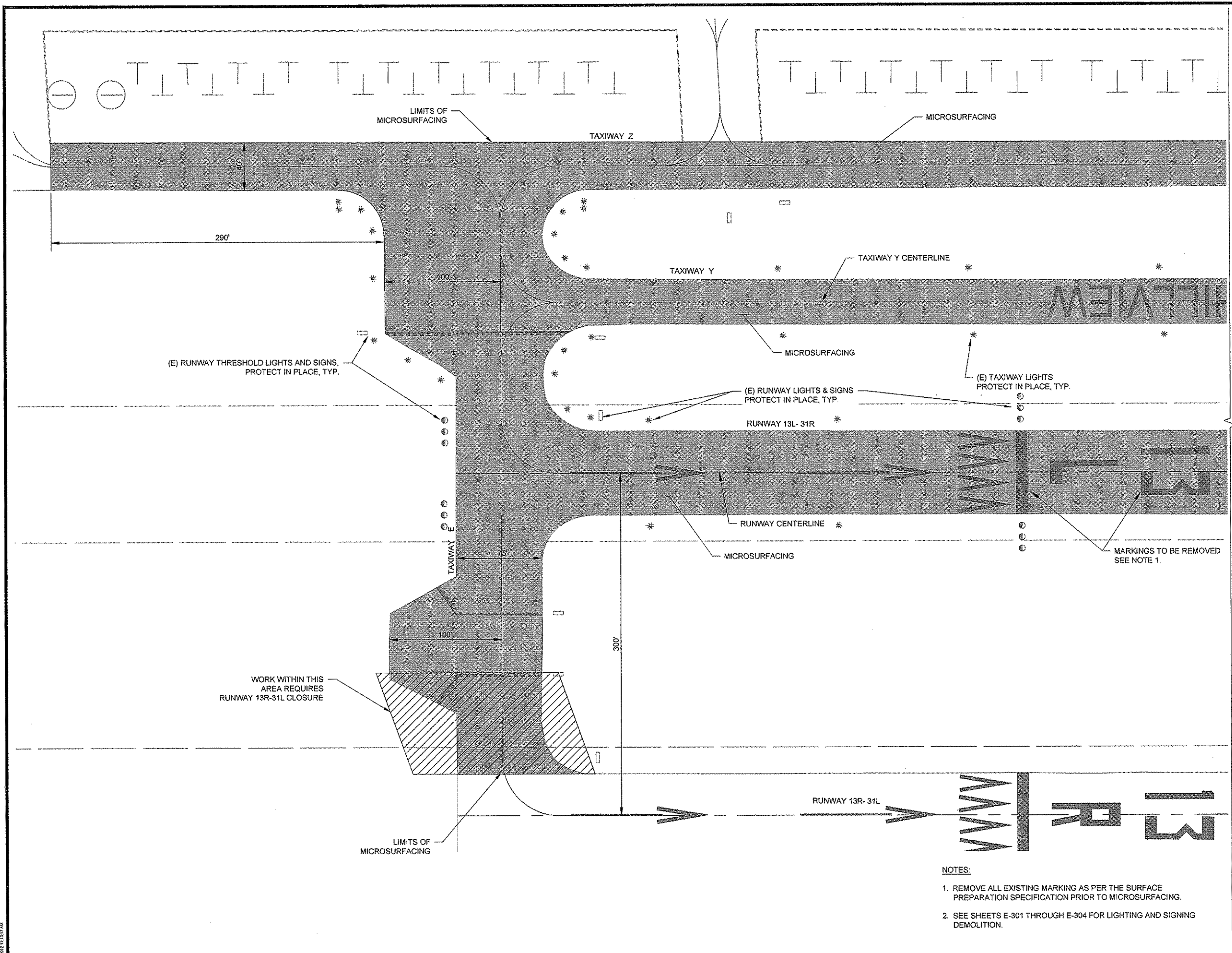
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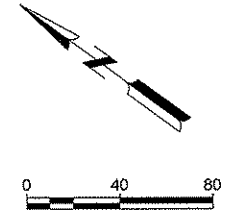
SHEET CONTENTS
CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 4

SHEET NO. 1 of 15

20120508 10:45 AM C:\PROJECTS\REID-HILL VIEW AIRPORT\G-084\G-084.dwg
 20120508 11:29 AM



SEE SHEET C-202
MATCHLINE



LEGEND
 AREA OF MICROSURFACING

- NOTES:**
1. REMOVE ALL EXISTING MARKING AS PER THE SURFACE PREPARATION SPECIFICATION PRIOR TO MICROSURFACING.
 2. SEE SHEETS E-301 THROUGH E-304 FOR LIGHTING AND SIGNING DEMOLITION.

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**REID-HILLVIEW AIRPORT
 REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
 CONNECTOR TAXIWAYS**

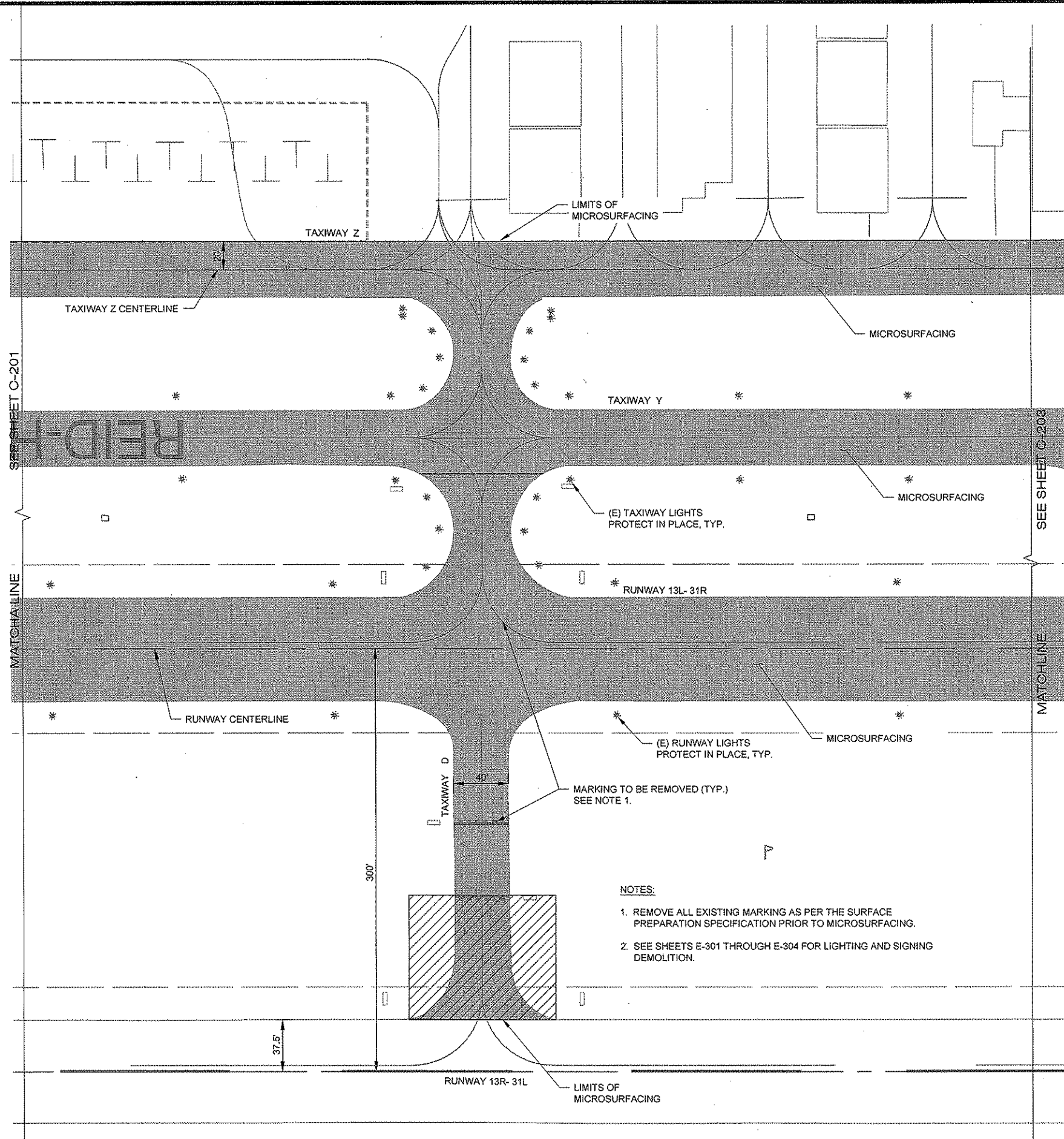
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LIMITS OF
MICROSURFACING

TAXIWAY Z

TAXIWAY Z CENTERLINE

MICROSURFACING

TAXIWAY Y

(E) TAXIWAY LIGHTS
PROTECT IN PLACE, TYP.

MICROSURFACING

RUNWAY 13L-31R

SEE SHEET C-201

SEE SHEET C-203

MATCHLINE

MATCHLINE

RUNWAY CENTERLINE

(E) RUNWAY LIGHTS
PROTECT IN PLACE, TYP.

MICROSURFACING

MARKING TO BE REMOVED (TYP.)
SEE NOTE 1.

TAXIWAY D

300'

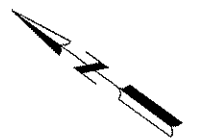
37.5'

RUNWAY 13R-31L

LIMITS OF
MICROSURFACING

NOTES:

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2. SEE SHEETS E-301 THROUGH E-304 FOR LIGHTING AND SIGNING DEMOLITION.

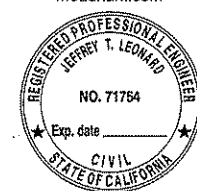


LEGEND

■ AREA OF
MICROSURFACING

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**REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
CONNECTOR TAXIWAYS**

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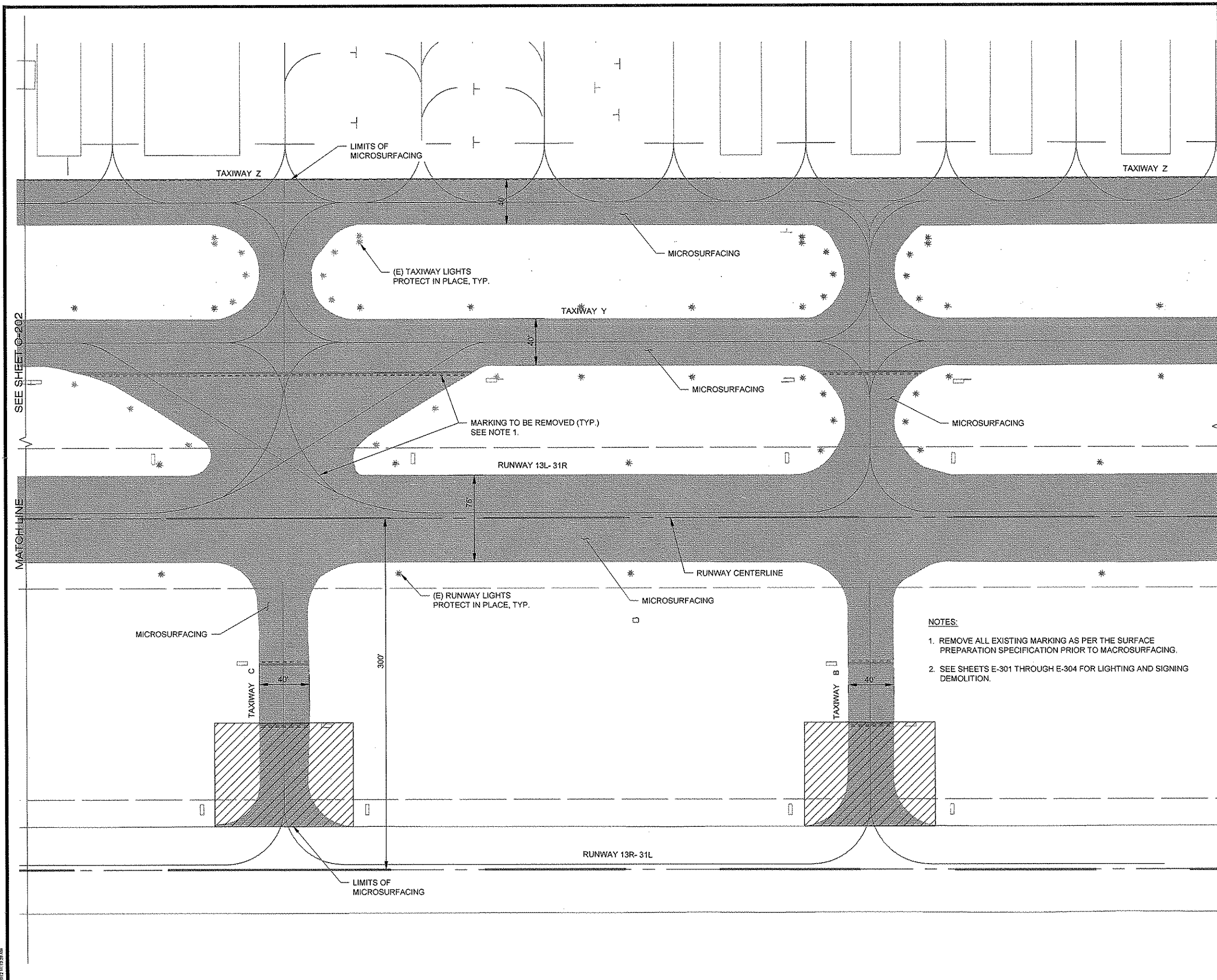
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SHEET CONTENTS
ASPHALT PAVEMENT
REHABILITATION PLAN

SHEET NO. 5 of 15

C-202

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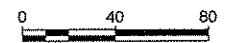
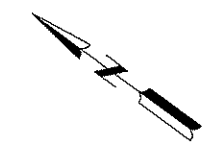
SEE SHEET C-204

MATCHLINE

MATCHLINE

NOTES:

1. REMOVE ALL EXISTING MARKING AS PER THE SURFACE PREPARATION SPECIFICATION PRIOR TO MACROSURFACING.
2. SEE SHEETS E-301 THROUGH E-304 FOR LIGHTING AND SIGNING DEMOLITION.



