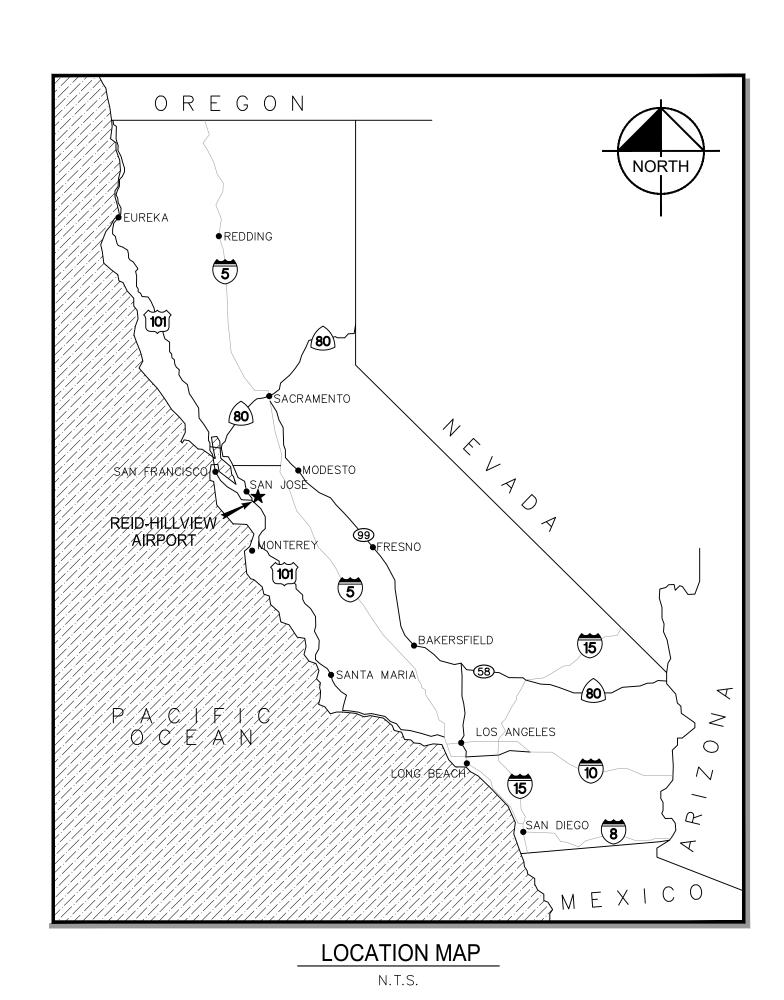
# REID-HILLVIEW AIRPORT SANTA CLARA COUNTY, CALIFORNIA

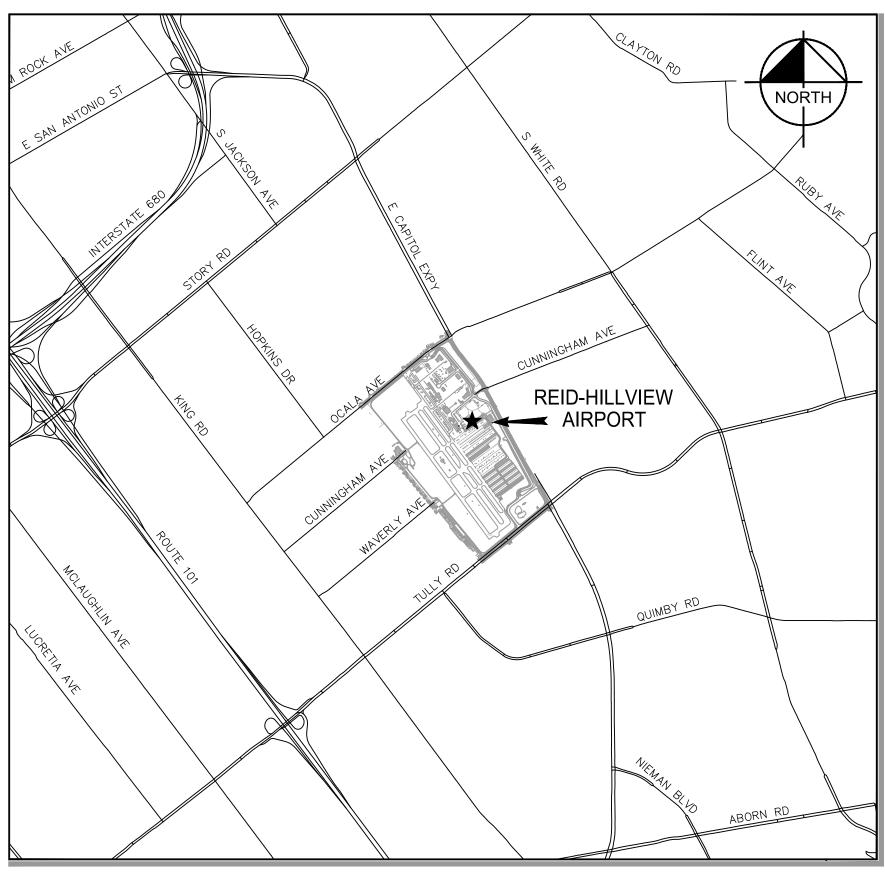
## AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

### REID-HILLVIEW AIRPORT IMPROVEMENTS COUNTY OF SANTA CLARA



Sheet Index						
Sheet Number	Sheet Title	Sheet Description				
1	T0.0	COVER SHEET				
2	T0.1	GENERAL NOTES, ABBREVIATIONS, AND LEGEND				
3	T0.2	PROJECT LAYOUT PLAN				
4	T0.3	SURVEY CONTROL PLAN				
5	T1.0	CONSTRUCTION PHASING PLAN OVERALL				
6	T1.1	CONSTRUCTION PHASING PLAN PHASE 1				
7	T1.2	CONSTRUCTION PHASING PLAN PHASE 2				
8	T1.3	CONSTRUCTION PHASING PLAN PHASE 3				
9	T1.4	CONSTRUCTION PHASING PLAN PHASE 4				
10	T1.5	CONSTRUCTION PHASING PLAN PHASE 5				
11	T1.6	CONSTRUCTION PHASING PLAN DETAILS				
12	C1.0	SEGMENTED CIRCLE LAYOUT				
13	C2.0	CIVIL DEMOLITION - 1				
14	C2.1	CIVIL DEMOLITION - 3				
15	C2.2	CIVIL DEMOLITION - 4				
16	C2.3	CIVIL DEMOLITION - 5				
17	C2.4	CIVIL DEMOLITION - 6				
18	C2.5	DEMOLITION PLAN - 7				
19	C2.6	DEMOLITION PLAN - 8				
20	C2.7	DEMOLITION PLAN - 9				
21	C2.8	DEMOLITION PLAN - 10				
22	C2.9	DEMOLITION PLAN - 11				
23	C2.10	DEMOLITION PLAN - 12				
24	C2.11	DEMOLITION PLAN - 13				
25	C2.12	DEMOLITION PLAN - 14				
26	C2.13	DEMOLITION PLAN - 15				
27	C2.14	DEMOLITION PLAN - 16				
28	C3.0	PAVEMENT MARKING PLAN - 1				
29	C3.1	PAVEMENT MARKING PLAN - 2				
30	C3.2	PAVEMENT MARKING PLAN - 3				
31	C3.3	PAVEMENT MARKING PLAN - 4				
32	C3.4	PAVEMENT MARKING PLAN - 5				
33	C3.5	PAVEMENT MARKING PLAN - 6				
34	C3.6	PAVEMENT MARKING PLAN - 7				
35	C3.7	PAVEMENT MARKING PLAN - 8				
36	C3.8	PAVEMENT MARKING PLAN - 9				
37	C3.9	PAVEMENT MARKING PLAN - 10				
38	C3.10	PAVEMENT MARKING PLAN - 11				

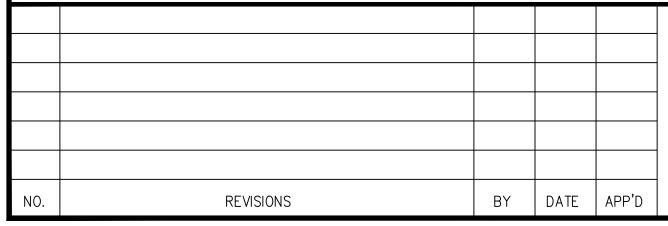
		Sheet Index
Sheet Number	Sheet Title	Sheet Description
39	C3.11	PAVEMENT MARKING PLAN - 12
40	C3.12	PAVEMENT MARKING PLAN - 13
41	C3.13	PAVEMENT MARKING PLAN - 14
42	C3.14	PAVEMENT MARKING PLAN - 15
43	C3.15	PAVEMENT MARKING PLAN - 16
44	C3.16	PAVEMENT MARKING DETAILS 1
45	C3.17	PAVEMENT MARKING DETAILS 2
46	E1.0	ELECTRICAL SCOPE OF WORK
47	E1.1	ELECTRICAL LAYOUT
48	E1.2	ELECTRICAL LAYOUT
49	E1.3	ELECTRICAL LAYOUT
50	E1.4	ELECTRICAL LAYOUT
51	E1.5	ELECTRICAL LAYOUT
52	E2.1	SIGN LAYOUT
53	E2.2	SIGN LAYOUT
54	E2.3	SIGN LAYOUT
55	E2.4	SIGN LAYOUT
56	E3.1	ELECTRICAL DEMOLITION
57	E3.2	ELECTRICAL DEMOLITION
58	E3.3	ELECTRICAL DEMOLITION
59	E3.4	ELECTRICAL DEMOLITION
60	E3.5	ELECTRICAL DEMOLITION
61	E4.1	ELECTRICAL DETAILS
62	E4.2	ELECTRICAL DETAILS
63	E4.3	ELECTRICAL DETAILS
64	E4.4	ELECTRICAL DETAILS
65	E4.5	ELECTRICAL DETAILS
66	E5.1	RUNWAY 13L-31R CIRCUIT MAP
67	E5.2	TAXIWAY CIRCUIT MAP
68	E5.3	RUNWAY GUARD LIGHT CIRCUIT MAP



COUNTY OF SANTA CLARA VICINITY MAP



DRAWING NO.