LEGEND

AREA OF MICROSURFACING

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**REID-HILLVIEW AIRPORT
 REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
 CONNECTOR TAXIWAYS**

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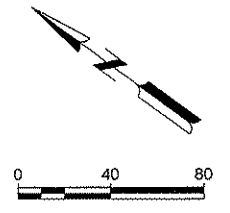
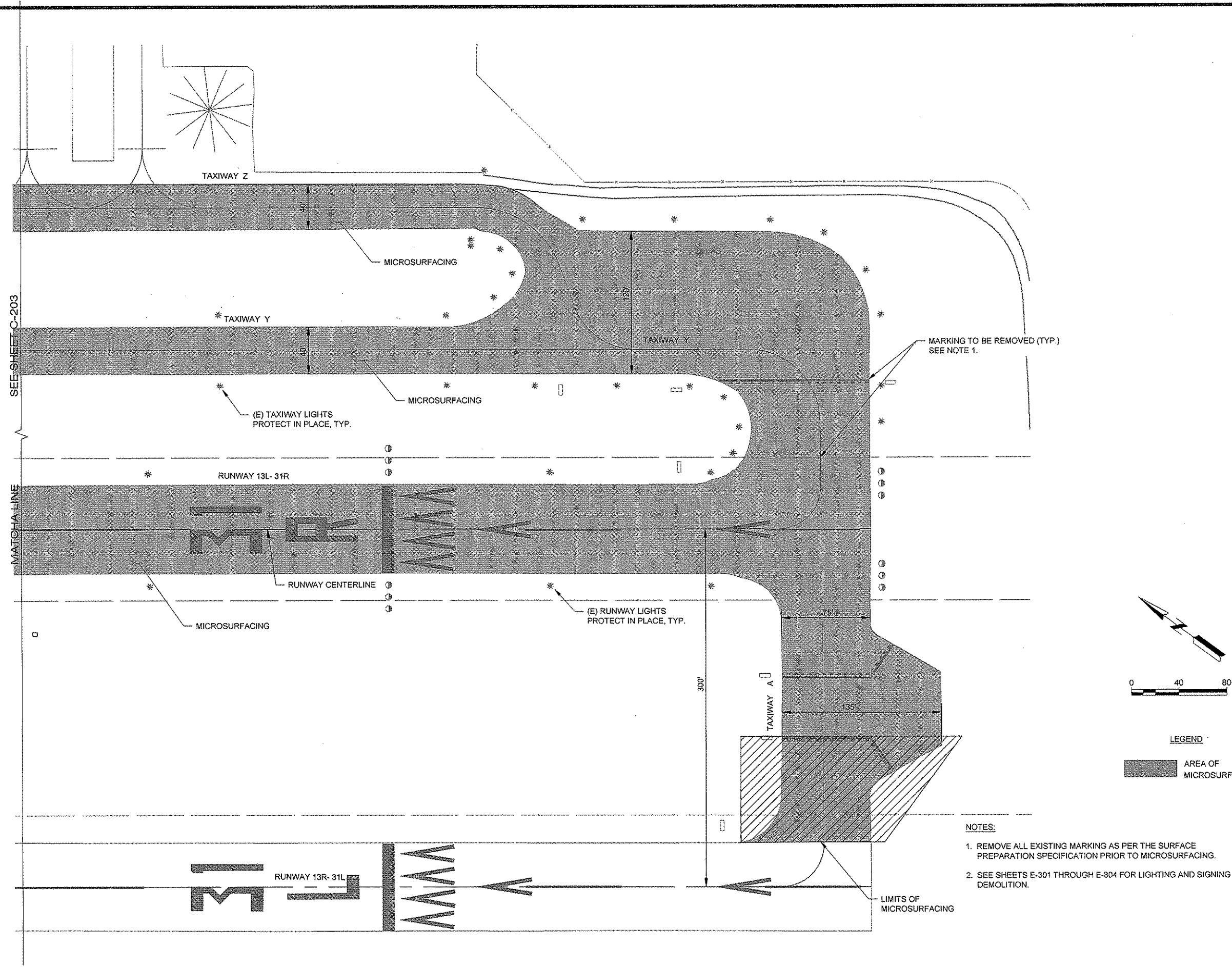
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SHEET CONTENTS
 ASPHALT PAVEMENT
 REHABILITATION PLAN

SHEET NO. 6 of 15

C-203

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LEGEND

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| [Shaded Area] | AREA OF MICROSURFACING |
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NOTES:

1. REMOVE ALL EXISTING MARKING AS PER THE SURFACE PREPARATION SPECIFICATION PRIOR TO MICROSURFACING.
2. SEE SHEETS E-301 THROUGH E-304 FOR LIGHTING AND SIGNING DEMOLITION.

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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

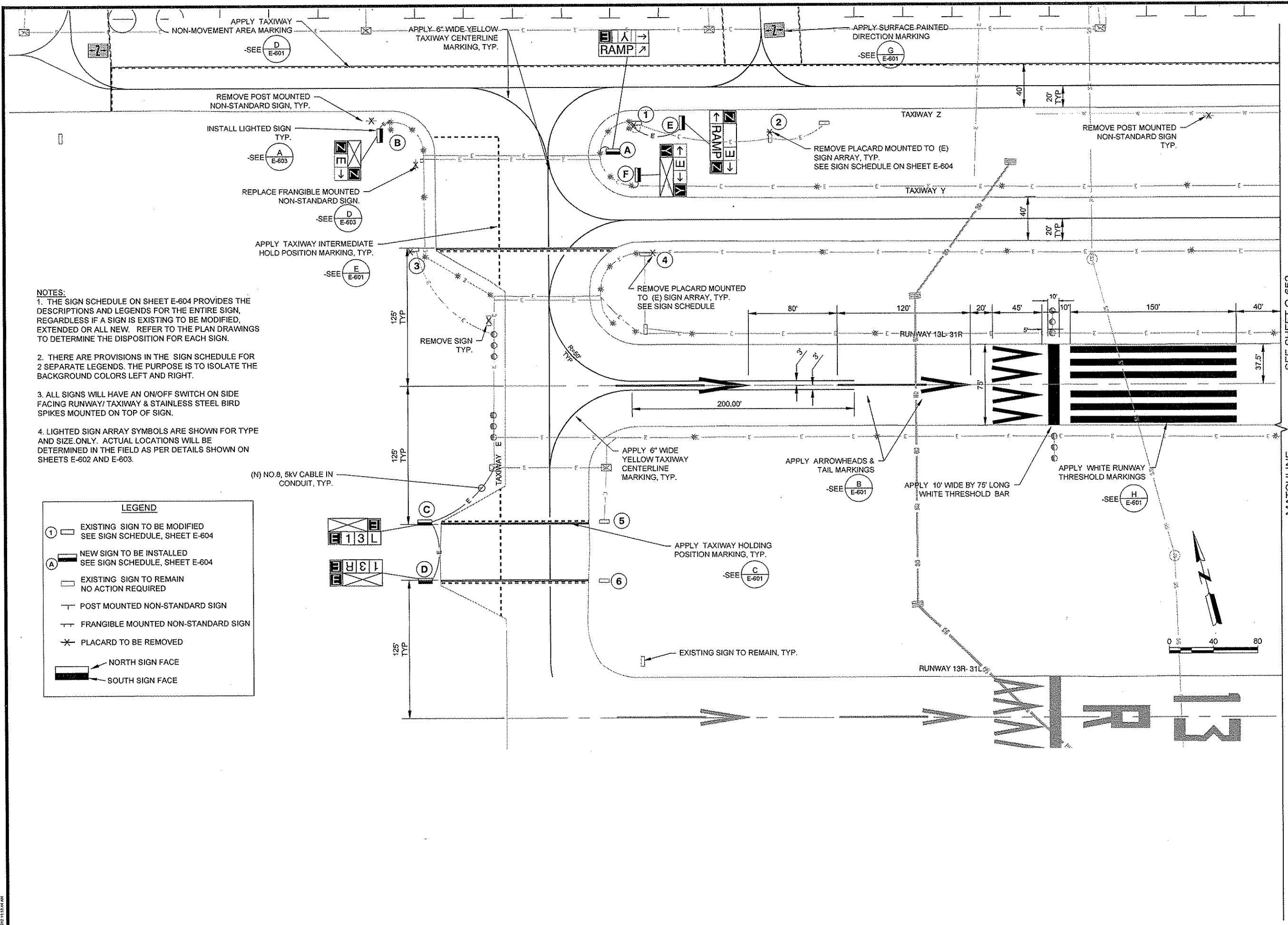
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SHEET CONTENTS
 ASPHALT PAVEMENT
 REHABILITATION PLAN

SHEET NO. 7 of 15
C-204



NOTES:

1. THE SIGN SCHEDULE ON SHEET E-604 PROVIDES THE DESCRIPTIONS AND LEGENDS FOR THE ENTIRE SIGN, REGARDLESS IF A SIGN IS EXISTING TO BE MODIFIED, EXTENDED OR ALL NEW. REFER TO THE PLAN DRAWINGS TO DETERMINE THE DISPOSITION FOR EACH SIGN.
2. THERE ARE PROVISIONS IN THE SIGN SCHEDULE FOR 2 SEPARATE LEGENDS. THE PURPOSE IS TO ISOLATE THE BACKGROUND COLORS LEFT AND RIGHT.
3. ALL SIGNS WILL HAVE AN ON/OFF SWITCH ON SIDE FACING RUNWAY/ TAXIWAY & STAINLESS STEEL BIRD SPIKES MOUNTED ON TOP OF SIGN.
4. LIGHTED SIGN ARRAY SYMBOLS ARE SHOWN FOR TYPE AND SIZE ONLY. ACTUAL LOCATIONS WILL BE DETERMINED IN THE FIELD AS PER DETAILS SHOWN ON SHEETS E-602 AND E-603.

| LEGEND | |
|--------|--|
| ① | EXISTING SIGN TO BE MODIFIED SEE SIGN SCHEDULE, SHEET E-604 |
| Ⓐ | NEW SIGN TO BE INSTALLED SEE SIGN SCHEDULE, SHEET E-604 |
| □ | EXISTING SIGN TO REMAIN NO ACTION REQUIRED |
| ⊥ | POST MOUNTED NON-STANDARD SIGN |
| ⊥ | FRANGIBLE MOUNTED NON-STANDARD SIGN |
| ✱ | PLACARD TO BE REMOVED |
| ← | NORTH SIGN FACE |
| → | SOUTH SIGN FACE |



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**REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
CONNECTOR TAXIWAYS**

REVISION

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SHEET CONTENTS
SIGNING AND MARKING PLAN



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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
CONNECTOR TAXIWAYS

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SHEET CONTENTS
SIGNING AND MARKING PLAN