Kimley » Horn 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113

(669) 800-4130

JPT	8/18/23
DESIGNED	DATE
JPT	8/18/23
DRAWN	DATE
JDB	8/18/23
CHECKED	DATE

SIGNATURE

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT HARRY FREITAS DIRECTOR

**REID-HILLVIEW AIRPORT** AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

192232000

**COVER SHEET** 

1 OF 68

T0.0

CONTRACT NO. T0.XX-COVER SHEET

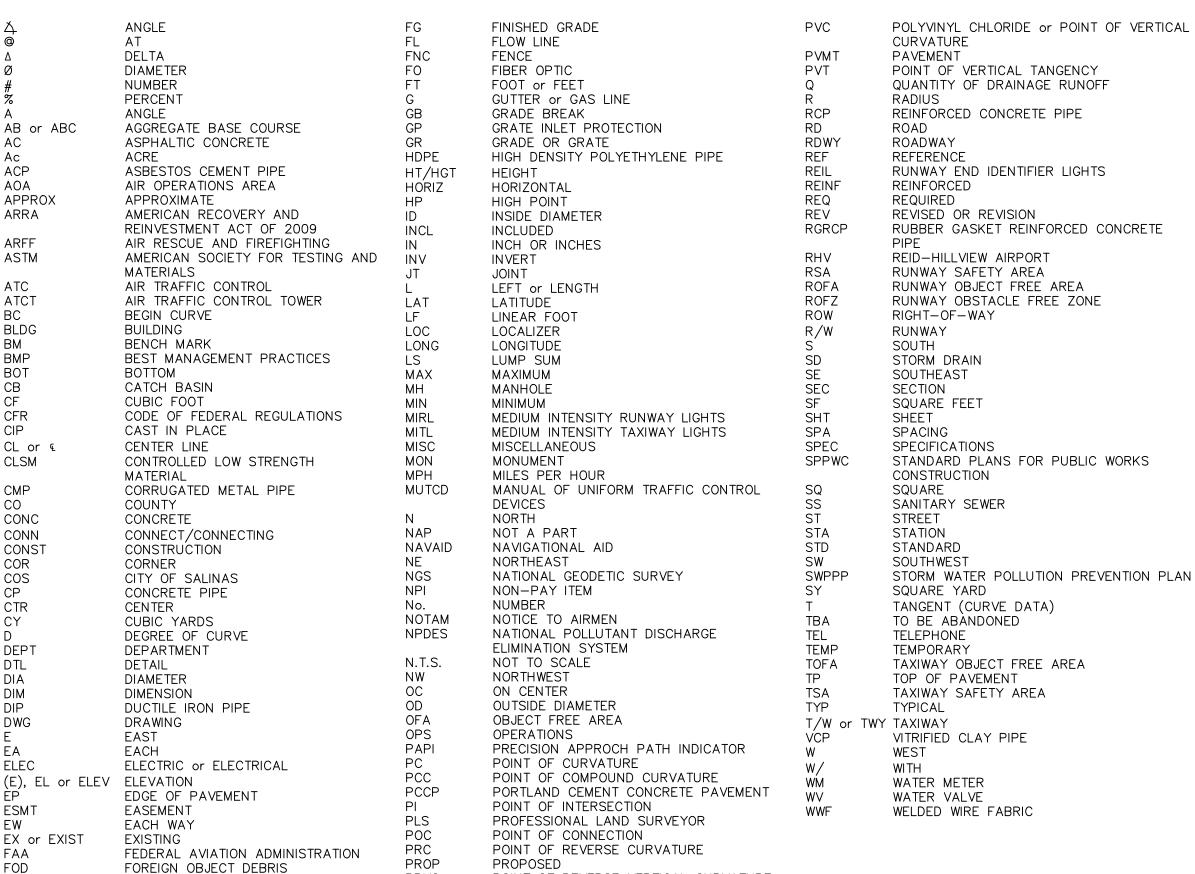
- 2. THE CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE AND LOCAL SAFETY REGULATIONS.
- 3. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS FROM LOCAL GOVERNMENTS OR JURISDICTIONAL AGENCIES PRIOR TO STARTING ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL PROVIDE A COPY OF ALL CONSTRUCTION PERMITS TO THE ENGINEER WITHIN SEVEN (7) DAYS OF ISSUE OF SUBJECT PERMIT.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF ALL EXISTING IMPROVEMENTS, PROPERTY AND FACILITIES. THE CONTRACTOR SHALL PROTECT CAREFULLY, FROM DAMAGE, OR DISTURBANCE, ALL LAND AND SURVEY MONUMENTS AND PROPERTY MARKERS. IF DAMAGE TO AIRPORT PROPERTY AND/OR FACILITIES DOES OCCUR AS A RESULT OF THE CONTRACTORS OPERATIONS, THE CONTRACTOR, AT HIS SOLE EXPENSE, SHALL RESTORE SUCH PROPERTY AND/OR FACILITIES TO A CONDITION EQUAL TO THAT EXISTING BEFORE SUCH DAMAGE AND TO THE SATISFACTION OF THE ENGINEER.
- 5. SAFETY AND SECURITY SAFETY AND SECURITY OF THE CONSTRUCTION OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (AOA) AND THE SAFETY AND SECURITY OF AIRCRAFT OPERATIONS THROUGH AND IN THE VICINITY OF THE CONSTRUCTION PROJECT LIMITS IS THE CONTRACTOR'S RESPONSIBILITY. THE CONTRACTOR SHALL COORDINATE WITH RHV OPERATIONS AND THE COUNTY ON ALL MATTERS RELATED TO SAFETY AND SECURITY WITHIN THE AOA. WORK SHALL NOT BE PERFORMED WITHIN THE AOA WITHOUT PRIOR COORDINATION WITH THE AIRPORT REPRESENTATIVES.
- 6. THE CONSTRUCTION WORK ON THIS PROJECT WILL OCCUR WITHIN AN ACTIVE AOA AND IS SUBJECT TO THE OPERATIONAL SAFETY AND SECURITY REQUIREMENTS OF REID-HIILLVIEW AIRPORT (RHV) AND THE FAA AS IDENTIFIED WITHIN THE CONTRACT DOCUMENTS. IN ADDITION THE CONTRACTOR SHALL COMPLY WITH ANY ADDITIONAL SAFETY AND SECURITY REQUIREMENTS NOT IDENTIFIED IN THESE CONTRACT DOCUMENTS, BUT DEEMED NECESSARY BY RHV OR THE FAA AND AT NO ADDITIONAL COST TO THE OWNER.
- 7. THE CONTRACTOR SHALL <u>NOT</u> ENTER ONTO ANY AREA OF THE AOA OUTSIDE OF THE CONSTRUCTION LIMITS, THE STAGING AREAS, OR THE DESIGNATED HAUL ROUTES WITHOUT THE WRITTEN APPROVAL OF THE ENGINEER OR RHV.
- 8. THE CONTRACTOR WILL BE REQUIRED TO (1) COORDINATE WITH RHV OPERATIONS AND THE FAA TOWER PERSONNEL ON ALL ITEMS RELATED TO THE AOA SAFETY AND SECURITY; AND (2) FULLY COMPLY WITH THE AIRPORT PROCEDURES AND SPECIAL REQUIREMENTS FOR AOA ACCESS, AIRPORT SECURITY TRAINING/INDUCTION, AND SECURITY BADGING. THE CONTRACTOR SHALL CARRY OUT THE WORK IN A MANNER TO MAINTAIN THE INTEGRITY OF THE AOA SECURITY AT ALL TIMES.
- 9. THE CONTRACTOR IS RESPONSIBLE FOR ALL CONSTRUCTION SURVEYING. EXISTING AIRPORT SURVEY MONUMENTS ARE LOCATED WITHIN THE AOA. THE CONTRACTOR SHALL, AT HIS EXPENSE, HAVE A REGISTERED LAND SURVEYOR REPLACE ANY DISTURBED MONUMENTS USING FIRST ORDER TECHNIQUES. REPLACEMENT MONUMENTS SHALL BE PLACED AT LEAST TEN-FEET (10') BUT NOT MORE THAN FIFTY-FEET (50') FROM THE ORIGINAL MONUMENT.
- 10. IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE SERVICES OF A SUBSURFACE UTILITY LOCATE COMPANY AND LOCATE ALL EXISTING UTILITIES AND UNDERGROUND AIRPORT FACILITIES THAT MAY BE AFFECTED BY THIS PROJECT AND TO UTILITY QUALITY LEVEL 'B' PER ASCE 38-02. THE CONTRACTOR SHALL SUBSEQUENTLY EMPLOY POT-HOLE TECHNIQUES TO VERIFY EXACT LOCATIONS AND ELEVATIONS IN ACCORDANCE WITH UTILITY QUALITY LEVEL 'A' PER ASCE 38-02 PRIOR TO COMMENCING WORK. EXISTING UTILITY INFORMATION, INCLUDING LIGHTING DUCTS AND CABLES, SHOWN ON THE PLANS WAS COMPILED BASED ON UTILITY RECORDS AVAILABLE TO THE ENGINEER AND SHOULD BE TREATED AS UTILITY QUALITY LEVEL 'D' PER ASCE 38-02. A REASONABLE ATTEMPT HAS BEEN MADE TO SHOW THE LOCATIONS OF UNDERGROUND OBSTRUCTIONS, UTILITIES, AND UNDERGROUND AIRPORT FACILITIES IN THE WORK AREAS. THE UTILITIES, AND FACILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY UNDERGROUND UTILITIES, AIRPORT FACILITIES, OR OBSTACLES THAT MAY OCCUR ON THE SITE. THE OWNER AND ENGINEER BEAR NO RESPONSIBILITY FOR UTILITIES NOT SHOWN OR SHOWN IN AN INCORRECT LOCATION OR ELEVATION ON THE PLANS. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE. ALL EXCAVATIONS IMMEDIATELY ADJACENT TO UTILITIES SHALL BE DONE BY HAND. UTILITIES INTERFERING WITH CONSTRUCTION SHALL BE RESET OR RELOCATED BY THE CONTRACTOR UNLESS NOTED OTHERWISE. THE CONTRACTOR SHALL PROVIDE A UTILITY LOCATOR AND VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND PROCEED WITH CAUTION AROUND ANY SUCH FEATURES. THE CONTRACTOR SHALL GIVE PROPER NOTICE TO ALL UTILITY COMPANIES AND FACILITY OWNERS REGARDING REMOVAL AND RELOCATION ACTIVITIES, AND WHEN WORKING IN THE VICINITY OF UTILITY LINES AND AIRPORT FACILITIES.
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITY LINES OR AIRPORT FACILITIES ENCOUNTERED DURING CONSTRUCTION. ANY DAMAGE TO UTILITIES MUST BE REPAIRED IMMEDIATELY BY THE CONTRACTOR, TO THE SATISFACTION OF THE ENGINEER AND AT NO COST TO THE AIRPORT
- 12. POWER, CONTROL CABLES AND FIXTURES FOR AIRFIELD LIGHTING AND NAVIGATIONAL AIDS ARE LOCATED IN THE CONSTRUCTION AREAS. THE CONTRACTORS PERSONNEL SHALL BECOME FAMILIAR WITH THESE CABLE AND FIXTURE LOCATIONS AND KEEP VEHICLES AND EQUIPMENT CLEAR FROM THEM AT ALL TIMES. CONTRACTOR SHALL ADHERE TO ANY RHV LOCKOUT/TAGOUT PROCEDURES.
- 13. ANY DAMAGE TO THE EXISTING AIRPORT LIGHTING SYSTEM CAUSED BY CONSTRUCTION OPERATIONS SHALL BE REPORTED TO THE OWNER IMMEDIATELY AND REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 14. CONTRACTOR SHALL NOTE IN THE RECORD DRAWINGS ALL PIPES, DUCTS AND CABLES FOUND DURING EXCAVATION AND INDICATE EXACT POSITION, ELEVATION, DIRECTION, SIZE, MATERIAL, PURPOSE AND ACTIVE STATUS IF KNOWN.
- 15. WASTE MATERIALS ALL WASTE MATERIAL INCLUDING BUT NOT LIMITED TO ASPHALT, CONCRETE, RUBBLE, PAVEMENT REINFORCEMENT FABRIC AND/OR GEOGRID. AND WASTE EXCAVATION SHALL BE PROPERLY DISPOSED OF OFF AIRPORT PROPERTY AND IN ACCORDANCE WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS. WASTE MATERIAL SHALL NOT BE WASTED OR STOCKPILED ON THE AIRPORT PROPERTY.
- 16. SALVAGED ITEMS ANY SALVAGED ITEMS ARE TO REMAIN THE PROPERTY OF AIRPORT.
- 17. THE CONTRACTOR IS ADVISED THAT NO INVESTIGATIONS WERE CARRIED OUT ON THE EXISTING ASPHALT CONCRETE PAVEMENT TO DETERMINE IF THE PAVEMENT CONTAINS REINFORCEMENT FABRIC OR GEOGRID. IT IS THE CONTRACTORS RESPONSIBILITY TO FURTHER INVESTIGATE CONDITIONS AS IT DETERMINES NECESSARY. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR THE REMOVAL OF PAVEMENT CONTAINING REINFORCEMENT FABRIC OR GEOGRID, IT SHALL BE DEEMED INCLUDED IN THE UNIT COSTS FOR THE VARIOUS ITEMS.
- 18. THE CONTRACTOR IS ADVISED THAT NO INVESTIGATIONS WERE CARRIED OUT ON THE EXISTING STORM DRAIN SYSTEM OR NAVAID AND ELECTRICAL SYSTEM, AND THESE SYSTEMS MAY BE BACKFILLED WITH CONCRETE, SLURRY, CLSM OR A COMBINATION OF THE ABOVE. IT IS THE CONTRACTORS RESPONSIBILITY TO FURTHER INVESTIGATE CONDITIONS AS IT DETERMINES NECESSARY. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR CONCRETE, SLURRY, AND/OR CLSM BACKFILL REMOVAL, IT SHALL BE DEEMED INCLUDED IN THE UNIT COSTS FOR THE VARIOUS ITEMS.
- 19. ANY TEMPORARY STOCKPILING OF MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT AS A RESULT OF WING VORTEX, PROP WASH OR JET BLAST FROM AIRCRAFT OPERATIONS, OR OTHER SURFACE WIND CURRENTS.
- 20. CONTRACTOR GENERATED DEBRIS, WASTE, AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEAR, PROPELLERS, ROTORS, OR LIKELY TO BE INGESTED BY JET ENGINES SHALL NOT BE LEFT ON PAVEMENTS WITHIN THE ACTIVE MOVEMENT AREAS OF THE AOA. CONTRACTOR GENERATED DEBRIS OR MATERIAL ON PAVEMENT SURFACES WITHIN THESE AREAS SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR. DURING WORKING HOURS THE CONTRACTOR SHALL MONITOR AND CONTINUOUSLY REMOVE DEBRIS AND MATERIAL FROM THESE AREAS.
- 21. THE CONTRACTOR SHALL CONDUCT POWER VACUUM CLEANING OF ALL AFFECTED AIRPORT PAVEMENTS INCLUDING CONSTRUCTION TRAFFIC HAUL ROUTES AND CROSSINGS AT THE END OF EACH WORK DAY, PRIOR TO THE REOPENING OF ANY PAVEMENT TO AIR TRAFFIC AND IMMEDIATELY AFTER PROJECT COMPLETION. THE CONTRACTOR IS RESPONSIBLE FOR CONTINUOUS DAILY CLEAN UP OF THE WORK AREAS.

#### **GENERAL NOTES (CONTINUED)**

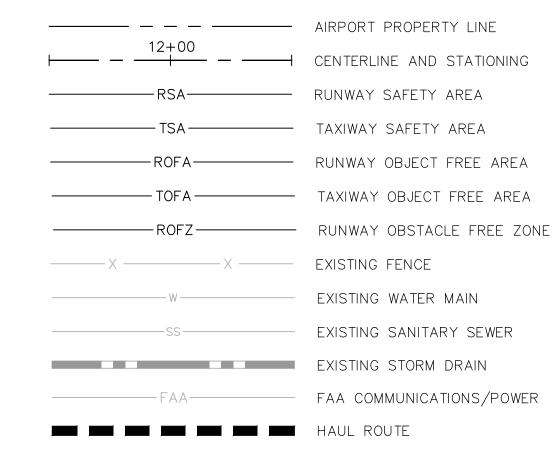
- 22. THE CONTRACTOR SHALL COMPLETE CLEAN UP AND RESTORATION OF THE ENTIRE PROJECT AREA, INCLUDING ALL STAGING AND STORAGE AREAS, AS APPROVED BY THE ENGINEER, WITHIN FIFTEEN DAYS OF PROJECT SUBSTANTIAL COMPLETION DATE.
- 23. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COMPARE THE EXISTING SITE CONDITIONS WITH THE PLANS AND NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES OBSERVED. SHOULD ANY GRADE OR DESIGN INDICATED ON THE PLANS BE SUSPECTED TO BE IN ERROR, THE ENGINEER SHALL BE NOTIFIED OF SAID AREA AT LEAST FORTY-EIGHT HOURS BEFORE CONSTRUCTION IS SCHEDULED TO BEGIN. IF THE ENGINEER IS NOT NOTIFIED PRIOR TO THE START OF CONSTRUCTION, ANY DISCREPANCIES AND ASSOCIATED COSTS SUBSEQUENTLY FOUND SHALL BE DEEMED TO BE THE SOLE RESPONSIBILITY OF CONTRACTOR.
- 24. THE OWNER RESERVES THE RIGHT TO MAKE REVISIONS TO THE FINISHED ELEVATIONS AND GRADIENTS. IF CHANGES ARE NECESSARY, THE OWNER WILL FURNISH A REVISED GRADING AND/OR PAVING PLAN. UNIT PRICES SHALL GOVERN FOR ANY REVISED QUANTITIES.
- 25. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ALL UTILITIES AND HOOKUPS FOR THE CONTRACTORS STAGING AREA. REQUIRED UTILITIES FOR THE CONTRACTORS OPERATIONS SHALL BE ARRANGED AND PAID FOR BY THE CONTRACTOR, IF REQUIRED DIRECTLY TO THE APPROPRIATE UTILITY.
- 26. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INVESTIGATE THE AVAILABILITY OF AN ADEQUATE SUPPLY OF SUITABLE WATER FOR THE CONSTRUCTION. THE CONTRACTOR SHALL SECURE AND MAKE ALL ARRANGEMENTS NECESSARY FOR THE PURCHASE OF WATER FOR THE CONSTRUCTION, AND TO OBTAIN ALL NECESSARY PERMITS, AND PROVIDE ALL NECESSARY FACILITIES TO FURNISH WATER FOR USE DURING THE CONSTRUCTION. NO ADDITIONAL PAYMENT OR MEASUREMENT WILL BE MADE FOR PROVIDING WATER FOR THE CONSTRUCTION, IT SHALL BE AT THE CONTRACTOR'S SOLE EXPENSE.
- 27. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FIRE HYDRANTS AT ALL TIMES.
- 28. THE CONTRACTOR SHALL NOT DRAW CONSTRUCTION WATER FROM ANY FIRE HYDRANT FOR USE DURING THE PROJECT WITHOUT FIRST OBTAINING A WRITTEN PERMIT FOR THE WATER METER FROM THE WATER COMPANY.
- 29. THE CONTRACTOR SHALL APPLY WATER, CHEMICALS, VEGETATION OR OTHER MATERIALS WHEN THE ENGINEER DEEMS NECESSARY TO PREVENT THE OCCURRENCE OF DUST WHICH IS OBJECTIONABLE FOR AIRCRAFT OPERATIONS AND OTHER USERS OF THE AREA INCLUDING BUT NOT LIMITED TO RHV OPERATIONS, AIRCRAFT OPERATIONS AND LAND SIDE OPERATIONS. ALL COSTS FOR CONTROLLING CONSTRUCTION DUST OR POLLUTANTS FROM THE CONTRACTORS OPERATIONS SHALL BE INCIDENTAL TO THE CONTRACT.
- 30. THE CONTRACTOR IS REQUIRED TO PROVIDE ADEQUATE LIGHTING FOR CONSTRUCTION OF ALL WORK AREAS DURING THE HOURS OF DARKNESS, REDUCED VISIBILITY, OR AS REQUIRED BY THE SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER. COSTS FOR SAID LIGHTING SHALL BE INCIDENTAL TO THE CONTRACT.
- 31. THE CONTRACTOR'S SUPERINTENDENT SHALL BE ON THE CONSTRUCTION SITE AT ALL TIMES WHEN WORK IS BEING CARRIED OUT AND THE CONTRACTORS SUPERINTENDENT, OR AN APPROVED DESIGNEE, SHALL BE ON-CALL AND AVAILABLE IN CASE OF EMERGENCIES ON A TWENTY-FOUR-HOUR DAILY BASIS FOR THE DURATION OF THE PROJECT. THE CONTRACTOR'S SUPERINTENDENT SHALL BE DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE REPRESENTATIVE.