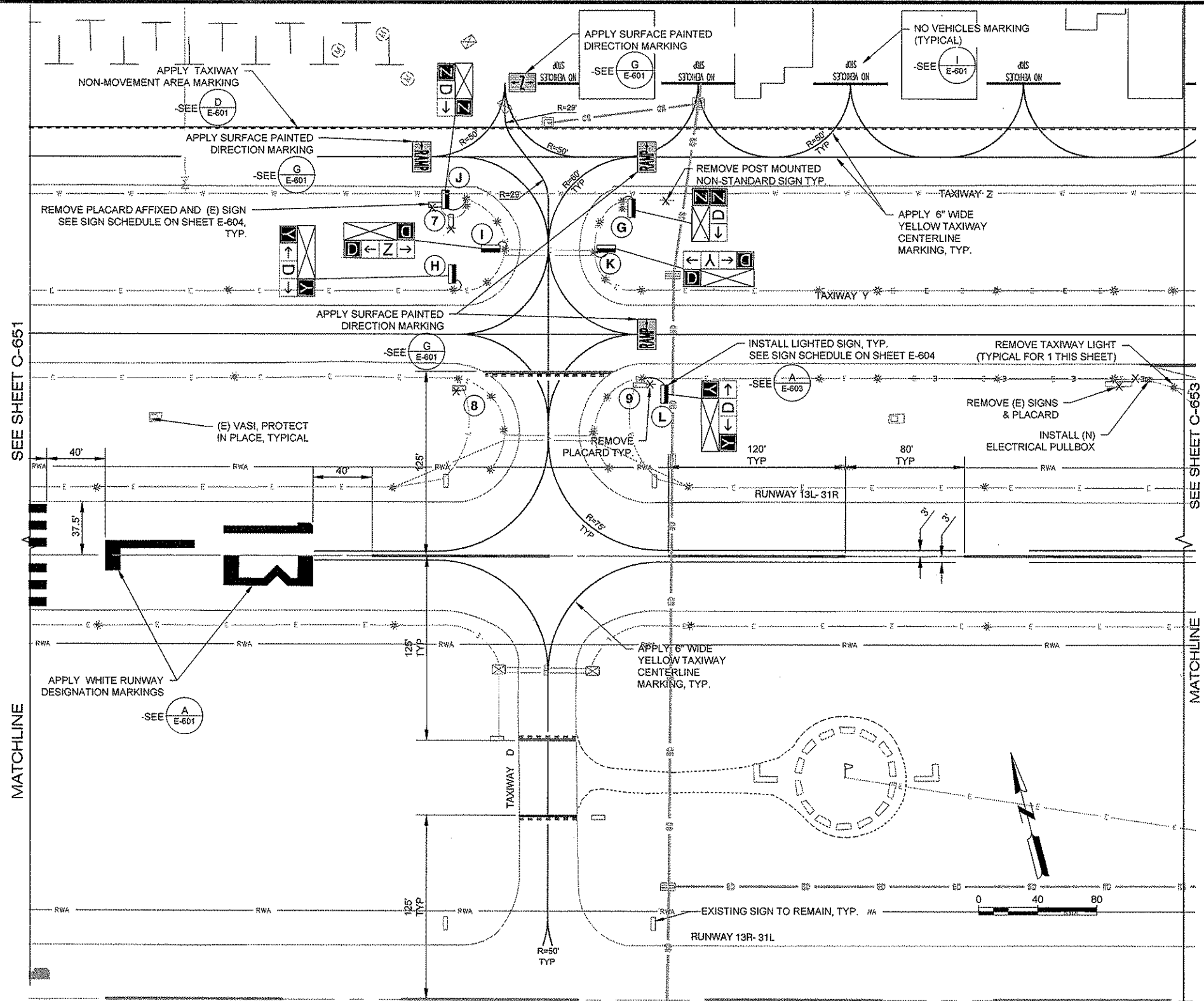
SHEET NO. 9 of 15

E-302

- NOTES:**
1. THE SIGN SCHEDULE ON SHEET E-604 PROVIDES THE DESCRIPTIONS AND LEGENDS FOR THE ENTIRE SIGN, REGARDLESS IF A SIGN IS EXISTING TO BE MODIFIED, EXTENDED OR ALL NEW. REFER TO THE PLAN DRAWINGS TO DETERMINE THE DISPOSITION FOR EACH SIGN.
 2. THERE ARE PROVISIONS IN THE SIGN SCHEDULE FOR 2 SEPARATE LEGENDS. THE PURPOSE IS TO ISOLATE THE BACKGROUND COLORS LEFT AND RIGHT.
 3. ALL SIGNS WILL HAVE AN ON/OFF SWITCH ON SIDE FACING RUNWAY/TAXIWAY & STAINLESS STEEL BIRD SPIKES MOUNTED ON TOP OF SIGN.
 4. LIGHTED SIGN ARRAY SYMBOLS ARE SHOWN FOR TYPE AND SIZE ONLY. ACTUAL LOCATIONS WILL BE DETERMINED IN THE FIELD AS PER DETAILS SHOWN ON SHEET E-602 AND E-603.

LEGEND

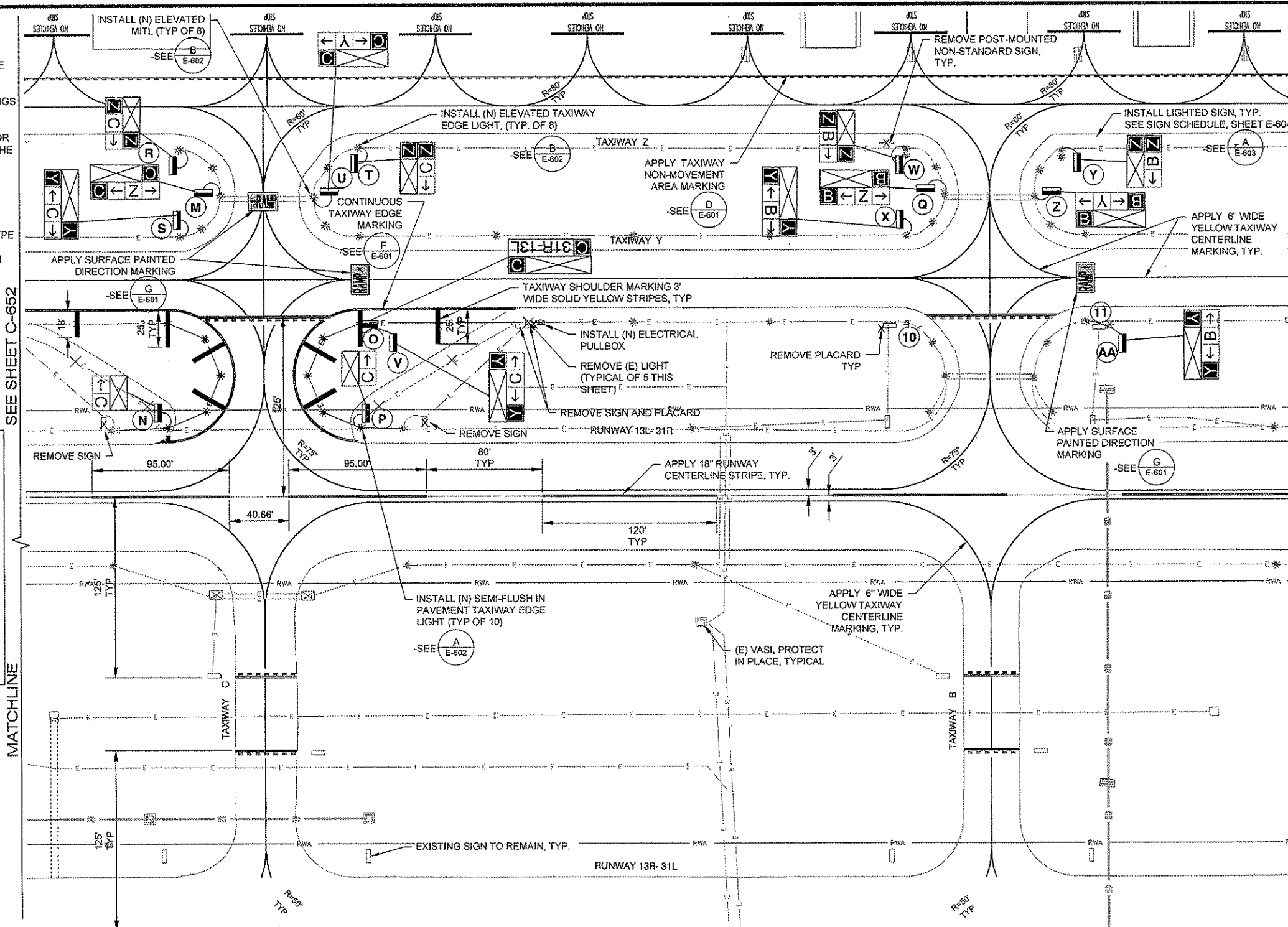
- ① EXISTING SIGN TO BE MODIFIED
SEE SIGN SCHEDULE, SHEET E-604
- A NEW SIGN TO BE INSTALLED
SEE SIGN SCHEDULE, SHEET E-604
- EXISTING SIGN TO REMAIN
NO ACTION REQUIRED
- POST MOUNTED NON-STANDARD SIGN
- FRANGIBLE MOUNTED NON-STANDARD SIGN
- ✕ PLACARD TO BE REMOVED
- ← NORTH SIGN FACE
- SOUTH SIGN FACE



- NOTES:**
1. THE SIGN SCHEDULE ON SHEET E-604 PROVIDES THE DESCRIPTIONS AND LEGENDS FOR THE ENTIRE SIGN, REGARDLESS IF A SIGN IS EXISTING TO BE MODIFIED, EXTENDED OR ALL NEW. REFER TO THE PLAN DRAWINGS TO DETERMINE THE DISPOSITION FOR EACH SIGN.
 2. THERE ARE PROVISIONS IN THE SIGN SCHEDULE FOR 2 SEPARATE LEGENDS. THE PURPOSE IS TO ISOLATE THE BACKGROUND COLORS LEFT AND RIGHT.
 3. ALL SIGNS WILL HAVE AN ON/OFF SWITCH ON SIDE FACING RUNWAY/ TAXIWAY & STAINLESS STEEL BIRD SPIKES MOUNTED ON TOP OF SIGN.
 4. LIGHTED SIGN ARRAY SYMBOLS ARE SHOWN FOR TYPE AND SIZE ONLY. ACTUAL LOCATIONS WILL BE DETERMINED IN THE FIELD AS PER DETAILS SHOWN ON SHEET E-602 AND E-603.

LEGEND

- ① EXISTING SIGN TO BE MODIFIED
SEE SIGN SCHEDULE, SHEET E-604
- Ⓐ NEW SIGN TO BE INSTALLED
SEE SIGN SCHEDULE, SHEET E-604
- EXISTING SIGN TO REMAIN
NO ACTION REQUIRED
- ⊥ POST MOUNTED NON-STANDARD SIGN
- ⊥ FRANGIBLE MOUNTED NON-STANDARD SIGN
- ✱ PLACARD TO BE REMOVED
- ← NORTH SIGN FACE
- SOUTH SIGN FACE

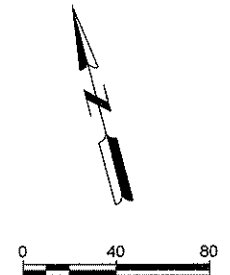


SEE SHEET C-652

SEE SHEET C-654

MATCHLINE

MATCHLINE



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 133 Aviation Boulevard,
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 phone: 707-526-6010
 meadhunt.com

REGISTERED PROFESSIONAL ENGINEER
 JEFFREY T. LEONARD
 NO. 71754
 Exp. date _____
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**REID-HILLVIEW AIRPORT
 REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
 CONNECTOR TAXIWAYS**

REVISION

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AIP NO: 3-06-0225-14
 ASN NO: R323300-115095.01
 DATE: MAY 2012
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 DRAWN BY: BE
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SHEET CONTENTS

SIGNING AND MARKING PLAN



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NOTES:

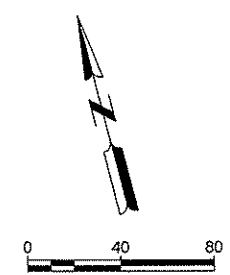
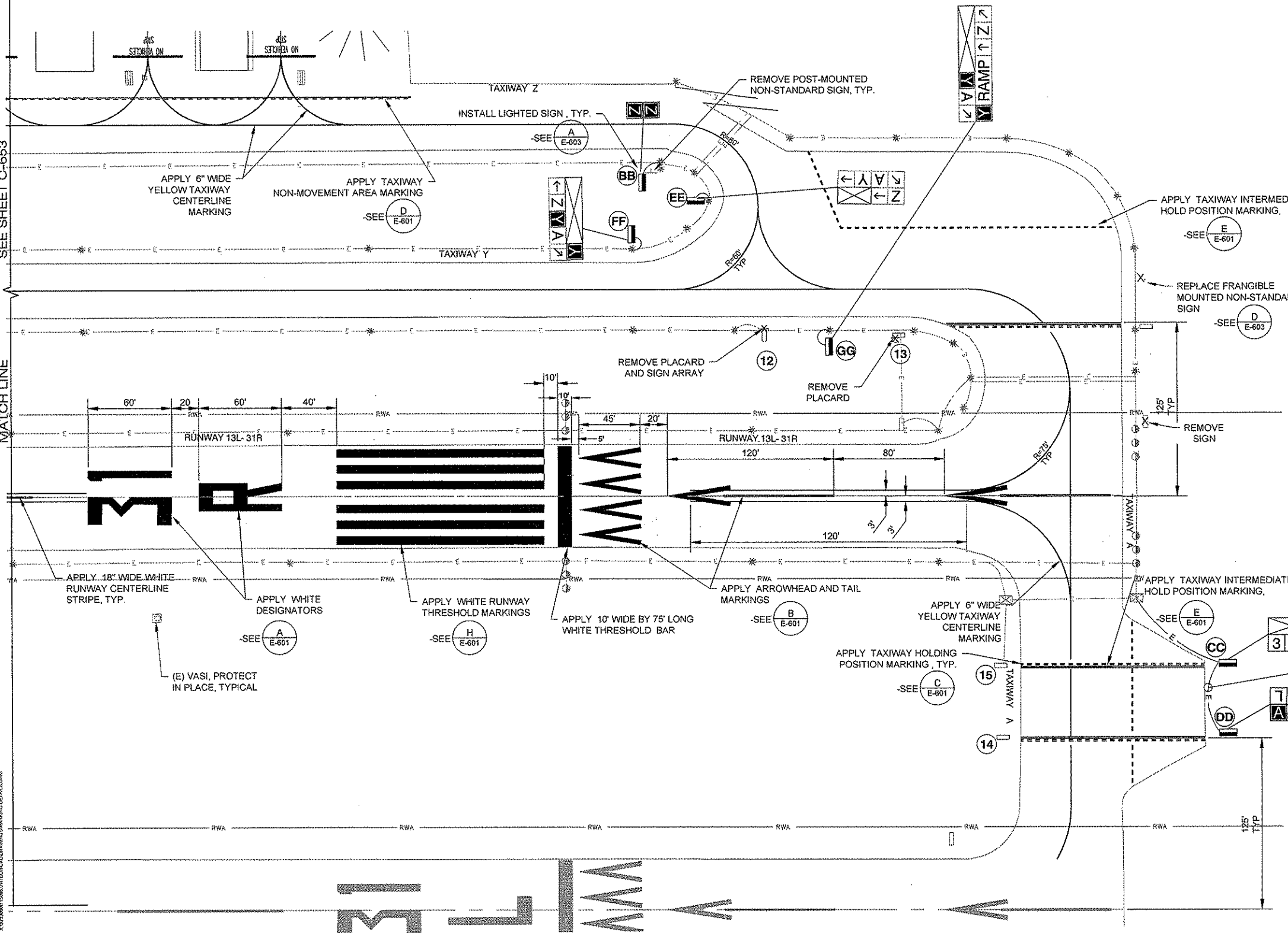
1. THE SIGN SCHEDULE ON SHEET E-604 PROVIDES THE DESCRIPTIONS AND LEGENDS FOR THE ENTIRE SIGN, REGARDLESS IF A SIGN IS EXISTING TO BE MODIFIED, EXTENDED OR ALL NEW. REFER TO THE PLAN DRAWINGS TO DETERMINE THE DISPOSITION FOR EACH SIGN.
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| LEGEND | |
|--------|--|
| ① | EXISTING SIGN TO BE MODIFIED SEE SIGN SCHEDULE, SHEET E-604 |
| Ⓐ | NEW SIGN TO BE INSTALLED SEE SIGN SCHEDULE, SHEET E-604 |
| □ | EXISTING SIGN TO REMAIN NO ACTION REQUIRED |
| ⊥ | POST MOUNTED NON-STANDARD SIGN |
| ⊥⊥ | FRANGIBLE MOUNTED NON-STANDARD SIGN |
| ✕ | PLACARD TO BE REMOVED |
| ◀ | NORTH SIGN FACE |
| ▶ | SOUTH SIGN FACE |

SEE SHEET C-653

MATCH LINE

3/22/2012 11:12:23 AM
K:\PROJECTS\2012\05\REID-HILLVIEW AIRPORT\DRAWINGS\DESIGN\DESIGN



**REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, &
CONNECTOR TAXIWAYS**

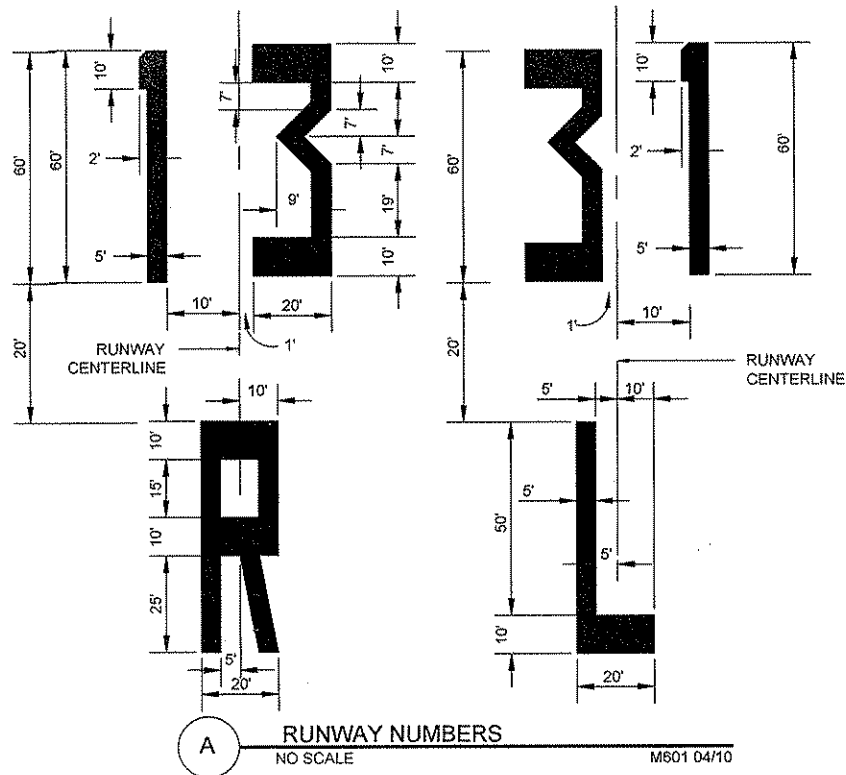
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APP NO: 3-05-0225-14
MSH NO: R323300-115095.01
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CHECKED BY: JTL
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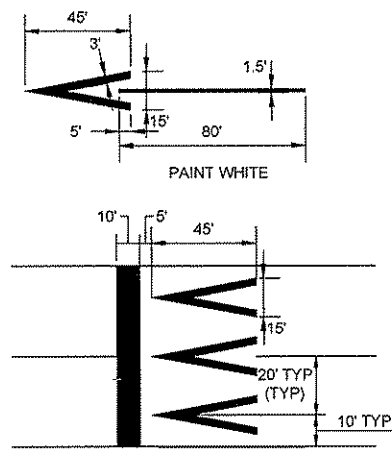
SHEET CONTENTS
SIGNING AND MARKING PLAN

SHEET NO. 11 of 15

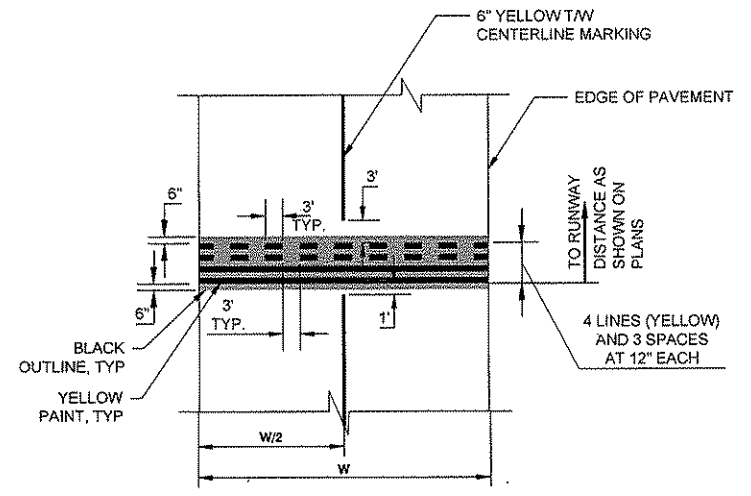
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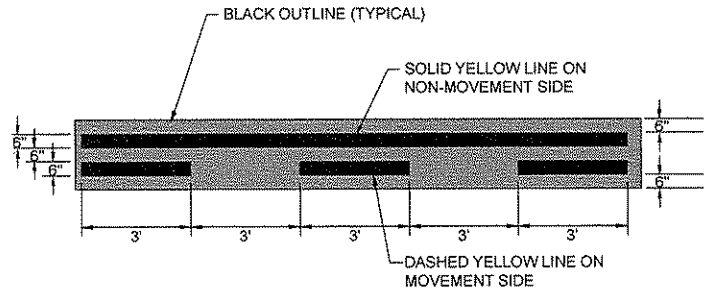
A RUNWAY NUMBERS
NO SCALE M601 04/10



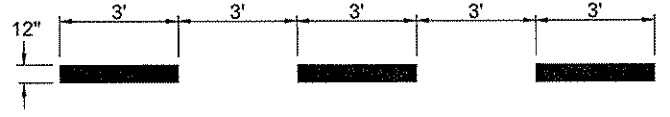
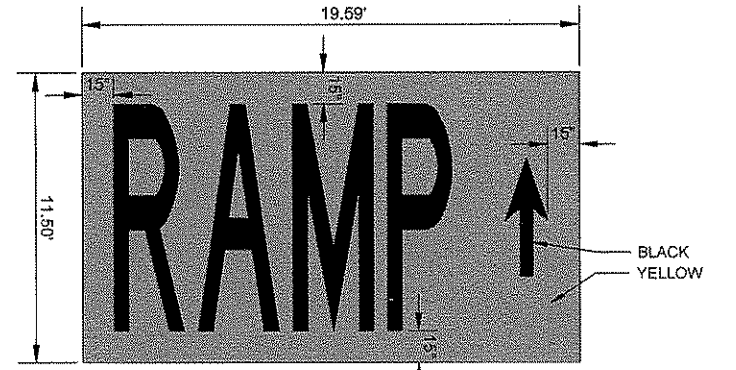
B ARROWHEADS AND TAIL MARKING
NO SCALE M205 05/10



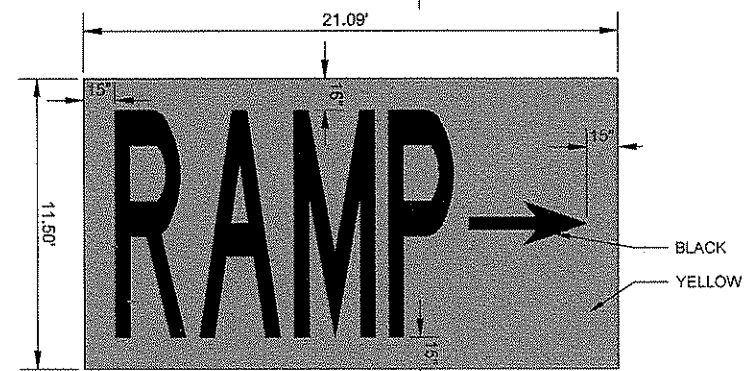
C TAXIWAY HOLD LINE DETAIL
NO SCALE M302 05/10



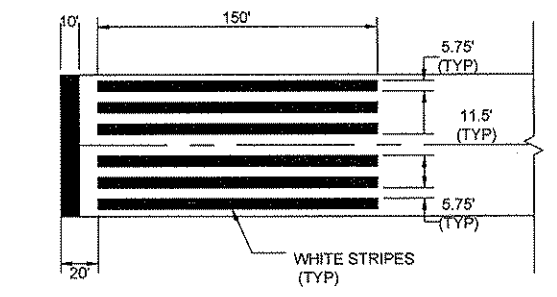
D NON-MOVEMENT AREA MARKING
NO SCALE M306 04/10



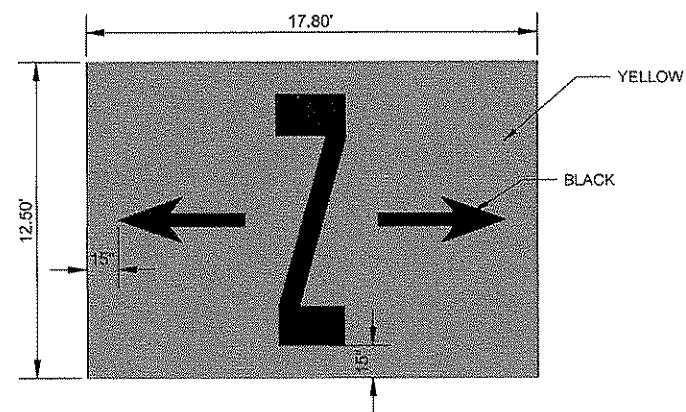
E INTERMEDIATE HOLDING POSITION MARKING
NO SCALE



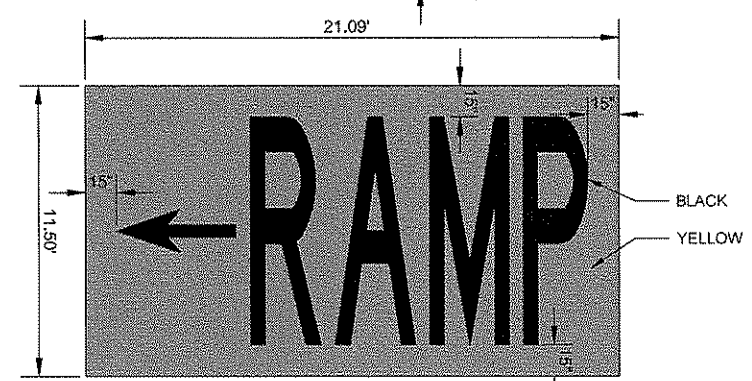
F CONTINUOUS DOUBLE TAXIWAY EDGE MARKING
NO SCALE M306 04/10



H THRESHOLD MARKING
NO SCALE M207A 05/10



G SURFACE PAINTED DIRECTION MARKING
NO SCALE M306 04/10



I STOP NO VEHICLES MARKING
NO SCALE M306 04/10

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SERPENT T. LEONARD
NO. 71754
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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

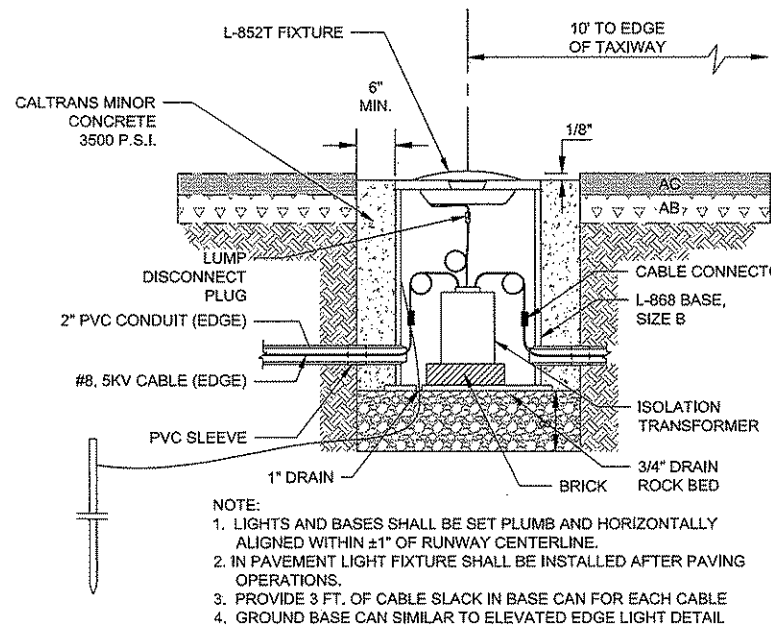
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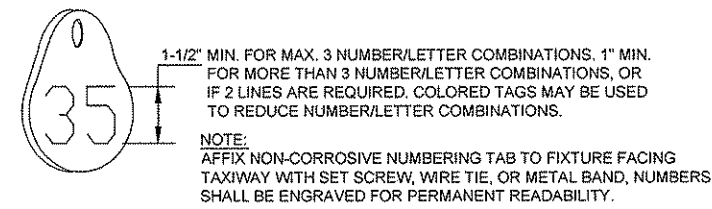
APP NO: 3-06-0225-14
 M&H NO: R323300-115095.01
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SHEET CONTENTS
MARKING DETAILS

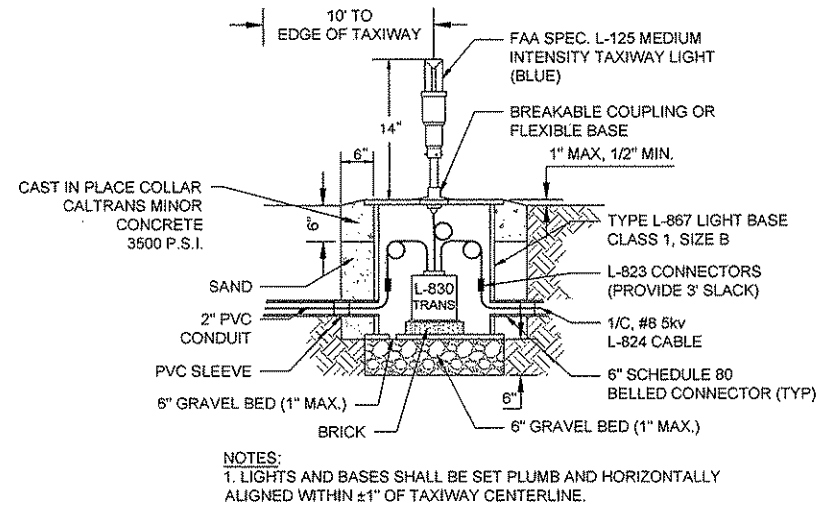
COURTESY OF THE CALIFORNIA HIGHWAY PATROL
 COURTESY OF THE CALIFORNIA HIGHWAY PATROL



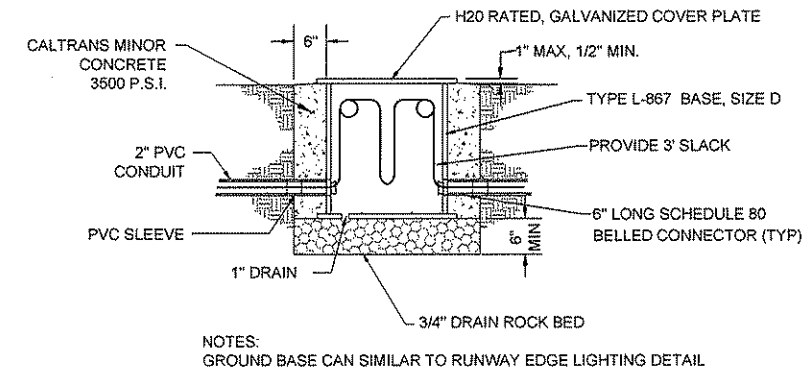
A SEMI-FLUSH IN PAVEMENT TAXIWAY EDGE LIGHT
NO SCALE



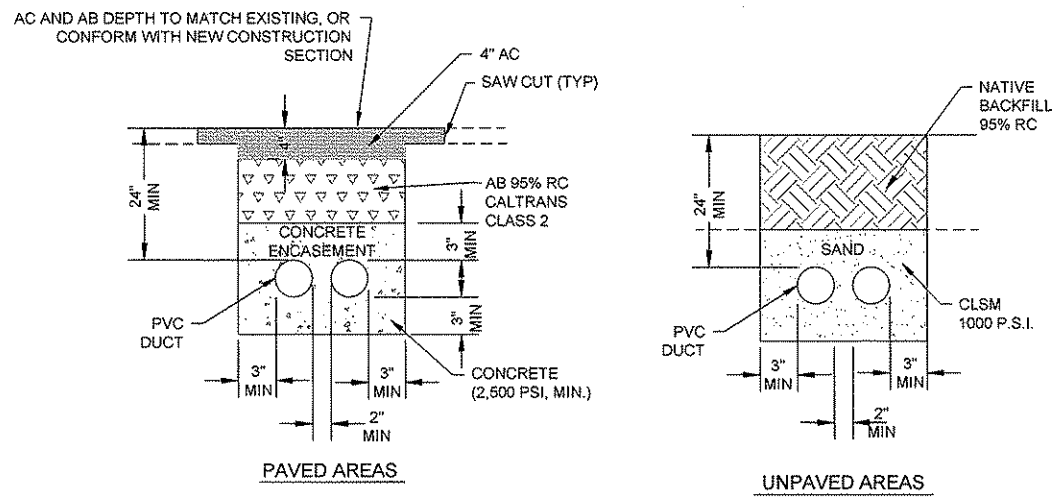
ELEVATED LIGHT NUMBERING FIXTURE



B ELEVATED TAXIWAY EDGE LIGHT INSTALLATION
NO SCALE E516A 09/09

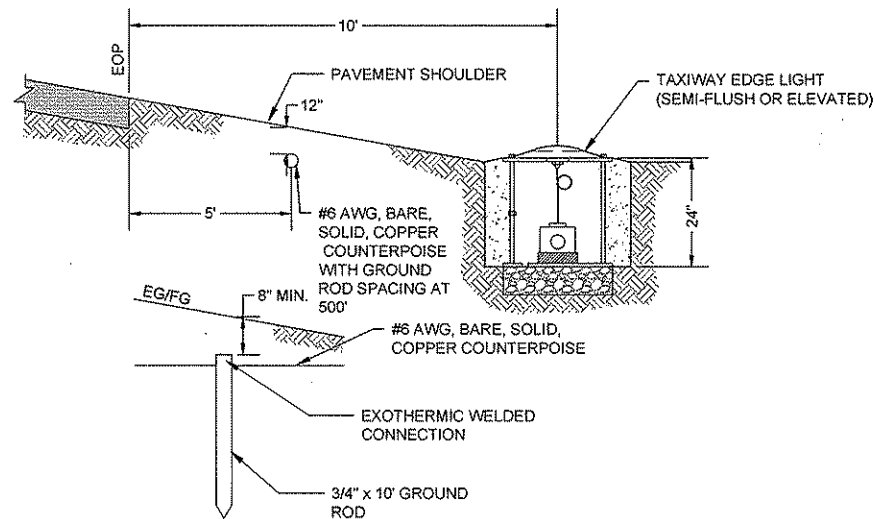


C PULL CAN / JUNCTION CAN
NO SCALE



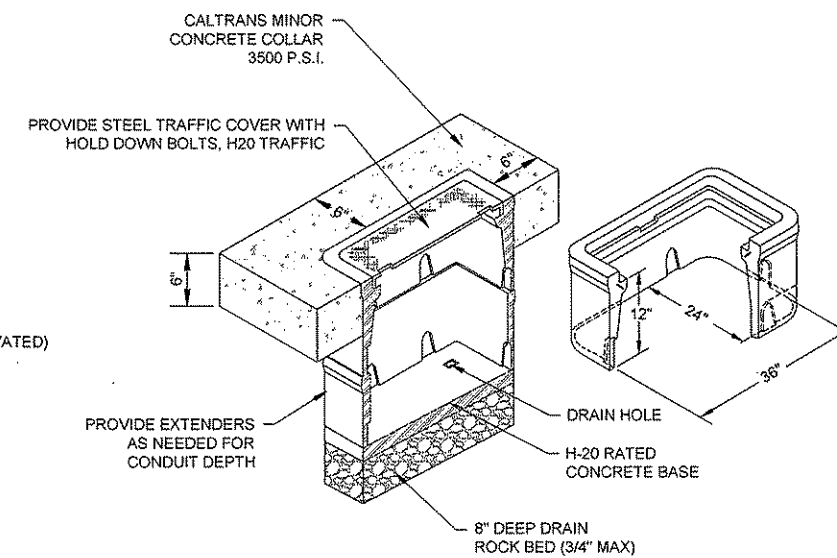
- NOTES:**
1. WHEN STACKING DUCT IN A TRENCH, THE HORIZONTAL CLEARANCE BETWEEN CONDUITS MUST BE A MINIMUM OF 2\".
 2. DUCT UNDER NEW PAVEMENT SHALL BE INSTALLED BEFORE THE PAVEMENT IS CONSTRUCTED.
 3. CONCRETE ENCASED CONDUIT SHALL EXTEND A MIN. OF 5 FT. BEYOND ALL EDGES OF PAVEMENT.