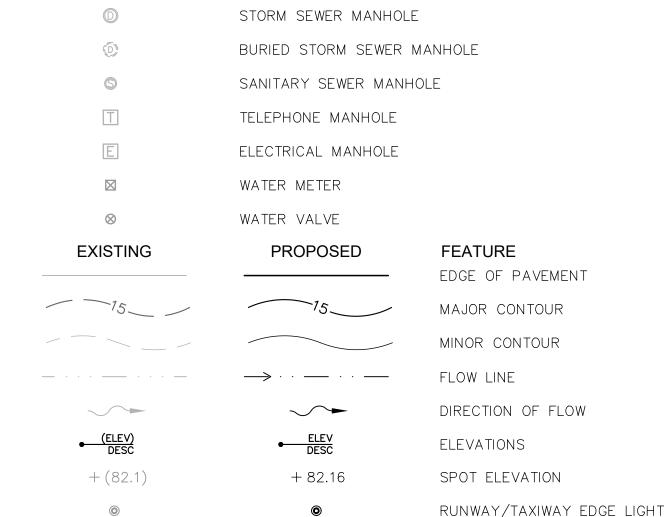
#### **ABBREVIATIONS**

\*SEE E1.0 FOR MORE ABBREVIATIONS



#### LEGEND





AIRPORT SIGN

TRAFFIC SIGN POST

SURVEY MONUMENT

STORM DRAIN INLET

BORING/CORE LOCATION AND DEPTH

CORE #

DATE APP'D BY **REVISIONS** 



2023 KIMLEY-HORN AND ASSOCIATES, INC.

(669) 800-4130

FND

FEMA

FOUND

FEDERAL EMERGENCY MANAGEMENT

JAMIL S. SALAS SENIOR CIVIL ENGINEER

POINT OF REVERSE VERTICAL CURVATURE

POUNDS PER SQUARE INCH

POINT OF TANGENCY

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT No C34920 **√Exp.** 09–30–23

### **REID-HILLVIEW AIRPORT**

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

**GENERAL NOTES, ABBREVIATIONS, AND LEGEND** 

CONTRACT NO.

192232000

2 OF 68

T0.1

DRAWING NO.

T1.XX-GENERAL NOTES

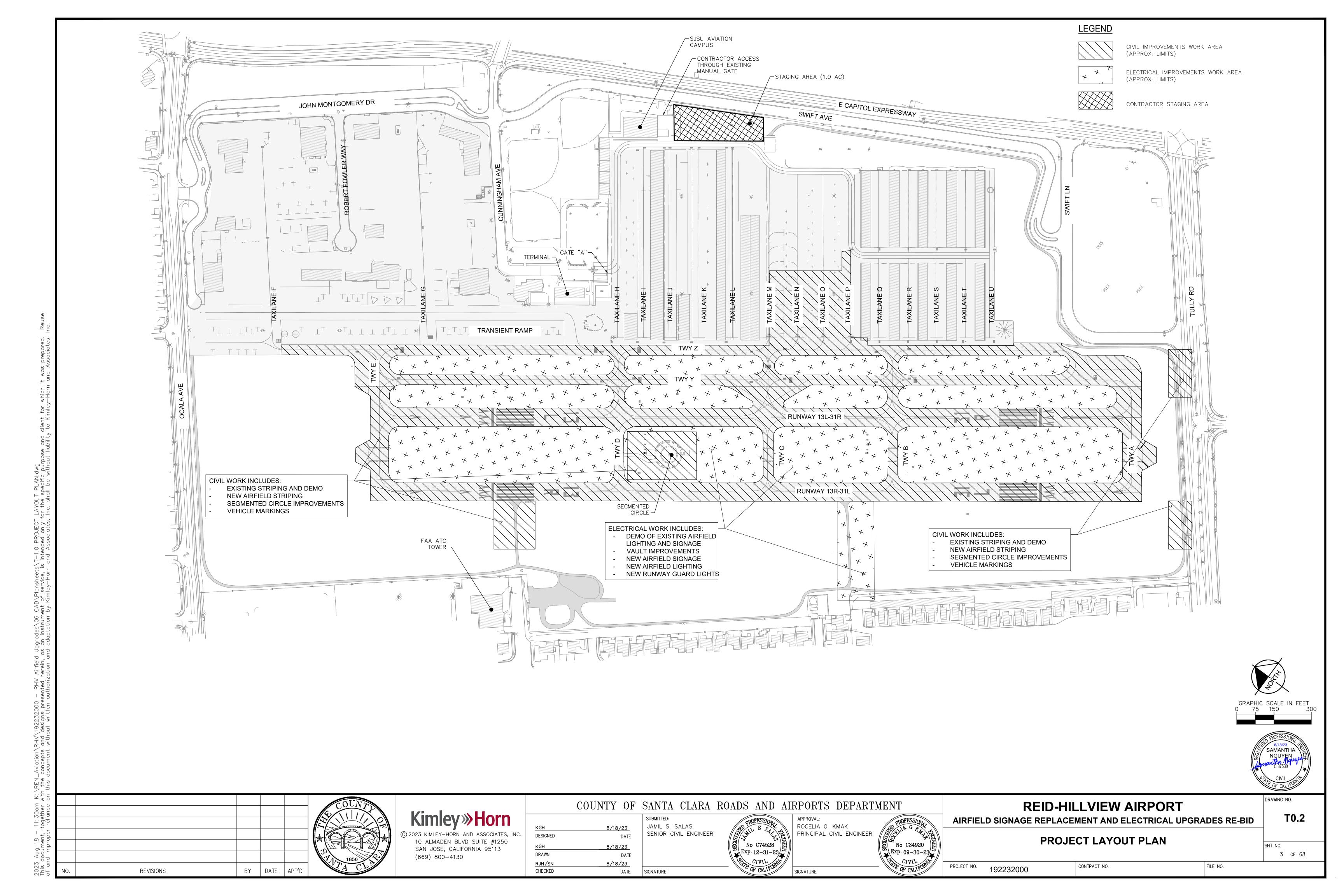
SUBMITTED: 8/18/23 DESIGNED DATE 10 ALMADEN BLVD SUITE #1250 8/18/23 SAN JOSE, CALIFORNIA 95113 DRAWN DATE RJH/SN 8/18/23 CHECKED DATE SIGNATURE

PRVC

PSI

ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C74528 Exp. 12-31-23/

**SIGNATURE** 



#### **GENERAL NOTES**

**LEGEND** 

POINT ID

POINT ID

TWY Z

BP: 0+00.00

N: 1948203.6850

E: 6177139.9037

RWY 13L END POINT

RWY 13R END POINT

STA: 1+00.30

N: 1947538.3179

E: 6176920.9597

STA: 1+00.00

N: 1947716.4823

E: 6177162.8624

0+00

0+00

0+00

1. THE CONTRACTOR SHALL VERIFY THE ACCURACY OF ALL CONTROL POINTS ESTABLISHED BY THE ENGINEER AS SHOWN ON THESE PLANS. IF ERRORS ARE DISCOVERED DURING THIS VERIFICATION PROCESS, AND THE CONTROL POINTS DO NOT AGREE WITH THE GEOMETRY SHOWN IN THE PLANS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING, EXPLAINING THE ISSUE IN DETAIL. UPON COMPLETION OF THIS VERIFICATION PROCESS, THE CONTRACTOR'S REGISTERED LAND SURVEYOR SHALL CERTIFY, IN WRITING, THAT ALL CONTROL POINTS ESTABLISHED BY THE ENGINEER ARE ACCEPTABLE AND ADEQUATE TO ALLOW THE CONTRACTOR'S CONSTRUCTION STAKING TO MEET THE ACCURACY REQUIREMENTS OF THE SPECIFICATIONS.

#### HORIZONTAL AND VERTICAL CONTROL

- 1. THE HORIZONTAL DATUM FOR THIS PROJECT IS BASED ON THE NORTH AMERICAN DATUM OF 1983, CONVERTED TO THE CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 3, SURVEY FEET UNITS.
- 2. THIS SURVEY IS BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).

PRIMARY CONTROL POINT

JOHN MONTGOMERY DR

SECONDARY CONTROL POINT

#### **SURVEY**

SURVEY PREPARED FOR: KIMLEY-HORN AND ASSOCIATES, INC. SURVEY DATE: NOVEMBER 2021

SURVEY PREPARED BY: SANTA CLARA COUNTY SURVEY SANTA CLARA, CA 95110

(408) 573-2466 CONTACT: JON OSWALD, FIELD SURVEY TECHNICIAN II

#### CONTROL POINT DATA

10+00\_

RWY 13L-DT

STA: 5+99.00

RWY 13R-DT

STA: 5+99.30

N: 1947136.2443

E: 6177216.4894

N: 1947314.3783

E: 6177458.3480

TWY D - EAST

<sup>-</sup>N:1947087.7935<sup>-</sup>

E: 6177997.1409

STA: 11+00.64

BP: 0+00.00

OFF: 0.00' = TWY D - EAST

EP: 3+00.00

\_10+00/

\_10+00\_

	CONTROL	POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	
	TYPE	ID	NORTHING	EASTING	ELEVATION		
1	PRIMARY	HS5437	1947535.338	6177118.155	121.452'	FAA RHV A	
2	SECONDARY	548	1946886.692	6177615.083	123.807'	_	
3	PRIMARY	HS5426	1946402.226	6177981.365	126.916'	ARP 1969 RHV	
4	SECONDARY	768	1945205.536	6178798.753	132.911'	_	

	RUNWAY END POINT DATA TABLE							
F	Point #	Elevation	Northing	Easting	Description			
	1	122.80	1947538.32	6176920.96	RWY 13R			
	2	123.70	1947314.38	6177458.35	RWY 13L -DT			
	3	123.60	1947716.48	6177162.86	RWY 13L			
	4	135.40	1945218.64	6178998.39	RWY 31R			
	5	133.30	1945549.03	6178755.60	RWY 31R-DT			
	6	123.60	1947136.24	6177216.49	RWY 13R-DT			
	7	133.90	1945041.59	6178756.09	RWY 31L			
	8	130.90	1945371.14	6178513.86	RWY 31L - DT			

-DETAIL 2

23+00

-RWY 31R END POINT

STA: 31+99.74

E: 6178998.3883

8 N: 1945218.6426

33+00

TAXIWAY Z2

35+00

**Q** ALIGNMENT

TAXIWAY Z3

€ ALIGNMENT ¬

RWY 13L-31R30+00

30+00

TWY A1

30+00

RWY 31L END POINT

STA: 31+98.91

N: 1945041.5882

E: 6178756.0913

BP: 0+00.00 N: 1945051.8850 E: 6178748.5060

TAXIWAY Z4

**Q** ALIGNMENT-

30+00

STA: 27+89.99

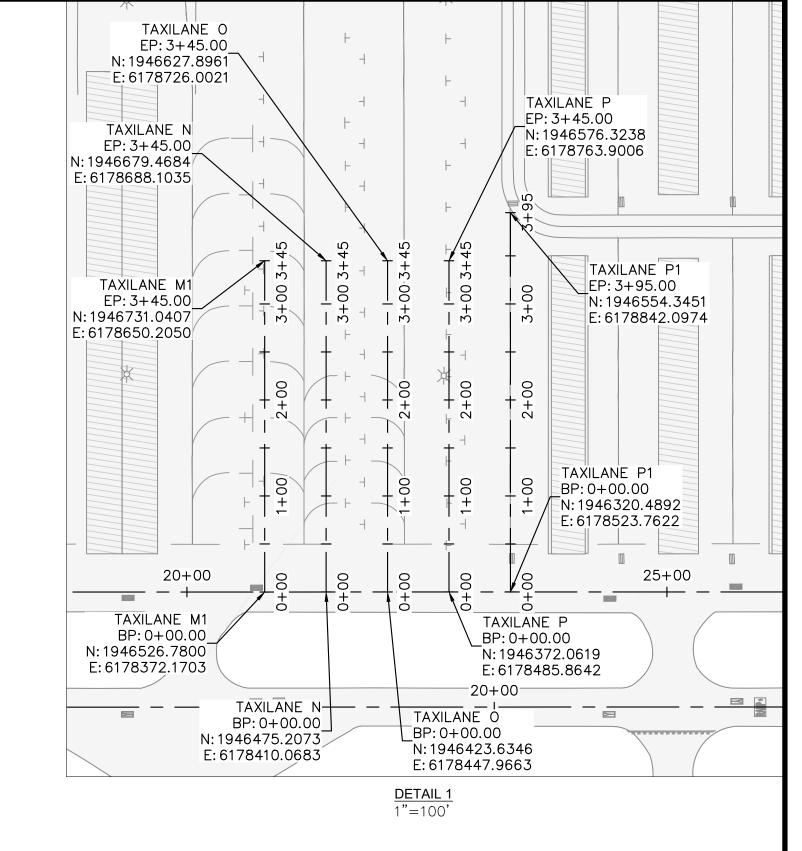
OFF: 296.73'LT

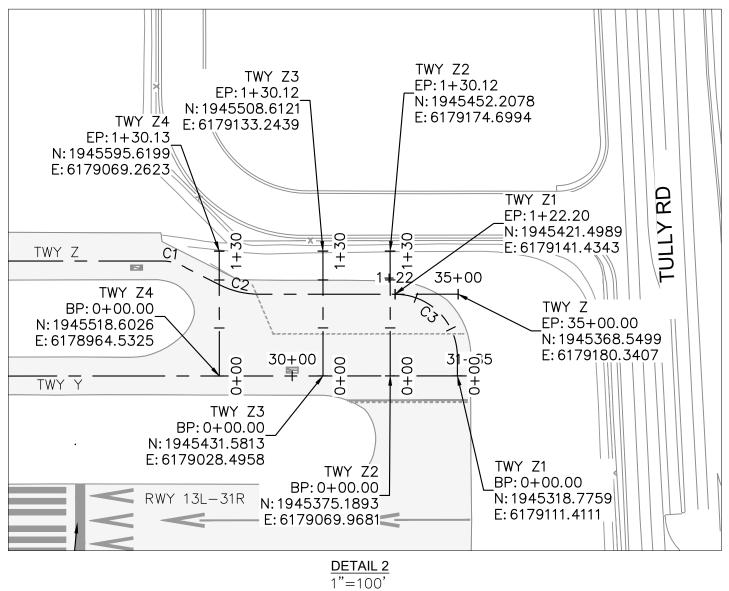
RWY 31L-DT

STA: 27+89.91

N: 1945371.1434

E: 6178513.8635





CURVE TABLE							
CURVE RADIUS LENGTH DELTA TANGENT							
C1	47.00'	25.53'	31°03'36"	13.09'			
C2	90.00'	41.91'	26°40'43"	21.34'			
C3	65.00'	102.11	89°58'00"	64.99'			



GRAPHIC SCALE IN FEET
O 75 150 30

FILE NO.



DRAWING NO.

					COUNT
					1850
NO.	REVISIONS	BY	DATE	APP'D	TA CLA

TWY Z5 EP: 1+50.00 N: 1947842.9787 E: 6177442.1962

OFF: 150.01'LT

\_BP: 0+00.00\_

=TWY Z5

STA: 1+14.81

OFF: 0.01'LT

BP: 0+00.00

N: 1947526.6747 E: 6176929.5176

STA: 3+00.43

=TWY E3

(669) 800-4130

STA: 14+06.83

STA: 2+70.02

STA: 10+86.73

\_STA: 1+50.02

STA: 10+99.42

=TWY D - WEST

OFF: 0.00'RT

EP: 3+00.32

RWY 13R-31L

BP: 0+00.00

STA: 33+00.00

=TWY D - WEST

N: 1946733.2684

E: 6177512.6835

=TWY D - EAST

OFF: 0.00'

=TWY D - EAST

OFF: 0.02'LT

8/18/23 DESIGNED DATE 8/18/23 DRAWN DATE RJH/SN 8/18/23 CHECKED DATE

E CAPITOL EXPRESSWAY

-DETAIL 1

TWY B - EAST

∕N: 1946198.8046

E: 6178644.2058

\_EP: 2+95.00

=TWY B - EAST

STA: 22+00.60

□ N m EP: 3+00.20

STA: 33+00.00

BP: 0+00.00

=TWY B - WEST

\_N: 1945846.0018

E: 6178164.8359

OFF:0.00' =TWY B - WEST

STA: 1+50.15 \_25+00\_

25+00

25+00

-TAXILANE P

STA: 22+00.17

OFF:0.00' =TWY B - EAST—

20+00

RUNWAY 13L-31R

RUNWAY 13F29#L00

STA: 17+00.80

ຸດ EP: 3+00.26

STA: 33+00.00

BP: 0+00.00

=TWY C - WEST

N: 1946248.6927

E: 6177868.8534

OFF: 0.00' =TWY C - WEST

BP: 0+00.00

**Q** ALIGNMENT

TAXILANE P1

Q ALIGNMENT

SWIFT AVE

TAXILANE O

TAXILANE N

Q ALIGNMENT <sup>→</sup>

TAXILANE M1

€ ALIGNMENT —

TWY C - EAST

N: 1946603.0035

E: 6178353.3868

Twy Y

-STA: 16+88.33

STA: 1+50.00

\_15+00\_

STA: 17+02.24

OFF: 0.00' =TWY C - EAST

<sup>15+00</sup>

15+00

BP: 0+00.00

HS5426

EP: 3+00.00

SUBMITTED: JAMIL S. SALAS SENIOR CIVIL ENGINEER No C74528 SIGNATURE

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 Exp. 09-30-23

**REID-HILLVIEW AIRPORT** AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

**SURVEY CONTROL PLAN** 

SHT NO. 4 OF 68

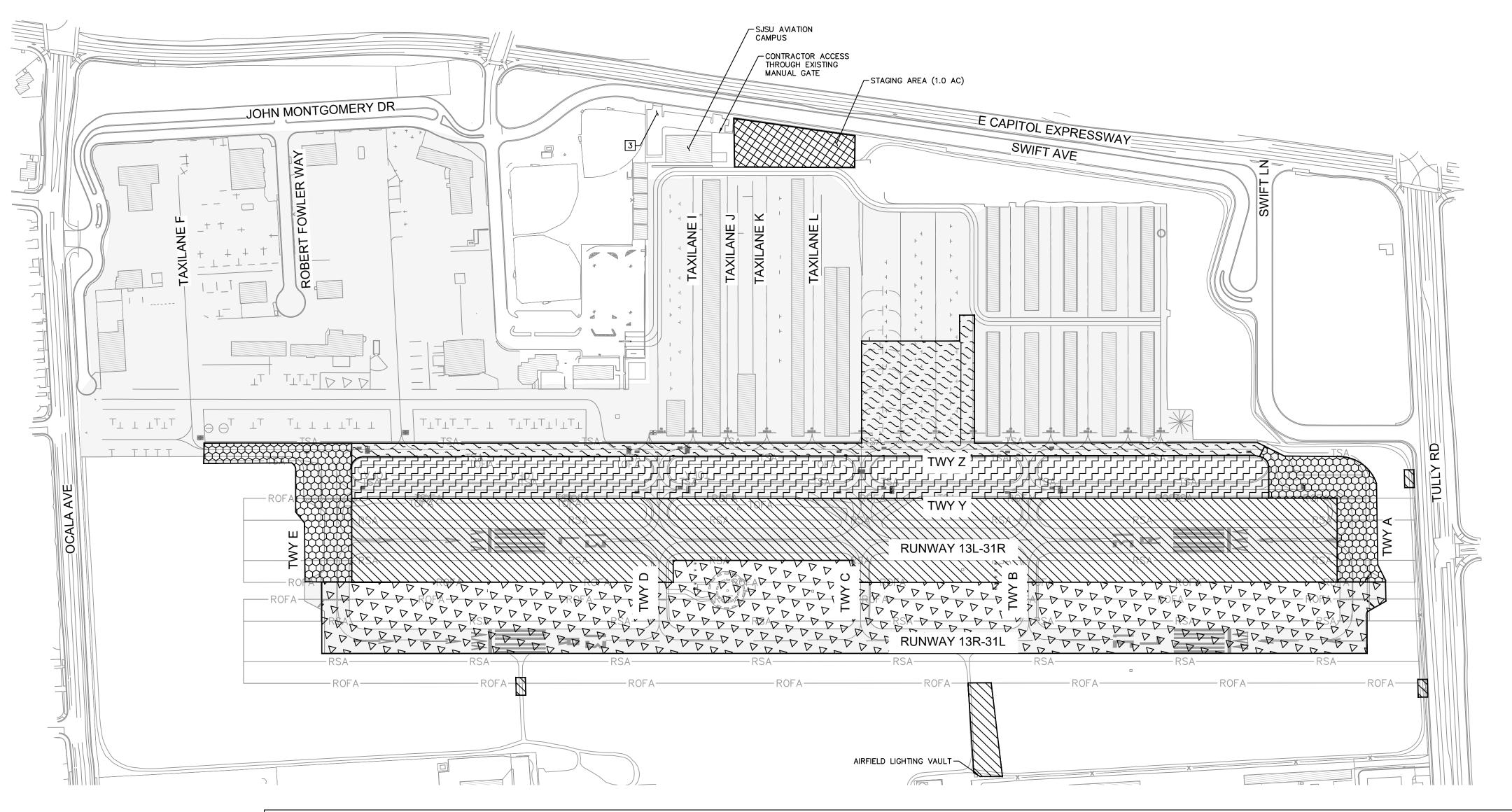
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)2023 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113

**Exp.** 12−31−23/≈

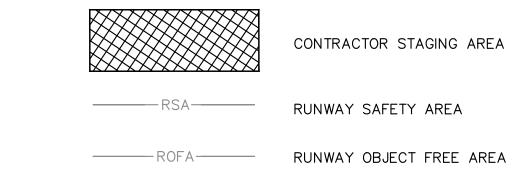
PROJECT NO. CONTRACT NO.

192232000



CONSTRUCTION PHASING SUMMARY, AIRFIELD RESTRICTIONS & CLOSURES							
LEGEND	WORK AREA PHASE	PHASE DURATION (CALENDAR DAYS)	CONCURRENT PHASES	AIRFIELD OPERATION IMPACT (TAXIWAY/APRON/RUNWAY CLOSED TO AIRCRAFT OPERATIONS)	DESCRIPTION OF WORK	WORK HOURS	NOTES
	0	90	1, 2, 3, 4, & 5	SEE PHASES 1-4	MOBILIZATION		
	1	14	NONE	PARTIAL CLOSURE OF: 1. TWY Z BETWEEN TWY E AND TWY Z4 2. TWY Z4, B, C, AND D BETWEEN TWY Y AND TWY Z	DEMOLISH AND INSTALL TAXILANE MARKINGS     DEMOLISH EXISTING TIE DOWN MARKINGS	DAYTIME WORK (SEVEN DAYS A WEEK) 8 HOURS PER DAY (0700 TO 1530)	COORDINATE ALL CLOSURES WITH RHV PROJECT MANAGER. CLOSURES OF TWYS REQUIRE A MINIMUM OF TWO WEEK NOTICE. CONTRACTOR TO REMAIN CLEAR OF ACTIVE TOFA'S.
	2	14	4 & 5	FULL CLOSURE OF: 1. RWY 13L-31R 2. TWY B, C, AND D BETWEEN TWY Z AND RWY 13R-31L	DEMOLISH AND INSTALL NEW AIRFIELD MARKINGS     INSTALL LED RUNWAY EDGE/END LIGHT, CABLING AND ISOLATION TRANSFORMER ON EXISTING AND NEW BASE CAN     INSTALL AIRFIELD CABLING FOR RUNWAY AND TAXIWAY LIGHTING CIRCUIT     INSTALL LED AIRFIELD GUIDANCE SIGN     INSTALL DIRECT BURIED CONDUIT AND CABLING     INSTALL BASE CAN WITH BLANK STEEL COVER     INSTALL ELEVATED RUNWAY GUARD LIGHT     AIRFIELD LIGHTING VAULT IMPROVEMENTS     INSTALL VEHICLE ROADWAY SIGNS AND MARKINGS	DAYTIME WORK (SEVEN DAYS A WEEK) 8 HOURS PER DAY (0700 TO 1530)	COORDINATE ALL CLOSURES WITH RHV PROJECT MANAGER. CLOSURES OF RWY AND TWY REQUIRE A MINIMUM OF TWO WEEK NOTICE. CONTRACTOR TO REMAIN CLEAR OF ACTIVE TOFA'S.
	3	14	NONE	FULL CLOSURE OF: 1. RWY 13R-31L 2. TWY A, B, C, D, AND E WEST OF RWY 13L-31R	DEMOLISH AND INSTALL NEW AIRFIELD MARKINGS     INSTALL AIRFIELD GUIDANCE SIGN ON NEW CONCRETE FOUNDATION     INSTALL RETRO-REFLECTIVE AIRFIELD GUIDANCE SIGN     INSTALL DIRECT BURIED CONDUIT AND CABLING     INSTALL BASE CAN WITH BLANK STEEL COVER     INSTALL ELEVATED RUNWAY GUARD LIGHT WITH NEW BASECAN AND CONCRETE FOUNDATION     SEGMENTED CIRCLE RECONFIGURATION     AIRFIELD LIGHTING VAULT IMPROVEMENTS	DAYTIME WORK (SEVEN DAYS A WEEK) 8 HOURS PER DAY (0700 TO 1530)	COORDINATE ALL CLOSURES WITH RHV PROJECT MANAGER. CLOSURES OF TWYS REQUIRE A MINIMUM OF TWO WEEK NOTICE. CONTRACTOR TO REMAIN CLEAR OF ACTIVE TOFA'S.
	1 1 1 4	14	2	FULL CLOSURE OF: 1. TWY A 2. TWY E	DEMOLISH AND INSTALL NEW AIRFIELD MARKINGS     INSTALL ELEVATED RUNWAY EDGE LIGHT WITH CABLING AND ISOLATION TRANSFORMER ON EXISTING BASECAN     INSTALL CABLING FOR RUNWAY AND TAXIWAY LIGHTING CIRCUITS IN EXISTING CONDUIT OR DUCTBANK     INSTALL AIRFIELD GUIDANCE SIGN ON NEW CONCRETE FOUNDATION     INSTALL DIRECT BURIED CONDUIT AND CABLING     INSTALL DIRECTIONAL BORE CONDUIT AND CABLING     INSTALL BASE CAN WITH BLANK STEEL COVER     INSTALL ELEVATED RUNWAY GUARD LIGHT WITH NEW BASECAN AND CONCRETE FOUNDATION     INSTALL ELEVATED RUNWAY END LIGHT AND CABLING ON NEW BASE CAN WITH NEW ISOLATION TRANSFORMER     AIRFIELD LIGHTING VAULT IMPROVEMENTS	NIGHT WORK (SEVEN DAYS A WEEK) 8 HOURS PER NIGHT (2200 TO 0600)	COORDINATE ALL CLOSURES WITH RHV PROJECT MANAGER. CLOSURES OF TWYS REQUIRE A MINIMUM OF TWO WEEK NOTICE. CONTRACTOR TO REMAIN CLEAR OF ACTIVE TOFA'S.
	5	14	2 (WITH APPROVAL FROM ENGINEER)	FULL CLOSURE OF:  1. TWY Y BETWEEN TWY A AND TWY E  2. TWYS B, C, AND D BETWEEN TWY Z  AND RWY 13L-31R	<ul> <li>INSTALL CABLING AND ISOLATION TRANSFORMER IN EXISTING TAXIWAY EDGE LIGHT BASE CAN</li> <li>INSTALL CABLING FOR TAXIWAY LIGHTING CIRCUIT IN EXISTING CONDUIT OR DUCTBANK</li> <li>INSTALL AIRFIELD GUIDANCE SIGN ON NEW CONCRETE FOUNDATION</li> <li>INSTALL BASECAN WITH BLANK STEEL COVER</li> <li>INSTALL ELEVATED RUNWAY GUARD LIGHT WITH NEW BASECAN AND CONCRETE FOUNDATION</li> <li>INSTALL LED AIRFIELD GUIDANCE SIGN</li> <li>INSTALL DIRECT BURIED CONDUIT AND CABLING</li> <li>INSTALL DIRECTIONAL BORE CONDUIT AND CABLING</li> <li>INSTALL AIRFIELD CABLING FOR RUNWAY LIGHTING CIRCUIT</li> <li>AIRFIELD LIGHTING VAULT IMPROVEMENTS</li> </ul>	DAYTIME WORK (SEVEN DAYS A WEEK) 8 HOURS PER DAY (0700 TO 1530)	COORDINATE ALL CLOSURES WITH RHV PROJECT MANAGER. CLOSURES OF TWYS REQUIRE A MINIMUM OF TWO WEEK NOTICE. CONTRACTOR TO REMAIN CLEAR OF ACTIVE TOFA'S.