E PVC DUCT TRENCHING AND BACKFILL
NO SCALE



- NOTES:**
1. FOR CONDUITS NOT PARALLEL TO PAVEMENT, INSTALL COUNTERPOISE ABOVE CONDUITS IN SAME TRENCH PER SPECIFICATION REQUIREMENTS.

E COUNTERPOISE INSTALLATION DETAIL
NO SCALE



F ELECTRICAL PULLBOX
NO SCALE

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REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

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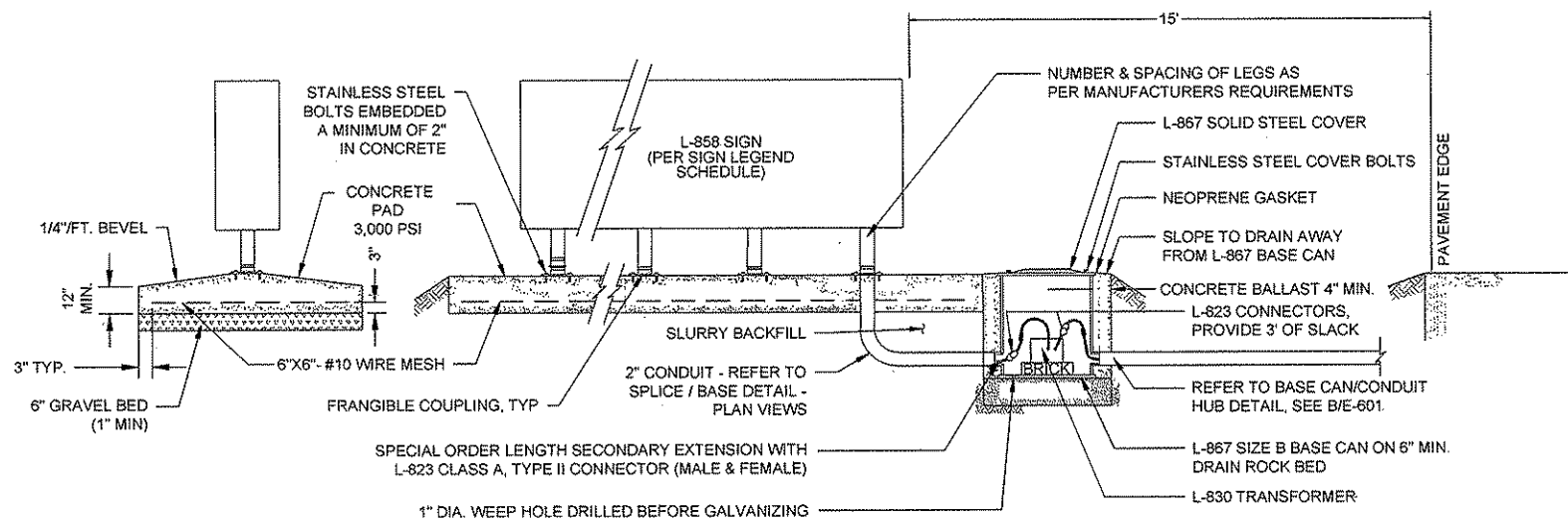
APP NO.: 3-08-0225-14
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 DATE: MAY 2012
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SHEET CONTENTS
ELECTRICAL AND SIGNING DETAILS

SHEET NO. 13 of 15

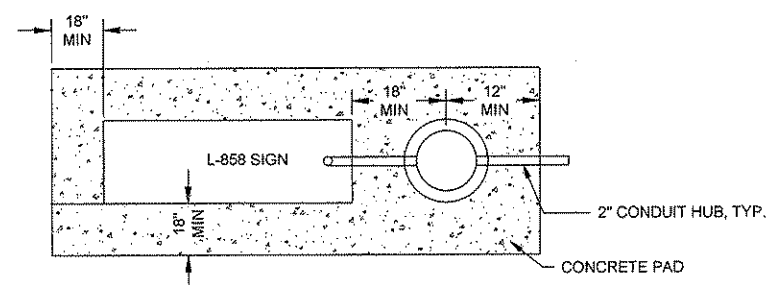
E-602

24032300115085.01 E-602 CONDUIT AND PULLBOX DETAILS 5/22/2012 11:14 AM



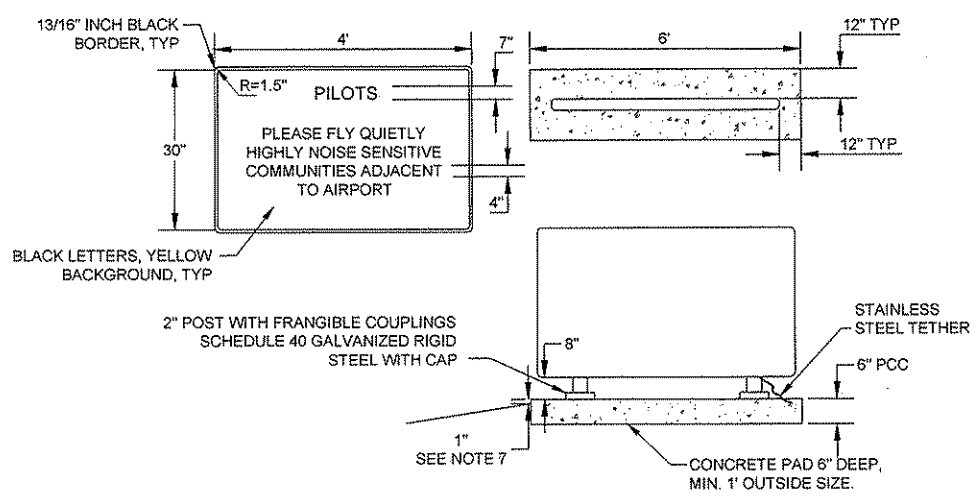
- GENERAL SIGN NOTES:**
1. FOLLOW MANUFACTURER'S RECOMMENDATION FOR L-823 CONNECTOR INSTALLATION.
 2. COUPLING WEEP HOLES SHALL NOT DRAIN TO THE OUTSIDE OF THE L-867 BASE CAN OR LID.
 3. PROVIDE ONE TETHER MINIMUM PER SIGN.
 4. INSTALL GROUND WIRE AND GROUND ROD TO INTERIOR LUG ON L-867 BASE CAN.

A TYPICAL LIGHTED SIGN DETAIL
NO SCALE



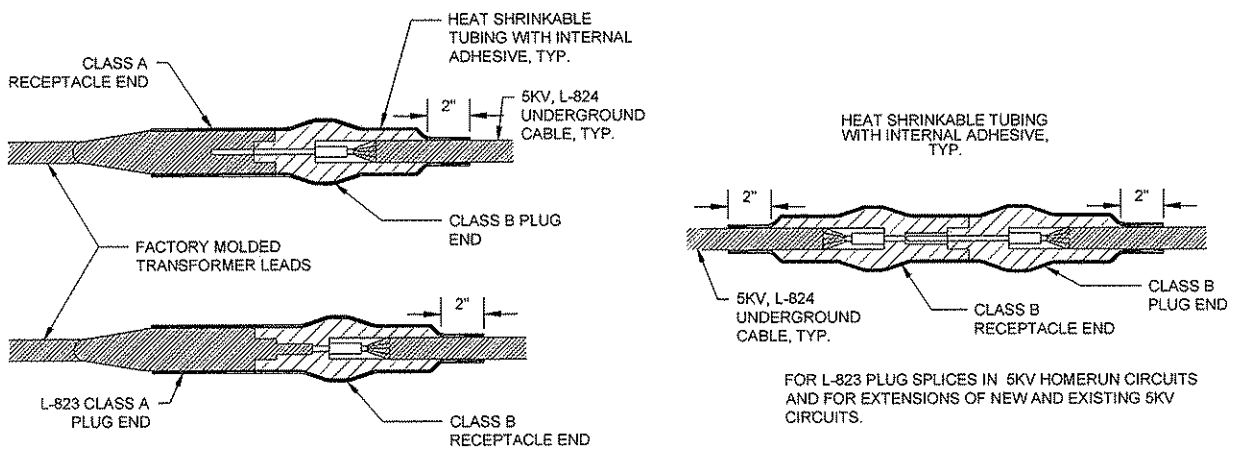
NOTE: SIGN PADS SHALL BE PRECAST PRIOR TO BEGINNING GUIDANCE SIGN INSTALLATION

B PRECAST SIGN PAD DIMENSIONS
NO SCALE



- NOTES:**
1. ALL SIGN MATERIAL AND CONSTRUCTION PER MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 2. ALL MATERIALS SHALL HAVE REFLECTIVE PROPERTIES.
 3. ALL SIGNS SHALL BE 0.080 SHEET ALUMINUM.
 4. THE SIGN/PIPE CONNECTION SHALL BE VIA U-BOLTS AS APPROVED BY THE ENGINEER.
 5. CONTRACTOR SHALL INSTALL STAINLESS STEEL TETHER AS APPROVED BY THE ENGINEER.
 6. CONCRETE SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 2500 PSI.
 7. TOP OF CONCRETE PAD SHALL BE 1\"/>

D FRANGIBLE MOUNTED UNLIGHTED SIGN
NO SCALE



FOR L-823 CONNECTORS USED AT L-830 ISOLATION TRANSFORMER JUNCTION WITH 5KV LOOP CIRCUIT.
FIGURE A

FIGURE B

NOTE:
TAPED CONNECTIONS, PER SPECIFICATION L-108 SHALL BE USED FOR SPLICES TO LIGHT FIXTURES IN L-868 BASE CANS. ALL SPLICES IN MANHOLES, IF ALLOWED, SHALL BE HEAT SHRINK TYPE OR CAST TYPE, PER FIGURE A & B.

C CABLE SPLICE WATERPROOFING DETAILS
NO SCALE

- GENERAL ELECTRICAL NOTES:**
1. REFER TO PLAN SHEET C-651 TO C-654 FOR SIGN LOCATIONS AND SIGN LEGEND. SEE PROJECT SPECIFICATIONS FOR TYPE, SIZE, STYLE, AND CLASS OF SIGN.
 2. COLOR CODE WIRE IDENTIFICATION TAPE AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE RIGHT IS CODED BLUE.
 3. NON-METALLIC COMPONENTS IN THE LIGHT FIXTURE STEMS & BREAKABLE COUPLINGS ARE NOT PERMITTED.
 4. L-830 TRANSFORMERS SHALL BE SIZED AS REQUIRED BY THE MANUFACTURER.
 5. PROVIDE 3 FEET OF SLACK IN EACH 5KV CABLE IN BASE CAN SO THE ISOLATION TRANSFORMER CONNECTION CAN BE MADE ABOVE GRADE.
 6. ALL FINISHED GRADES SHALL BE FLUSH WITH CONCRETE SLABS AND BALLASTS.
 7. ALL HARDWARE SHALL BE STAINLESS STEEL. ALL MACHINE THREADED CONNECTIONS SHALL HAVE ANTI-SEIZING COMPOUND.
 8. CHAMFER ALL CONCRETE EDGES.