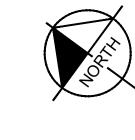
#### **LEGEND**



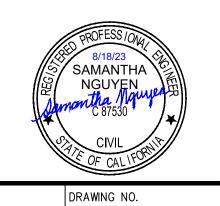
TOFA TAXIWAY OBJECT FREE AREA

#### **GENERAL NOTES**

- 1. ALL MOVEMENT AREAS WITHIN THE AOA WHICH ARE NOT DESIGNATED TO BE CLOSED TO AIRCRAFT TRAFFIC SHALL REMAIN OPEN TO AIRCRAFT OPERATIONS AT ALL TIMES DURING CONSTRUCTION.
- 2. CONTRACTOR SHALL ENSURE CONTINUOUS ACCESS FOR AIRPORT VEHICLES AND ARFF VEHICLES ALONG THE VSR DURING THE CONSTRUCTION
- 3. CONTRACTOR SHALL ENSURE ALL AIRFIELD ELECTRICAL CIRCUITS FOR MOVEMENT AREAS AND NON-MOVEMENT AREAS AND WHICH ARE OPEN TO AIRCRAFT OPERATIONS SHALL REMAIN IN SERVICE AND ENERGIZED DURING THE HOURS OF LOW VISIBILITY.
- 4. CONTRACTOR TRAFFIC TO REMAIN ON DESIGNATED HAUL ROUTES
- 5. AIRPORT OPERATIONS MAY REQUIRE SAFETY MEASURES IN ADDITION TO THOSE SHOWN ON THE PLANS. CONTRACTOR SHALL COMPLY AS DIRECTED BY AIRPORT OPERATIONS AT NO ADDITIONAL COST TO THE
- 6. CONTRACTOR SHALL NOTIFY RHV A MINIMUM OF 14 CALENDAR DAYS BEFORE REQUESTING TO COMMENCE WORK IN THIS PHASE AND RECEIVE WRITTEN APPROVAL TO PROCEED.
- 7. A PAVEMENT CURE PERIOD IS REQUIRED FOR ASPHALT PAVEMENT PRIOR TO THE INSTALLATION OF FINAL PAVEMENT MARKINGS. INSTALLATION OF FINAL PAVEMENT MARKINGS IS TO BE COORDINATED WITH SJC OPERATIONS IF IT FALLS OUTSIDE OF ORIGINAL WORK PHASE DURATIONS.
- 8. CONTRACTOR TO REMAIN OFF ANY ACTIVE MOVEMENT AREA UNLESS OTHERWISE APPROVED BY ATCT.







					COUNT
					1850
NO.	REVISIONS	BY	DATE	APP'D	TA CLE
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10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

8/18/23 DESIGNED DATE KGH DRAWN 8/18/23 RJH/SN CHECKED 8/18/23

DATE

SUBMITTED: JAMIL S. SALAS SENIOR CIVIL ENGINEER No C74528 SIGNATURE

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT

ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920

**REID-HILLVIEW AIRPORT** AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

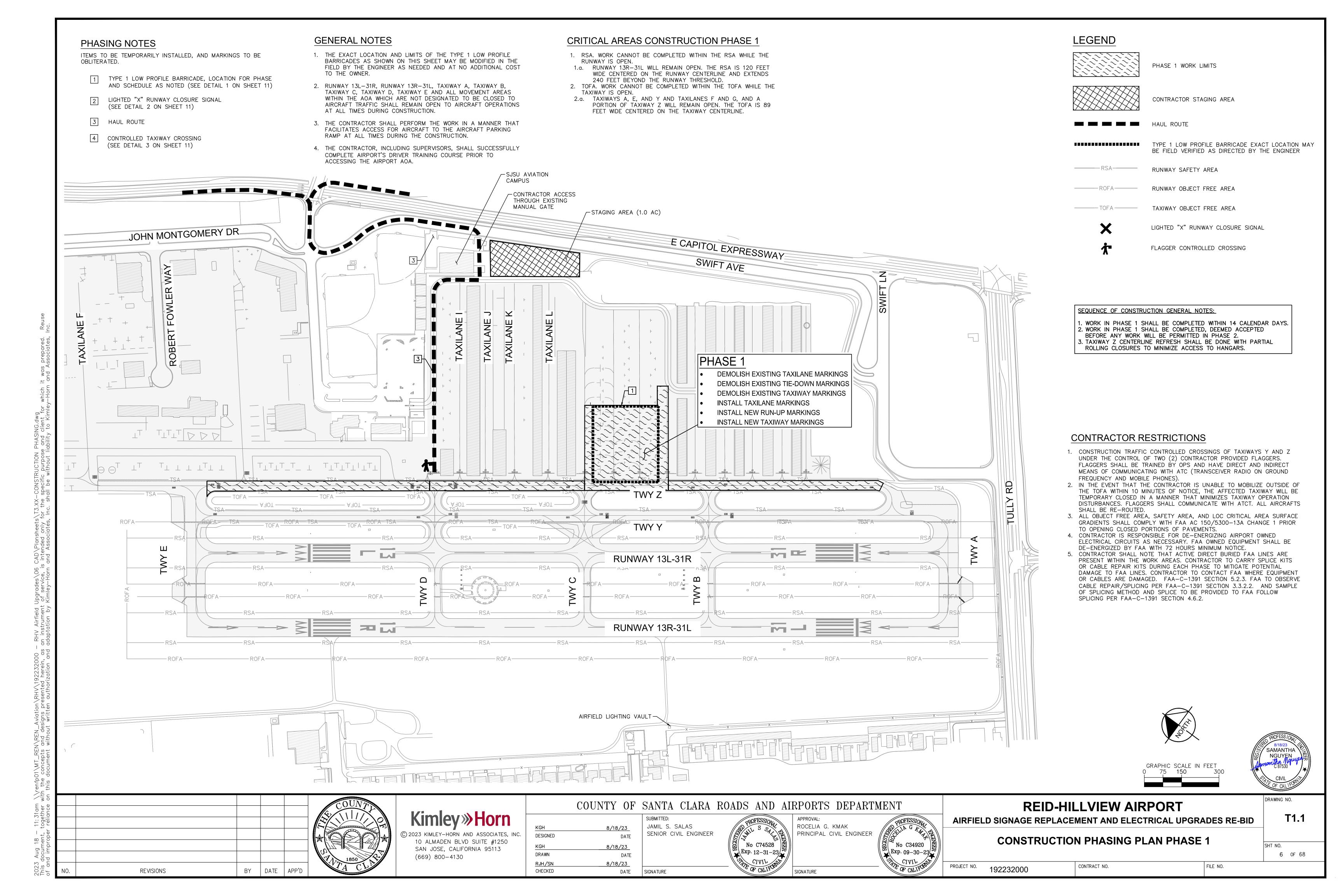
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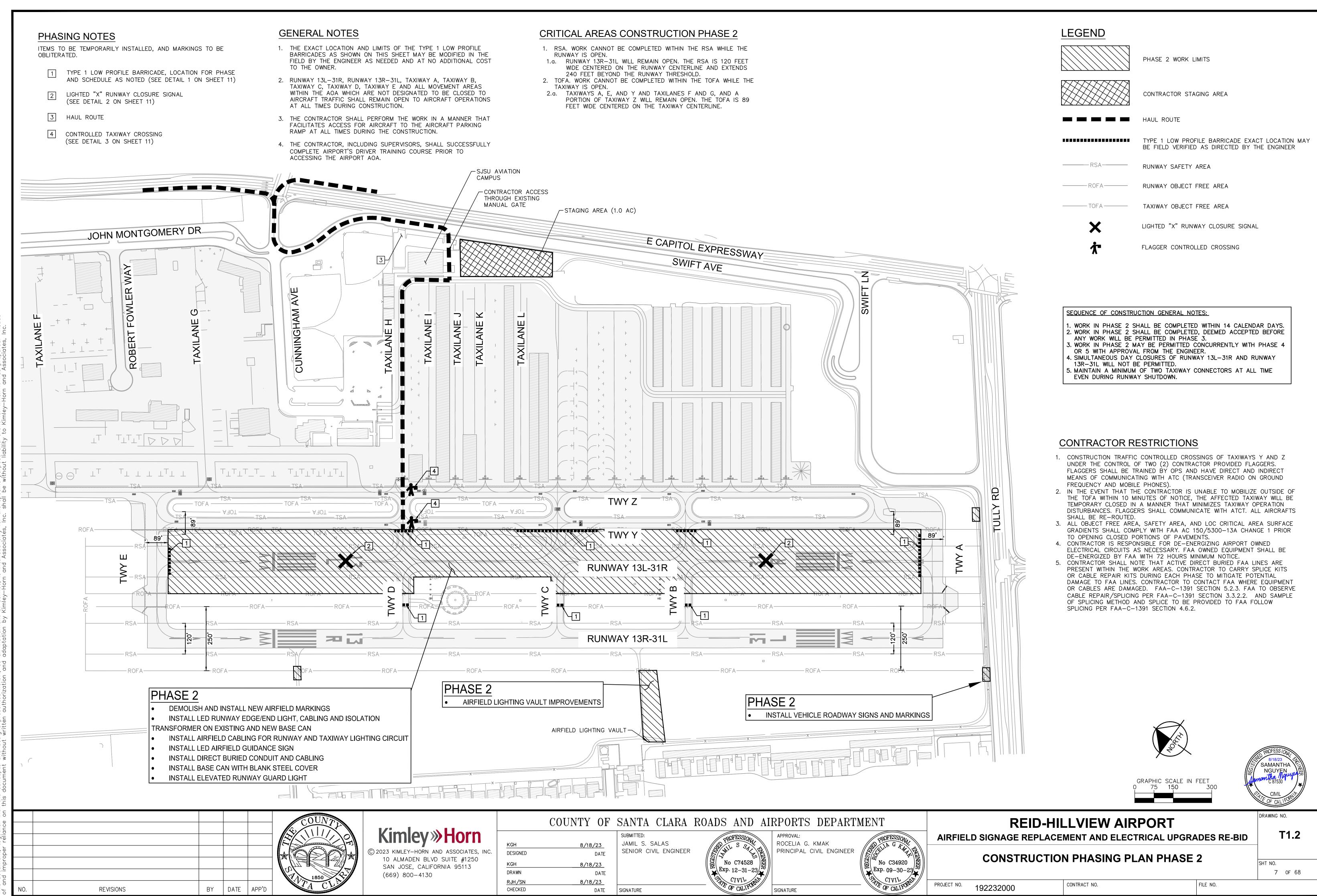
**CONSTRUCTION PHASING PLAN OVERALL** 

SHT NO. 5 OF 68

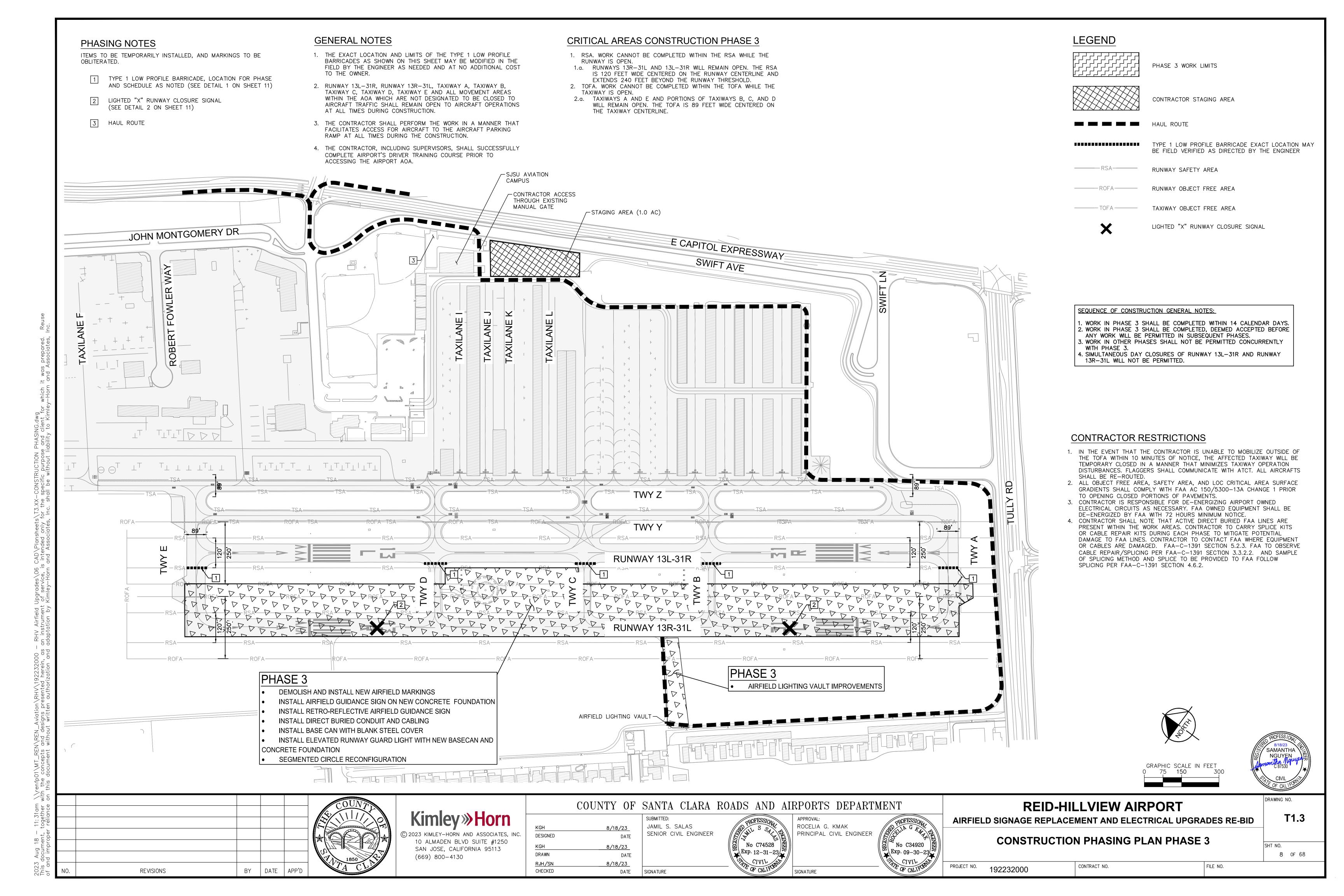
PROJECT NO. CONTRACT NO. 192232000

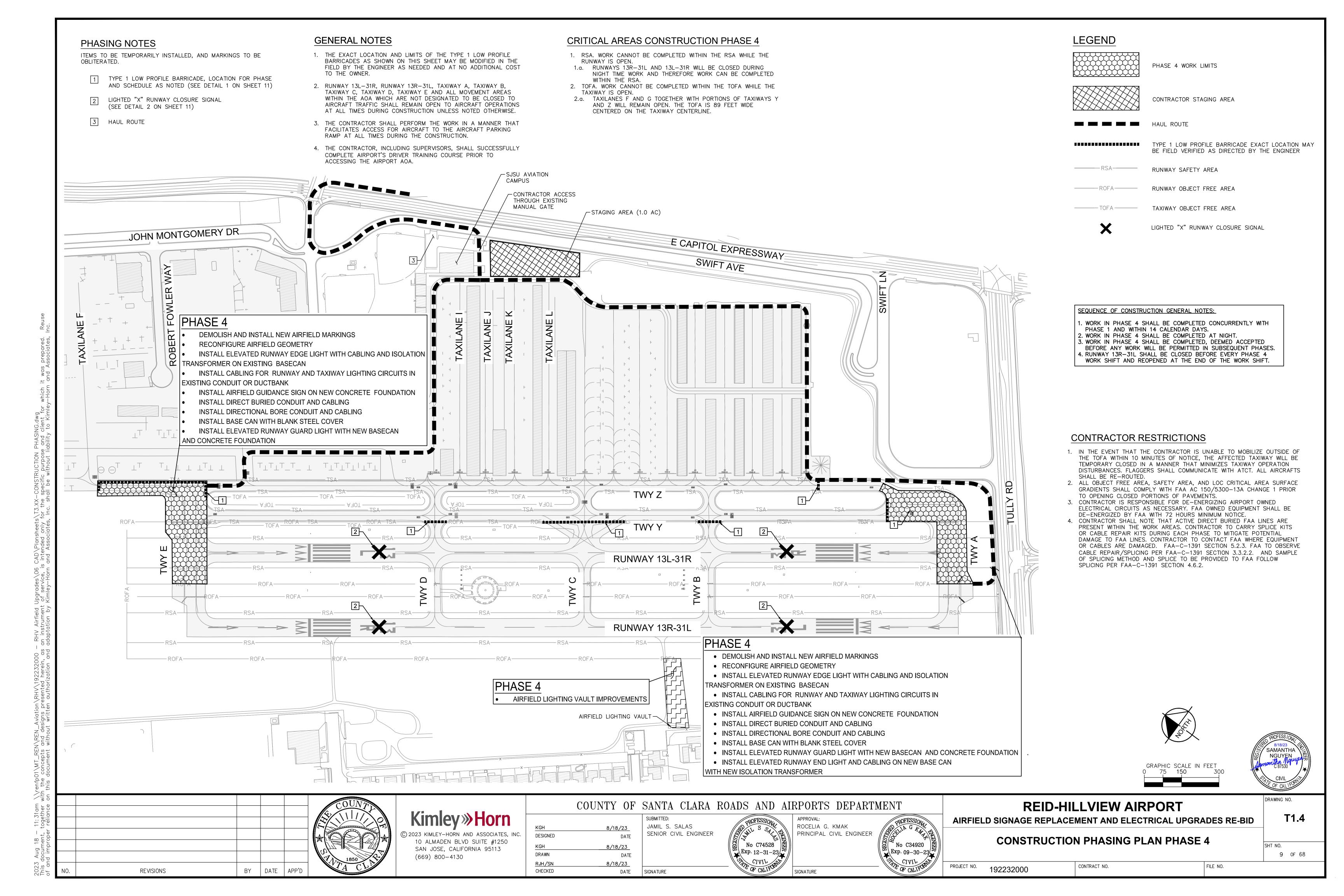
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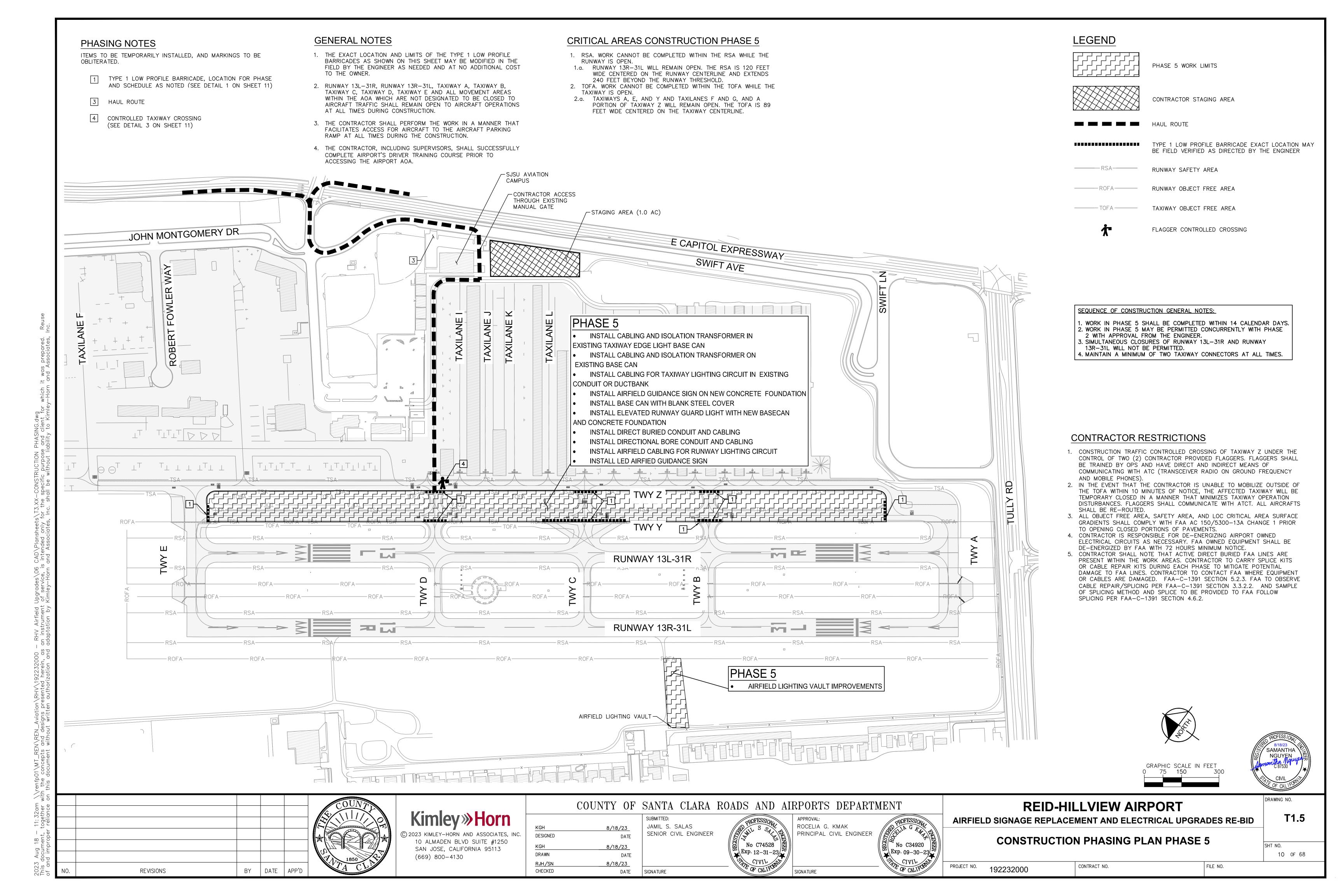




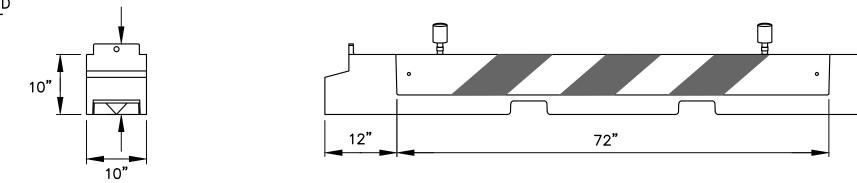
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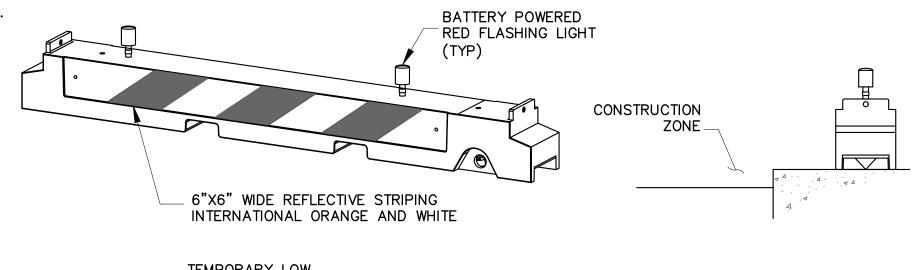