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CONNECTOR TAXIWAYS

REVISION

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SHEET CONTENTS
ELECTRICAL AND
SIGNING DETAILS

SHEET NO. 14 of 15

E-603

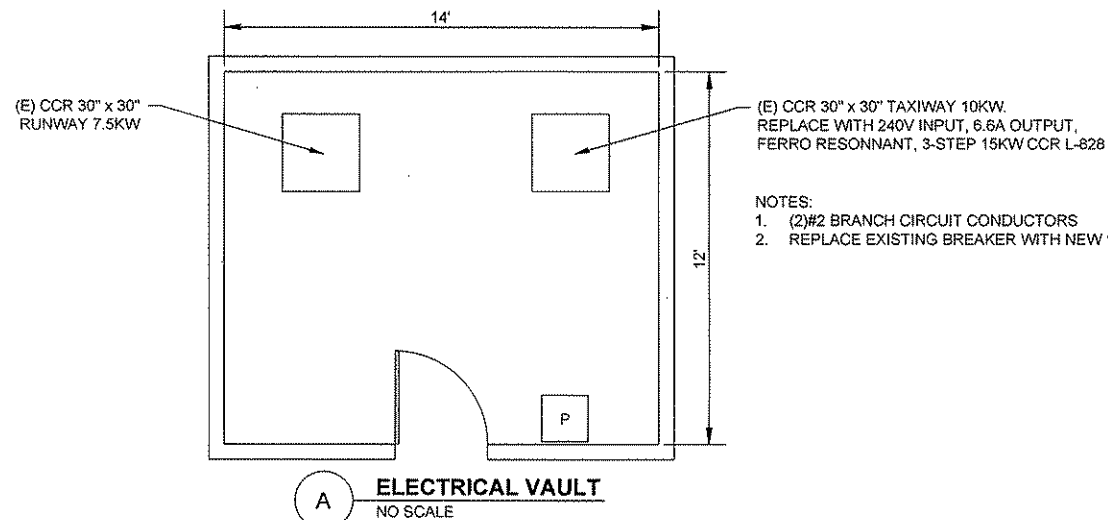
13/03/2011 11:58 AM E:\PROJECTS\REID-HILLVIEW AIRPORT\DETAILS\DWG\CONCRETE\FIG A.DWG

SIGN SCHEDULE

| SIGN # | COMMENT | (E) NORTH SIDE | | | | (E) SOUTH SIDE | | | | (N) NORTH SIDE | | | | (N) SOUTH SIDE | | | | | | | |
|--------|---|----------------|-------|----------|-------|----------------|-------|----------|-------|----------------|-------|----------|-------|----------------|--------|----------|--------|----------|-------|----------|-------|
| | | LEGEND 1 | COLOR | LEGEND 2 | COLOR | LEGEND 1 | COLOR | LEGEND 2 | COLOR | LEGEND 1 | COLOR | LEGEND 2 | COLOR | LEGEND 3 | COLOR | LEGEND 1 | COLOR | LEGEND 2 | COLOR | LEGEND 3 | COLOR |
| (1) | REMOVE SIGN ARRAY | | | | | PARKING → | B/Y | | | | | | | | | | | | | | |
| (2) | REMOVE "TWY Z" PLACARD FROM NORTH END OF SIGN | ← G | B/Y | | | G → | B/Y | | | | | | | | | | | | | | |
| (3) | REMOVE "TWY Y" PLACARD FROM SOUTH SIDE | 13L | WR | E | Y/B | | | E | Y/B | | | | | | | | | | | | |
| (4) | REMOVE "TWY Y" PLACARD FROM EAST END | E | Y/B | 13L | WR | E | Y/B | | | | | | | | | | | | | | |
| (5) | REVERSE ORDER OF (E) PANELS - SOUTH | E | Y/B | | | | | E | Y/B | | | | | | E | Y/B | 13L | WR | | | |
| (6) | REVERSE ORDER OF (E) PANELS - NORTH | E | Y/B | 13R | WR | | | | | 13R | WR | E | Y/B | | | | | | | | |
| (A) | NEW LIGHTED SIGN | | | | | | | | | E | Y/B | Y → | B/Y | | RAMP ↗ | B/Y | | | | | |
| (B) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | E → | B/Y | | Z | Y/B | | | | | |
| (C) | NEW LIGHTED SIGN | | | | | | | | | E | Y/B | | | | E | Y/B | 13L | WR | | | |
| (D) | NEW LIGHTED SIGN | | | | | | | | | 13R | WR | E | Y/B | | E | Y/B | | | | | |
| (E) | NEW LIGHTED SIGN | | | | | | | | | ← RAMP | B/Y | Z | Y/B | | ← E | B/Y | Z | Y/B | | | |
| (F) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | | | | Y | Y/B | ← E → | B/Y | | | |
| (7) | REMOVE SIGN ARRAYS (2 TOTAL) | ← 31 - | B/Y | 13 → | B/Y | ← | B/Y | PARKING | B/Y | | | | | | | | | | | | |
| (8) | REMOVE "TWY Y" PLACARD FROM SOUTH SIDE | D | Y/B | 31R-13L | WR | | | D | Y/B | | | | | | D | Y/B | | | | | |
| (9) | REMOVE "PARKING" LEGEND FROM SOUTH SIDE, REMOVE "TWY Y" PLACARD | D | Y/B | 31R-13L | WR | ↑ | B/Y | PARKING | B/Y | | | | | | | | | | | | |
| (G) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | | | | ← D | B/Y | Z | Y/B | | | |
| (H) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | ← D → | B/Y | | Y | Y/B | | | | | |
| (I) | NEW LIGHTED SIGN | | | | | | | | | D | Y/B | ← Y → | B/Y | | D | Y/B | ← Z → | B/Y | | | |
| (J) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | D → | B/Y | | Z | Y/B | | | | | |
| (K) | NEW LIGHTED SIGN | | | | | | | | | D | Y/B | ← Y → | B/Y | | D | Y/B | | | | | |
| (L) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | | | | Y | Y/B | ← D → | B/Y | | | |
| (10) | REMOVE "TWY Y" PLACARD FROM SOUTH SIDE | | | | | | | B | Y/B | | | | | | B | Y/B | | | | | |
| (11) | REMOVE "TWY Y" PLACARD FROM EAST END, REMOVE "PARKING" LEGEND ON SOUTH SIDE | B | Y/B | 31R-13L | WR | ← | B/Y | PARKING | B/Y | | | | | | | | | | | | |
| (M) | NEW LIGHTED SIGN | | | | | | | | | C | Y/B | | | | C | Y/B | ← Z → | B/Y | | | |
| (N) | NEW LIGHTED SIGN | | | | | | | | | ← C | B/Y | | | | | | | | | | |
| (O) | NEW LIGHTED SIGN | | | | | | | | | C | Y/B | 31R-13L | WR | | C | Y/B | | | | | |
| (P) | NEW LIGHTED SIGN | | | | | | | | | | | | | | C → | B/Y | | | | | |
| (Q) | NEW LIGHTED SIGN | | | | | | | | | B | Y/B | | | | B | Y/B | ← Z → | B/Y | | | |
| (R) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | C → | B/Y | | Z | Y/B | | | | | |
| (S) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | ← C → | B/Y | | Y | Y/B | | | | | |
| (T) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | | | | ← C | B/Y | Z | Y/B | | | |
| (U) | NEW LIGHTED SIGN | | | | | | | | | B | Y/B | ← Y → | B/Y | | B | Y/B | | | | | |
| (V) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | | | | Y | Y/B | ← C → | B/Y | | | |
| (W) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | B → | B/Y | | Z | Y/B | | | | | |
| (X) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | ← B → | B/Y | | Y | Y/B | | | | | |
| (Y) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | | | | ← B | B/Y | Z | Y/B | | | |
| (Z) | NEW LIGHTED SIGN | | | | | | | | | C | Y/B | ← Y → | B/Y | | C | Y/B | | | | | |
| (AA) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | | | | Y | Y/B | ← B → | B/Y | | | |
| (12) | REMOVE PLACARD AND SIGN ARRAY | | | | | PARKING | | | | | | | | | | | | | | | |
| (13) | REMOVE "TWY Y" PLACARD FROM SOUTH SIDE | 31R | WR | A | Y/B | | | A | Y/B | A | Y/B | 31R | WR | | A | Y/B | | | | | |
| (14) | REVERSE ORDER OF EXISTING PANELS | 31L | WR | A | Y/B | | | | | A | Y/B | 31L | WR | | | | | | | | |
| (15) | REVERSE ORDER OF EXISTING PANELS | | | | | A | Y/B | Y/B | WR | | | | | | 31R | WR | A | Y/B | | | |
| (BB) | NEW LIGHTED SIGN | | | | | | | | | Z | Y/B | | | | Z | Y/B | | | | | |
| (CC) | NEW LIGHTED SIGN | | | | | | | | | A | Y/B | | | | 31R | WR | A | Y/B | | | |
| (DD) | NEW LIGHTED SIGN | | | | | | | | | A | Y/B | 31L | WR | | A | Y/B | | | | | |
| (EE) | NEW LIGHTED SIGN | | | | | | | | | A ↗ | B/Y | Y → | B/Y | | ← Z | Y/B | | | | | |
| (FF) | NEW LIGHTED SIGN | | | | | | | | | ← Z | B/Y | Y | Y/B | | A ↗ | B/Y | Y | Y/B | | | |
| (GG) | NEW LIGHTED SIGN | | | | | | | | | Y | Y/B | A ↗ | B/Y | | Y | Y/B | RAMP ↑ | B/Y | Z ↗ | B/Y | |

KEY TO SIGN COLORS:

B/Y = BLACK LEGEND ON A YELLOW BACKGROUND (TYPE L-858Y)
 Y/B = YELLOW LEGEND AND BORDER ON A BLACK BACKGROUND (TYPE L-858L)
 W/R = WHITE LEGEND ON A RED BACKGROUND (TYPE L-858R)



- NOTES:
 1. (2)#2 BRANCH CIRCUIT CONDUCTORS
 2. REPLACE EXISTING BREAKER WITH NEW 100AMP 2P

Mead & Hunt

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 133 Aviation Boulevard,
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 Santa Rosa, CA 95403
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 meadhunt.com



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REID-HILLVIEW AIRPORT
REHABILITATE RUNWAY 13L-31R, TAXIWAYS, & CONNECTOR TAXIWAYS

REVISION

90% SUBMITTAL

APP NO: 3-06-0225-14
 M&H NO: R323300-115095.01
 DATE: MAY 2012
 DESIGNED BY: RAC
 DRAWN BY: BE
 CHECKED BY: JTL
 DO NOT SCALE DRAWINGS

SHEET COMMENTS
 ELECTRICAL AND SIGNING DETAILS

Appendix B
Engineer's Construction Cost Estimate

ENGINEER'S CONSTRUCTION COST ESTIMATE

| Item No. | | F/S | Description | Unit | Estimated Quantity | Unit Cost | Extension |
|---------------------|---|-----|---|--|--------------------|-------------------------------|----------------------|
| AIP # 3-06-0225-14 | | | | REID-HILLVIEW AIRPORT - BID SCHEDULE 2 | | | |
| ENGINEER'S ESTIMATE | | | | | | | |
| 1 | F | | MOBILIZATION | LS | 1 | \$ 55,500.00 | \$ 55,500.00 |
| 2 | F | | AIRFIELD SAFETY AND TRAFFIC CONTROL | LS | 1 | \$ 85,500.00 | \$ 85,500.00 |
| 3 | F | | DEMOLITION | LS | 1 | \$ 12,000.00 | \$ 12,000.00 |
| 4 | | | SURFACE PREPARATION - (95% PAINT REMOVAL) | SY | 9,400 | \$ 2.10 | \$ 19,740.00 |
| 5 | | | SURFACE PREPARATION - (60% PAINT REMOVAL) | SY | 17,360 | \$ 1.90 | \$ 32,984.00 |
| 6 | | | CRACK SEALING | LF | 12,200 | \$ 3.00 | \$ 36,600.00 |
| 7 | | | MICROSURFACING | SY | 71,600 | \$ 3.00 | \$ 214,800.00 |
| 8 | | | CALTRANS ASPHALT CONCRETE | TN | 20 | \$ 115.00 | \$ 2,300.00 |
| 9 | | | No. 8, 5KV CABLE | LF | 3,650 | \$ 2.45 | \$ 8,942.50 |
| 10 | F | | 15 KW CONSTANT CURRENT REGULATOR | LS | 1 | \$ 18,000.00 | \$ 18,000.00 |
| 11 | | | 2-INCH PVC DUCT | LF | 1,510 | \$ 3.50 | \$ 5,285.00 |
| 12 | | | TRENCH and BACKFILL - NATIVE | LF | 1,110 | \$ 15.00 | \$ 16,650.00 |
| 13 | | | TRENCH and BACKFILL - PAVEMENT | LF | 400 | \$ 50.00 | \$ 20,000.00 |
| 14 | | | PULL BOX TYPE A | EA | 2 | \$ 1,200.00 | \$ 2,400.00 |
| 15 | | | LED L-858 (L) GUIDANCE SIGNS - NEW 1 MOD | EA | 15 | \$ 3,800.00 | \$ 57,000.00 |
| 16 | | | LED L-858 (L) GUIDANCE SIGNS - NEW 2 MOD | EA | 9 | \$ 4,000.00 | \$ 36,000.00 |
| 17 | | | LED L-858 (L) GUIDANCE SIGNS - NEW 3 MOD | EA | 8 | \$ 4,800.00 | \$ 38,400.00 |
| 18 | | | LED L-858 (L) GUIDANCE SIGNS - NEW 4 MOD | EA | 10 | \$ 5,400.00 | \$ 54,000.00 |
| 19 | | | GUIDANCE SIGNS - ADJUST EXISTING | EA | 8 | \$ 1,200.00 | \$ 9,600.00 |
| 20 | | | L-852T SEMI-FLUSH TAXIWAY EDGE LIGHT | EA | 10 | \$ 950.00 | \$ 9,500.00 |
| 21 | | | L-861T ELEVATED TAXIWAY EDGE LIGHT | EA | 8 | \$ 850.00 | \$ 6,800.00 |
| 22 | | | PAVEMENT MARKING - WHITE | SF | 9,370 | \$ 2.10 | \$ 19,677.00 |
| 23 | | | PAVEMENT MARKING - BLACK | SF | 8,050 | \$ 2.00 | \$ 16,100.00 |
| 24 | | | PAVEMENT MARKING - YELLOW | SF | 17,360 | \$ 1.75 | \$ 30,380.00 |
| 25 | | | SURFACE PAINTED DIRECTION MARKINGS | EA | 6 | \$ 1,500.00 | \$ 9,000.00 |
| 26 | | | NO VEHICLE PAINTED MARKINGS | EA | 14 | \$ 1,400.00 | \$ 19,600.00 |
| | | | | | | BID SCHEDULE 2 - TOTAL | \$ 788,158.50 |

Appendix C

Additions and Modifications to FAA Standard Technical Specifications

**REID-HILLVIEW AIRPORT
REHABILITATION OF RUNWAY 13L-31R AND TAXIWAYS, AND AIRFIELD SIGNAGE
AIP NO. 3-06-0225-14**

**ADDITIONS AND MODIFICATIONS TO
FAA STANDARD TECHNICAL SPECIFICATIONS**

In the process of customizing the Specifications to the subject Project, we have added wording in numerous places throughout the Technical Specifications. This additional minor wording also provides clarification where we deemed necessary for issues such as measurement and payment and design intent. Any changes that significantly alter the FAA standards are detailed below.

Section 1. Miscellaneous Provisions for Airport Construction. This Specification was added to provide information for construction safety on the Airport. It includes limitations on construction operations, requirements for construction management and scheduling, and site specific information pertaining to potential impacts on construction activities. Direction is given to advise Contractor that he/she must comply with the Construction Safety and Phasing Plan which is included as an appendix to this Section.

The FAA does not have a Specification for this work. This section was developed based on State and Local Standard Specifications. Appropriate FAA guidelines have been reviewed and incorporated.

Section 2. Mobilization. This Specification addresses preparatory work and operations which must be performed prior to beginning work on the various Contract items on the Project site.

The FAA does not have a Specification for this work. This section was developed based on State and Local Standard Specifications and based on our past experience on other airport Projects and the specific needs of this Project. The Specification is formatted similar to FAA Standard Specifications and appropriate FAA guidelines have been reviewed and incorporated.

Section 3. Surface Preparation. This Specification addresses the requirements of cleaning and preparing the airfield pavement and the specifics of doing so. It includes removing rubber deposits and other materials and pavement markings.

The FAA does not have a Specification for this work. This section was developed based on state and local Standard Specifications and based on our past experience on other airport projects and the specific needs of this Project. The Specification is formatted similar to FAA Standard Specifications and appropriate FAA guidelines have been reviewed and incorporated.

Section 4. Pavement Repair and Crack Sealing. This Specification addresses the repair of asphalt pavement in proposed microsurfacing areas. Included in this item are crack cleaning, treating cleaned cracks with an herbicide, crack sealing, and required demolition of tie-downs, lights and signs.

The FAA does not have a Specification for this work. This section was developed based on state and local Standard Specifications and based on our past experience on other airport projects and the specific needs of this Project. The Specification is formatted similar to FAA Standard Specifications and appropriate FAA guidelines have been reviewed and incorporated.

Section 5. Bituminous Tack Coat, Item P-603

5-5.1 Language was added to indicate no separate measurement or payment will be made for Bituminous Tack Coat.

Section 6. Microsurfacing. This Specification addresses the requirements and specifications of microsurfacing material and the application of such.

The FAA does not have a Specification for this work. This section was developed based on state and local Standard Specifications and based on our past experience on other airport projects and the specific needs of this Project. The Specification is formatted similar to FAA Standard Specifications and appropriate FAA guidelines have been reviewed and incorporated.

Section 7. Caltrans Asphalt Concrete, Modified

The FAA does not have a specification for this work. The specification is based upon local and state specifications and is formatted similar to FAA standard specifications. Caltrans State standard asphalt concrete is specified for use on this project as the surface course in areas that are subject to 12,500-pound or less aircraft wheel loads and roadway pavements. The specifications include a provision for reducing payment if the in-place AC does not achieve specified compaction. Caltrans AC is typically 10% to 20% less expensive than Marshall mix in this locale. It also compares favorable to Marshall mix in terms of appearance and durability.

Section 8. Underground Cable for Airports, Item L-108

8-2.1A. General. Modification was made to wording to direct Contractor to verify proposed manufacturer is listed in the FAA's approved list prior to submitting catalog cuts for approval.

8-2.1D Wording added/modified to indicate that all submittals are required to be submitted during the Mobilization phase.

8-2.2 Cable. Underground cable requirement type is indicated in this subsection.

8-2.4A The Cast Splice. Deleted. Not used.

8-2.4B. The Field-attached Plug in Splice. Language was modified to indicate the acceptable connectors and components and the Contractor's responsibility.

8-2.4C. The Factory Molded Plug in Splice. Deleted. Not used.

8-3.4D. Tape or Heat Shrink Splices. Subsection deleted. Not applicable.

8-2.6 Concrete. Contractor is directed to see Subsection 12, Medium Intensity Taxiway Light System Specification for concrete requirements.

8-2.7 Flowable Backfill. Contractor is directed to Subsection 8, P-153, CLSM for requirements of backfill material.

8-3.5 Bare Counterpoise Wire Installation for Lightning Protection and Grounding. Deleted. Not Used.

8-3.8 Circuit Identification and Labeling. Requirements for identifying and labeling circuits are detailed in this subsection.

8-3.9 Testing. Subsection modified to include the requirement of 4 copies of tabulated results of all cable tests performed be supplied by the CONTRACTOR to the ENGINEER. Ground rod requirements are also addressed.

8-4.1 & 8-5.1 Measurement and Payment. These subsections were modified to indicate that no separate measurement or payment will be made for cable or counterpoise slack, and the payment specifics of counterpoise wire, cable, and grounding of electrical structures.

Section 10. Installation of New Constant Current Regulator, Item L-109

10-1.1 Language added/modified to give project specific general information of installation of Constant Current Regulator.

10-2.4 Wire. Specifications for wire in conduit are given.

10-3.3 Switchgear and Panels. Deleted. Not used.

10-3.5 Cable Entrance and High Voltage Bus System. Deleted. Not used.

10-3.6 Wiring and Connections. Section modified for project specific requirements regarding existing control wiring and CCR connection.

10-3.17 Electrical Service. Deleted. Not used.

Section 11. Airport Underground Electrical Duct Banks and Conduits, Item L-110

11-2.2 Steel Conduit. Deleted. Not used.

11-2.4 Split Conduit. Deleted. Not used.

11-2.5 Concrete. Deleted from this section.

11-2.7 Flowable Backfill. Modified to direct Contractor to Subsection 8, CLSM for requirements.

11-2.9 Electrical Pull boxes. Pull box requirements are addressed.

11-3.1 General. This section was modified to indicate that CLSM would be used instead of concrete for duct bank encasement.

11-3.2 Conduits Encased in Controlled Low Strength Material. Specifications of conduits encased in CLSM are given.

11-3.5 Backfilling for Conduits (Native). Subsection modified to specify that the trench shall be backfilled in accordance with the Plans.

11-3.6 Backfilling for Conduits (CLSM Encased). Subsection was modified to address specifications for backfilling for CLSM encased conduits.

11-3.7 Restoration. Deleted. Not applicable.

11-4.1 – 11-4.2 Method of Measurement. Measurement methods for conduit, trench and backfill, and pullboxes are addressed.

11-5.1 – 11-5.3 Basis of Payment. Payment for conduit, trench and backfill, and pull boxes are addressed.

Section 12. Medium Intensity Runway Light System, Item L-125

This section deals with furnishing and installing a new Medium Intensity Light System and new LED Guidance Signs. This section was adapted from the old L-125 Specification for Medium-Intensity Lighting. The FAA does not currently have a specification for this work.

Section 13. Runway and Taxiway Markings, Item P-620.

13-2.2 Paint. Subsection was modified to specify paint requirements.

13-2.3 Reflective Media. Subsection was modified to specify glass bead requirements.

13-3.4 Removal of Existing Markings. This subsection modified to indicate that marking removal shall be in conformance with Subsection 3, Surface Preparation, of the SPECIFICATIONS.

13-3.5 Layout of Markings. Modifications were made to this subsection to specify that no paint shall be applied until the COUNTY has had an opportunity to review the marking layout; and black paint must not be used to cover pavement marking errors.

13-3.6 Application. Modifications were made to this subsection to specify that runway markings shall receive two coats of paint; the second coat applied 30 days after the first. The addition of thinner will not be permitted. A period of 36 hours shall elapse between placement of a surface treatment and application of the paint.

13-4.1 – 13-4.4 Method of Measurement. Modifications were made to specify measurement of elements in this section.

13-5.1 – 13.5.4 Basis of Payment. Modifications were made to address specifics regarding payment of elements in this section.

Appendix D

Sponsor Certification for Project Plans and Specifications

U.S. DEPARTMENT OF TRANSPORTATION
 FEDERAL AVIATION ADMINISTRATION
 AIRPORT IMPROVEMENT PROGRAM
 SPONSOR CERTIFICATION
 PROJECT PLANS AND SPECIFICATIONS

County of Santa Clara
(Sponsor)

Reid-Hillview Airport
(Airport)

AIP 3-06-0225-14
(Project Number)

Pavement Rehabilitation

(Project Description)

Title 49, United States Code, section 47105(d), authorizes the Secretary to require certification from the sponsor that it will comply with the statutory and administrative requirements in carrying out a project under the Airport Improvement Program (AIP). AIP standards are generally described in FAA Advisory Circular (AC) 150/5100-6, Labor Requirements for the Airport Improvement Program, AC 150/5100-15, Civil Rights Requirements for the Airport Improvement Program, and AC 150/5100-16, Airport Improvement Program Grant Assurance One--General Federal Requirements. A list of current advisory circulars with specific standards for design or construction of airports as well as procurement/installation of equipment and facilities is referenced in standard airport sponsor Grant Assurance 34 contained in the grant agreement.

Except for the certified items below marked not applicable (N/A), the list includes major requirements for this aspect of project implementation, although it is not comprehensive, nor does it relieve the sponsor from fully complying with all applicable statutory and administrative standards.

| | Yes | No | N/A |
|---|-------------------------------------|--------------------------|-------------------------------------|
| 1. The plans and specifications were (will be) prepared in accordance with applicable Federal standards and requirements, so no deviation or modification to standards set forth in the advisory circulars, or State standard, is necessary other than those previously approved by the FAA. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 2. Specifications for the procurement of equipment are not (will not be) proprietary or written so as to restrict competition. At least two manufacturers can meet the specification. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 3. The development included (to be included) in the plans is depicted on the airport layout plan approved by the FAA. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 4. Development that is ineligible for AIP funding has been (will be) omitted from the plans and specifications. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 5. The process control and acceptance tests required for the project by standards contained in Advisory Circular 150/5370-10 are (will be) included in the project specifications. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 6. If a value engineering clause is incorporated into the contract, concurrence was (will be) obtained from the FAA. | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 7. The plans and specifications incorporate (will incorporate) applicable requirements and recommendations set forth in the federally approved environmental finding. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

| | Yes | No | N/A |
|--|-------------------------------------|--------------------------|--------------------------|
| 8. For construction activities within or near aircraft operational areas, the requirements contained in Advisory Circular 150/5370-2 have been (will be) discussed with the FAA as well as incorporated into the specifications, and a safety/phasing plan has FAA's concurrence, if required. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 9. The project was (will be) physically completed without Federal participation in costs due to errors and omissions in the plans and specifications that were foreseeable at the time of project design. | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

I certify, for the project identified herein, responses to the forgoing items are accurate as marked and have prepared documentation attached hereto for any item marked "no" that is correct and complete.

County of Santa Clara

(Name of Sponsor)

(Signature of Sponsor's Designated Official Representative)
 Carl Honaker

(Typed Name of Sponsor's Designated Official Representative)
 Director of Airports

(Typed Title of Sponsor's Designated Official Representative)

(Date)

Mead & Hunt, Inc.

(Name of Consultant)

(Signature of Mead & Hunt's Official Representative)
 Jeffrey T. Leonard, P.E.

(Typed Name of Mead & Hunt's Official Representative)
 Project Manager

(Typed Title of Mead & Hunt's Official Representative)

(Date)