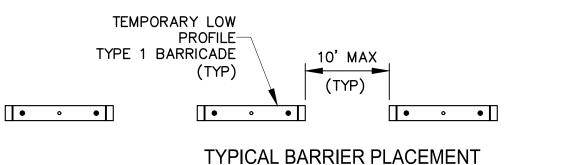




- 2. THE AIRPORT RESERVES THE RIGHT TO CHANGE THE LIMITS AND SEQUENCE OF ANY CONSTRUCTION PHASE DURING THE PROJECT FOR AIRPORT OPERATIONAL PURPOSES. 3. THE CONTRACTOR SHALL COOPERATE AND FULLY COMPLY WITH ANY SNS OPERATIONS PHASING MODIFICATIONS.
- 4. THE CONTRACTOR SHALL DEVELOP AND SUBMIT A CONSTRUCTION OPERATIONAL SAFETY PLAN TO THE OWNER AND ENGINEER FOR APPROVAL PRIOR TO COMMENCING WORK.
- 5. THIS SAFETY PLAN SHALL INCORPORATE THE REQUIREMENTS AND CRITERIA AS IDENTIFIED IN THE CONTRACT DOCUMENTS, AND SHALL COMPLY WITH THE REQUIREMENTS OF AC 150/5370-2F.
- 6. ALL AOA PAVEMENT SURFACES SHALL BE OPEN FOR AIRCRAFT OPERATIONS AT ALL TIMES OTHER THAN THE DESIGNATED CLOSURES.
- 7. ALL CONSTRUCTION TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. THE CONTRACTOR SHALL NOT MOVE EQUIPMENT ONTO THE ACTIVE AOA WITHOUT RHV OPERATIONS APPROVAL. DELAYS RESULTING FROM AIRCRAFT MOVEMENTS SHOULD BE EXPECTED, EXPENSES OR COSTS RESULTING FROM SUCH DELAYS ARE INCIDENTAL TO THE PROJECT. ADJUSTMENTS FOR ADDITIONAL COMPENSATION AND TIME WILL NOT BE MADE FOR TIME LOST IN WORK AREAS CONTIGUOUS TO TAXIWAYS AND RUNWAYS DUE TO AIRCRAFT TRAFFIC.
- 8. THE CONTRACTOR SHALL NOT CROSS AN ACTIVE RUNWAY OR TAXIWAY AND SHALL NOT ENCROACH INTO AN ACTIVE RSA OR TSA AT ANY TIME UNLESS ESCORTED BY RHV OPERATIONS AS DEFINED IN THE SPECIFICATIONS. VIOLATION COULD RESULT IN PERMANENT EJECTION FROM THE AIRPORT PROPERTY AND/OR AN ASSESSMENT OF FINES.
- 9. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT ADMINISTRATION STAFF PER THE FOLLOWING LIST; THIS LIST SHOULD NOT BE CONSIDERED A COMPREHENSIVE OR COMPLETE LIST OF ALL COORDINATION REQUIRED BUT SHALL BE TAKEN AS THE MINIMUM: a. PRIOR TO THE START OF DAILY OR NIGHT TIME CONSTRUCTION ACTIVITIES — A MINIMUM OF 1—HOUR PRIOR TO START TIME.
  - b. FOR ALL ACTIVE RUNWAY AND TAXIWAY CLOSURES A MINIMUM OF 72—HOURS PRIOR TO THE REQUESTED CLOSURE TIME. c. FOR ALL WORK REQUIRING ISSUANCE OF A "NOTAM" - A MINIMUM OF 72-HOURS PRIOR NOTAM START TIME.
- 10. LOW PROFILE BARRICADES AS SHOWN ON THE PLANS SHALL BE USED IN THE DELINEATION OF THE CONSTRUCTION AREAS OR CLOSED AIRFIELD PAVEMENT WITHIN THE AOA.
- 11. THE CONTRACTOR SHALL PROVIDE RHV AND THE ENGINEER, A SEVENTY-TWO (72) HOUR NOTICE PRIOR TO ANY REQUESTED AIRFIELD LIGHTING INTERRUPTIONS. RUNWAY CIRCUITS SHALL REMAIN OPERATIONAL AT ALL TIMES EXCEPT DURING A RUNWAY CLOSURE.
- 12. PRIOR TO STARTING WORK IN ANY SCHEDULE, ALL APPLICABLE AIRFIELD GUIDANCE SIGNS THAT DIRECT AIRCRAFT INTO THE WORK ZONES SHALL BE MASKED OR DE-ENERGIZED, AND ALL RUNWAY AND TAXIWAY LIGHTING SHALL BE DE-ENERGIZED FOR THE LIMITS OF THE WORK ZONES ONLY, ALL LEAD-IN AIRFIELD MARKINGS AS CALLED FOR ON THE PLANS SHALL BE OBLITERATED, AND ALL BARRICADES AND TEMPORARY MARKINGS SHALL BE INSTALLED AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
- 13. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN BARRICADES. TEMPORARY RUNWAY AND TAXIWAY LIGHTING. TEMPORARY RUNWAY AND TAXIWAY SIGNS. FOR ALL RUNWAY/TAXIWAY CLOSURES AS INDICATED ON THE PHASING PLANS AND DETAILS. UPON COMPLETION OF EACH PHASE, THE CONTRACTOR SHALL REMOVE THE ASSOCIATED BARRICADES AND UNCOVER AIRFIELD SIGNS AND EDGE
- 14. ALL AIRPORT PROPERTY, INCLUDING EDGE LIGHTS AND NAVAID EQUIPMENT, SHALL BE PROTECTED AND REMAIN IN PLACE UNLESS OTHERWISE NOTED ON THE PLANS. THE CONTRACTOR SHALL COORDINATE WITH RHV OPERATIONS AND MAINTENANCE STAFF PRIOR TO MASKING OR DE-ENERGIZING ANY EXISTING GUIDANCE SIGNS AND/OR EDGE LIGHTS AS INDICATED. DAMAGE TO SIGNS, EDGE LIGHTS, NAVAIDS OR OTHER AIRPORT PROPERTY AS A RESULT OF CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S SOLE EXPENSE AND TO THE SATISFACTION OF THE ENGINEER.
- 15. THE CONTRACTOR SHALL MAINTAIN ALL PAVEMENTS AND HAUL ROUTES IN A CLEAN AND SUITABLE CONDITION FOR AIRCRAFT MOVEMENT BY MEANS OF VACUUM SWEEPING. THE CONTRACTOR SHALL HAVE AN OPERATIONAL VACUUM SWEEPER AT THE WORK AREAS AT ALL TIMES TO IMMEDIATELY REMOVE ANY FOREIGN OBJECT DEBRIS, (FOD).
- 16. HAUL ROUTE LOCATIONS VARY FOR EACH PHASE AS INDICATED ON THE PHASING PLAN. THE ROUTES AND AOA ACCESS LOCATIONS MAY NEED TO CHANGE DUE TO RHV OPERATIONAL NEEDS, THE CONTRACTOR SHALL ADJUST AS REQUIRED OR REQUESTED BY RHV OPERATIONS. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE OFF SITE HAUL ROUTES WITH THE JURISDICTIONAL AGENCY AND OBTAIN ANY REQUIRED PERMITS.
- 17. THE CONTRACTOR SHALL COMPLY WITH ALL CITY, COUNTY AND STATE TRAFFIC REGULATIONS CONCERNING THE USE OF STREETS AND ROADWAYS FOR HAULING. ANY DAMAGE TO THE ROADWAYS DUE TO THE CONTRACTORS EQUIPMENT OR HAULING OPERATIONS SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AND AT NO ADDITIONAL COST TO THE
- 18. THE CONTRACTOR SHALL MAINTAIN 2-WAY TRAFFIC ON ALL EXISTING AIRPORT SERVICE AND PERIMETER ROADS AT ALL TIMES DURING THE CONSTRUCTION. SERVICE ROADS USED AS CONTRACTOR ROUTES WILL BE USED BY OTHER AIRPORT VEHICLES. THE CONTRACTOR SHALL NOT INTERFERE WITH OR IMPEDE AIRPORT VEHICLE TRAFFIC AND SHALL YIELD TO ARFF VEHICLES AND RHV OPERATIONS VEHICLES ALONG ALL AIRPORT SERVICE ROADS.
- 19. THE CONTRACTOR SHALL REPAIR ANY DAMAGE CAUSED AS A RESULT OF THE CONTRACTOR'S ACTIVITIES TO THE AIR SIDE HAUL ROUTES AND/OR AIRFIELD PAVEMENT AND EXISTING IMPROVEMENTS ADJACENT TO THE CONSTRUCTION LIMITS. REPAIRS OR ANY REQUIRED REPLACEMENTS SHALL BE CARRIED OUT PRIOR TO SUBSTANTIAL COMPLETION OF THIS PROJECT AND/OR DURING THE PROJECT WHEN DEEMED NECESSARY BY SNS OPERATIONS, TO THE SATISFACTION OF THE ENGINEER AND AT THE SOLE EXPENSE OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ALL CONSTRUCTION TRAFFIC WITHIN THE SPECIFIED HAUL ROUTES AS SHOWN ON THE CONSTRUCTION PHASING PLAN OR AS DIRECTED BY THE ENGINEER.
- 20. THE CONTRACTOR IS RESPONSIBLE FOR THE OPERATION, INCLUDING SUPPLYING ALL FUEL AND RE-FUELING OPERATIONS, AND THE INITIAL SERVICING AND ALL SERVICING AND MAINTENANCE REQUIRED DURING THE CONSTRUCTION PERIOD OF THE FOUR (4) CONTRACTOR PROVIDED LIGHTED "X" RUNWAY CLOSURE SIGNALS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND SUBSEQUENT REMOVAL OF THE LIGHTED 'X' RUNWAY CLOSURE SIGNALS FOR ALL RUNWAY CLOSURES. IN ADDITION AND FOR THE DURATION OF THE CONSTRUCTION WORK ONLY, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN WITHIN THE AOA A STANDBY LIGHTED 'X' RUNWAY CLOSURE SIGNAL PER THE DETAIL ON THIS SHEET AND THE SPECIFICATIONS. PLACEMENT AND REMOVAL OF THE LIGHTED 'X'S FOR THE RUNWAY CLOSURES SHALL BE COORDINATED WITH RHV AND SHALL BE IN ACCORDANCE WITH THE CONSTRUCTION PHASING PLAN.
- 21. AT THE PRE-CONSTRUCTION MEETING THE CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE FOR REVIEW AND APPROVAL BY THE ENGINEER. THE CONTRACTOR SHALL REVISE THIS SCHEDULE ON A WEEKLY BASIS DURING THE PROJECT IF THE ENGINEER DEEMS THE PROGRESS OF THE WORK NOT TO BE IN ACCORDANCE WITH THE APPROVED SCHEDULE.
- 22. CONSTRUCTION PERSONNEL, EQUIPMENT AND MATERIAL SHALL NOT PENETRATE THE RUNWAY OFZ AS DEFINED IN AC 150/5300-13A FOR AN OPERATIONAL RUNWAY. THE CONTRACTOR SHALL COMPLY WITH ANY HEIGHT LIMITS SHOWN ON THE PLANS OR IDENTIFIED IN FAA PART 77.
- 23. ALL CONTRACTOR VEHICLES, INCLUDING HAULING VEHICLES, CONSTRUCTION EQUIPMENT (ROLLERS, BACK HOES, SCRAPERS, ETC...) THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT WITHIN THE DESIGNATED LIMITS OF CONSTRUCTION OR HAUL ROUTES AS DEFINED HEREIN. SHALL DISPLAY A 3'X3' OR LARGER ORANGE AND WHITE CHECKERBOARD FLAG IN FULL VIEW ABOVE THE VEHICLE, EACH CHECKERBOARD COLOR SHALL BE ONE-FOOT (1') SQUARE. OPERATIONS DURING PERIODS OF DARKNESS OR LIMITED VISIBILITY SHALL REQUIRE THE CONTRACTORS VEHICLES TO BE EQUIPPED WITH ROTATING OR FLASHING AMBER LIGHTS. DURING SUCH PERIODS, HAULING VEHICLES NOT EQUIPPED WITH THE REQUIRED LIGHTS SHALL BE ESCORTED BY A VEHICLE SO EQUIPPED.
- 24. ANY WORK WITHIN THE RUNWAY SAFETY AREA (RSA) AND RUNWAY OBSTACLE FREE ZONE (ROFZ) REQUIRES A TEMPORARY CLOSURE OF THE RUNWAY. ANY WORK WITHIN THE TAXIWAY SAFETY AREA (TSA) REQUIRES A TEMPORARY CLOSURE OF THE TAXIWAY. THE CONTRACTOR SHALL SUBMIT WRITTEN NOTICE IN THE FORM OF A WEEKLY SCHEDULE OF CLOSURES, 72-HOURS PRIOR TO THE ANTICIPATED START OF WORK, WITHIN THE AOA.
- 25. PRIOR TO RE-OPENING A TEMPORARILY CLOSED RUNWAY, THE RUNWAY OFZ AND RSA MUST BE RETURNED TO THE FAA STANDARDS CRITERIA OUTLINED IN ADVISORY CIRCULAR 150/5370-2F. THIS CRITERIA PROHIBITS LEAVING ANY OPEN EXCAVATIONS, ANY PAVEMENT EDGE DROP OFF GREATER THAN THREE (3) INCHES, AND ANY GRADES STEEPER THAN FIVE (5) PERCENT WITHIN TWO-HUNDRED (200) FEET OF THE RUNWAY CENTERLINE. THE CONTRACTOR SHALL CARRY OUT AND SCHEDULE THE WORK ACCORDINGLY.
- 26. PRIOR TO RE-OPENING A TEMPORARY CLOSED RUNWAY OR TAXIWAY, THE PAVEMENT MUST BE THOROUGHLY CLEANED OF ALL FOD. THE CONTRACTOR SHALL ARRANGE TO HAVE THE ENGINEER AND RHV OPERATIONS INSPECT THE SITE TO CONFIRM THAT THE PAVEMENT IS BEING LEFT IN A SATISFACTORY AND CLEAN CONDITION. THE CONTRACTOR SHALL ALLOW SUFFICIENT TIME TO MAKE ANY CORRECTIONS TO PAVEMENT FOUND TO BE DEFICIENT BEFORE OPENING THE PAVEMENT TO AIRCRAFT MOVEMENT. ANY PAVEMENT THAT DOES NOT PASS THE ENGINEER'S AND SNS OPERATIONS' INSPECTION SHALL REMAIN CLOSED UNTIL CORRECTIVE MEASURES ARE COMPLETED BY THE CONTRACTOR AND APPROVED BY RHV OPERATIONS. THE CONTRACTOR SHALL BE SUBJECT TO DAMAGES PER THE SPECIFICATIONS FOR LATE RE-OPENING OF PAVEMENTS TO AIR TRAFFIC.
- 27. EXISTING PAVEMENT MARKINGS THAT HAVE BEEN OBLITERATED, DISTURBED, OR REMOVED DURING THE CONSTRUCTION PROCESS, MUST BE REINSTATED TO PREVIOUSLY EXISTING CONDITION PRIOR TO OPENING THE PAVEMENT TO AIRCRAFT TRAFFIC. SUFFICIENT TIME BETWEEN PAVEMENT MARKING APPLICATION AND THE OPENING TO AIRCRAFT TRAFFIC SHALL BE PROVIDED FOR THE MARKINGS TO THOROUGHLY DRY.
- 28. CONSTRUCTION MATERIAL STOCKPILING SHALL NOT BE ALLOWED WITHIN THE ROFZ, RSA, OR TSA. WHEN SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER THE PLACEMENT, SPREADING, AND COMPACTION OF MILLINGS AND/OR UNCLASSIFIED EXCAVATION MATERIAL WITHIN THE DESIGNATED AREAS CAN PROCEED DURING THE DAY ONLY IN THE AREAS OUTSIDE THE AIRPORT APPROACH SURFACES, THE ROFZ, RSA, AND TSA.
- 29. PRIOR TO SUBSTANTIAL COMPLETION THE CONTRACTOR SHALL RESTORE THE STAGING AREAS TO THE CONDITION PRIOR TO MOBILIZATION AND TO THE SATISFACTION OF THE ENGINEER.
- 30. THE CONTRACTOR IS RESPONSIBLE FOR SETTING NEWLY INSTALLED NAVIGATIONAL AIDS TO THE CORRECT ANGLES FOR THE FAA FLIGHT CHECK. IF THE FLIGHT CHECK DOES NOT PASS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE NAVAIDS TO THE CORRECTED POSITION UNTIL THE FLIGHT CHECK PASSES. ANY ADJUSTMENTS REQUIRED SHALL BE AT THE SOLE EXPENSE OF THE CONTRACTOR.





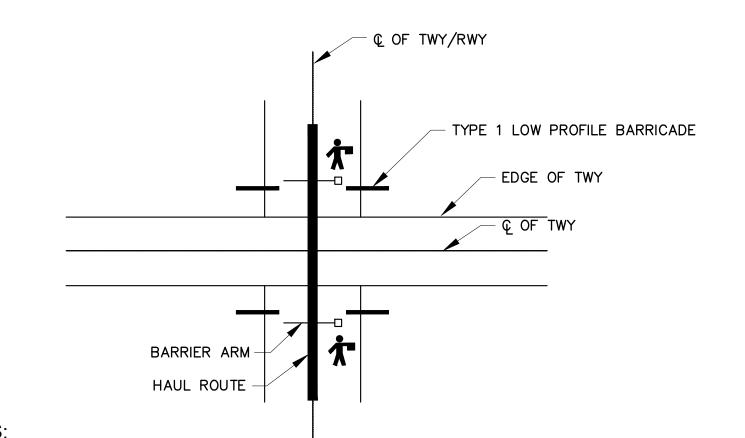


1. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING PROPER POSITIONING AND OPERATION OF ALL BARRICADES.

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- 2. ADDITIONAL SANDBAGS AND/OR ANCHORS MAY BE REQUIRED TO HOLD THE BARRICADES IN PLACE WHERE EXPOSED TO PROP WASH OR JET BLAST. SANDBAGS AND ANCHORS ARE INCIDENTAL TO THE
- LIGHTS TO BE 3" RED LEXAN HIGH—INTENSITY FLASHING OMNI—DIRECTIONAL LIGHTS OPERATIONAL FROM DUSK TO DAWN (BATTERY TO BE REPLACED EVERY 7 DAYS).
- 4. FACING OF BARRICADE TO BE COVERED WITH PRE-ATTACHED ORANGE AND WHITE HIGH-INTENSITY REFLECTIVE STRIPING (AIRCRAFT MOVEMENT SIDE ONLY).
- 5. BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS INDICATED ON THE CONSTRUCTION PHASING PLAN OR AS DIRECTED BY THE AIRPORT.
- 6. THE CONTRACTOR SHALL PROVIDE A SUBMITTAL TO THE ENGINEER FOR REVIEW OF THE PROPOSED LOW PROFILE BARRICADE TYPE AND MANUFACTURE AND PRIOR TO ORDERING OR ACQUIRING ANY BARRICADES. THE SUBMITTAL SHALL CONTAIN SUFFICIENT INFORMATION INCLUDING AS A MINIMUM BARRICADE TYPE, SIZE, MANUFACTURER, AND DATE OF MANUFACTURE, TO PROVIDE THE ENGINEER WITH SUFFICIENT INFORMATION TO PERFORM A THOROUGH AND COMPLETE REVIEW.

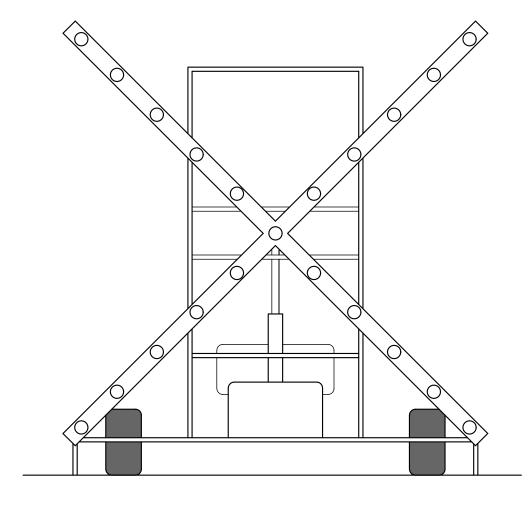




#### NOTES:

- 1. TWO (2) FLAGGERS SHALL BE REQUIRED AT EACH CONTROLLED TAXIWAY CROSSING
- FLAGGERS SHALL MONITOR AIR TRAFFIC CONTROL TOWER (ATCT) GROUND FREQUENCY AT ALL TIMES AND SHALL COMMUNICATE WITH THE ATCT FOR PERMISSION TO ALLOW CONSTRUCTION TRAFFIC TO CROSS ACTIVE TAXIWAY.
- 3. FLAGGERS SHALL AT ALL TIMES OBEY THE INSTRUCTIONS FROM AIR TRAFFIC CONTROLLER.
- 4. FLAGGERS SHALL NOT PERMIT CONSTRUCTION TRAFFIC TO ENTER TAXIWAY UNLESS EXIT IS ASSURED.
- 5. CONSTRUCTION TRAFFIC SHALL NOT BE ALLOWED TO DISRUPT OTHER AIRPORT TRAFFIC.



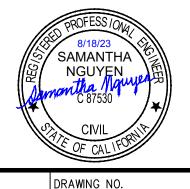


#### NOTES: THE LIGHTED "X" RUNWAY CLOSURE SIGNAL

- BE A PORTABLE, TOWABLE UNIT THAT CAN BE QUICKLY REMOVED FROM THE RUNWAY.
- 2. CONSIST OF CLEAR INCANDESCENT LAMPS OR TRANSMIT A WHITE COLOR, ARRANGED IN THE SHAPE OF A LETTER "X" WITH ARMS CROSSED AT AN APPROPRIATE ANGLE TO MAKE THE "X" DISCERNIBLE. THE ARMS SHALL BE PAINTED YELLOW ON ALL SIDES SO THAT THE UNIT WILL BE CLEARLY VISIBLE WHEN IT IS IN POSITION.
- BE ENERGIZED BY A PORTABLE POWER SUPPLY AND HAVE A HYDRAULIC LIFTING MECHANISM TO RAISE AND LOWER THE X PANEL.
- 4. BE CONTROLLED SO THAT THE LIGHTED SIGNAL WILL FLASH AT AN APPROXIMATE RATE OF 2.5-3 SECONDS "ON" AND 1-2.5 SECONDS "OFF".
- 5. PROVIDE THE FOLLOWING DAYTIME AND NIGHTTIME VISUAL REFERENCE DURING VISUAL FLIGHT RULE (VFR) CONDITIONS WHEN PLACED ON CENTERLINE AND WITHIN 250 FEET OF THE RUNWAY END:
  - (1) VISIBLE TO THE PILOT AT A RANGE OF AT LEAST 5 NAUTICAL MILES. (2) RECOGNIZABLE AS A LETTER "X" FROM A RANGE OF AT LEAST 1 NAUTICAL MILE.
- PROVIDE LAMP DIMMING CAPABILITY FOR NIGHTTIME OPERATIONS.
- PRODUCE A SIGNAL THAT PROVIDES A HORIZONTAL COVERAGE TO AT LEAST 15 DEGREES ON EACH SIDE OF THE RUNWAY CENTERLINE, AND A VERTICAL COVERAGE FROM 0 DEGREES TO 10 DEGREES ABOVE HORIZONTAL, BOTH DAY AND NIGHT, AT A RANGE OF 1 NAUTICAL MILES.
- 8. ADJUSTABLE AIMING AND LEVELING TO ALLOW TILTING TO AN OPTIMUM ANGLE OF THREE DEGREES FROM VERTICAL.
- WITHSTAND A MINIMUM WIND SPEED OF AT LEAST 40 MPH WITHOUT AFFECTING AIMING OR
- 10. INCLUDE AN ILLUMINATED FAILURE INDICATOR THAT IS VISIBLE FROM THE BACK (RUNWAY SIDE) OF THE UNIT.
- 11. INCLUDE AN OPERATIONS PLACARD IN A CONSPICUOUS LOCATION THAT INSTRUCTS OPERATORS TO VISUALLY CHECK THE OPERATION OF THE DEVICE EVERY TWO HOURS.
- 12. BE CAPABLE OF ONE PERSON SET UP IN LESS THAN 5 MINUTES USING HYDRAULIC RAISING/LOWERING MECHANISM.
- 13. HAVE DIESEL PORTABLE POWER WITH ADAPTER TO RUN DIRECTLY FROM ELECTRICAL OUTLETS.
- 14. HAVE TRAILER HITCH OPTIONS INCLUDING TANDEM TOWING FOR ON—AIRPORT OPERATIONS.
- 15. HAVE THE ABILITY TO PROVIDE UP TO 120 HOURS OF CONTINUOUS OPERATION.
- 16. HAVE FAIL SAFE PROTECTION TO ENSURE THAT THE UNIT STAYS ON AS CONTINUOUS LIT IF THE FLASHER UNIT SHOULD FAIL.
- 17. HAVE DIMENSIONING AND LIGHTING ARRANGEMENT THAT FOLLOWS THE DESIGN RECOMMENDATION OF THE FAA/AC 150/5345-55.

THE CONTRACTOR SHALL PROVIDE A SUBMITTAL TO THE ENGINEER FOR REVIEW OF THE PROPOSED LIGHTED "X" RUNWAY CLOSURE SIGNAL THAT THE CONTRACTOR INTENDS TO USE ON THE PROJECT AND PRIOR TO ORDERING OR ACQUIRING ANY UNITS. THE SUBMITTAL SHALL CONTAIN SUFFICIENT INFORMATION ON THE PROPOSED LIGHTED "X" RUNWAY CLOSURE SIGNAL TYPE, MANUFACTURER, AND DATE OF MANUFACTURE, TO PROVIDE THE ENGINEER WITH SUFFICIENT INFORMATION/DATA TO PERFORM A THOROUGH AND COMPLETE REVIEW. THE CONTRACTOR SHALL ACQUIRE THE UNITS ONLY AFTER THE ENGINEER HAS DEEMED THE SUBMITTAL ACCEPTABLE.

LIGHTED 'X' RUNWAY CLOSURE SIGNAL



DATE **REVISIONS** 



2023 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250

DESIGNED SAN JOSE, CALIFORNIA 95113 DRAWN (669) 800-4130 RJH/SN CHECKED

SUBMITTED: 8/18/23 DATE 8/18/23 DATE 8/18/23 DATE SIGNATURE

JAMIL S. SALAS SENIOR CIVIL ENGINEER No C74528 ∧Exp. 12-31-23/~

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 \Exp. 09-30-23

### REID-HILLVIEW AIRPORT

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

**CONSTRUCTION PHASING PLAN DETAILS** 

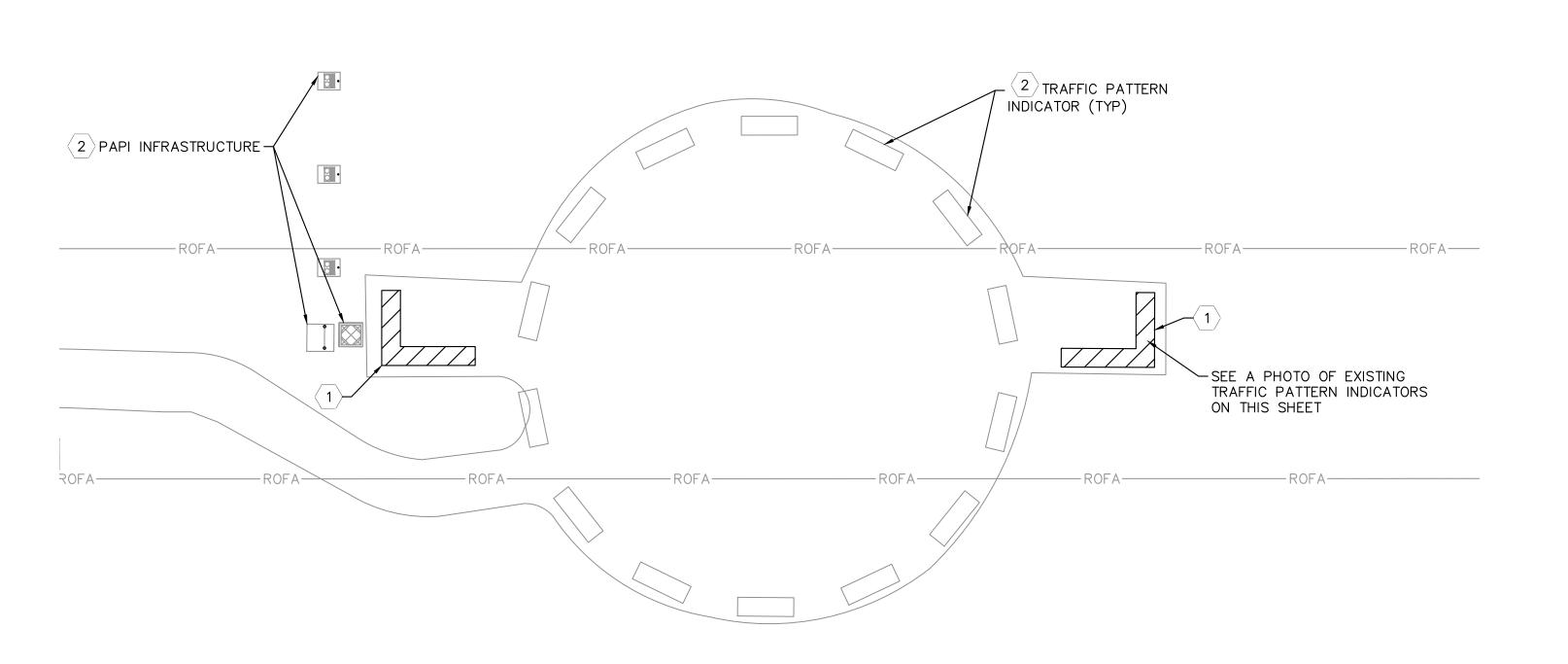
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11 OF 68

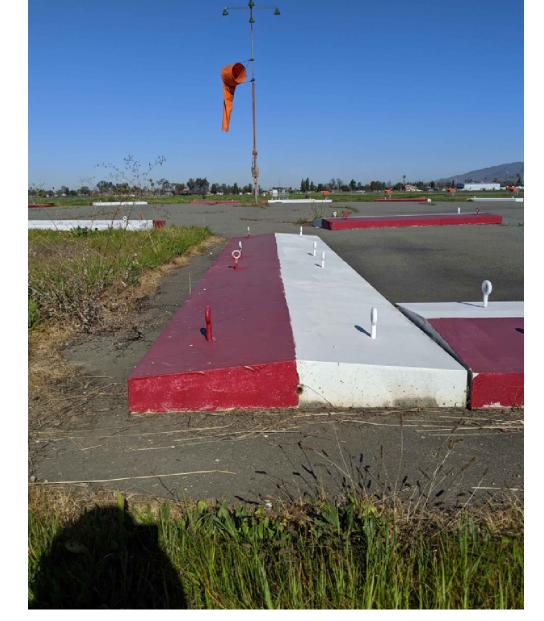
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FILE NO.

PROJECT NO. 192232000



DEMO OF EXISTING SEGMENTED CIRCLE



EXISTING TRAFFIC PATTERN INDICATORS (TO BE REMOVED) NOT TO SCALE

#### **GENERAL NOTES**

- 1. ALL STATIONING AND OFFSETS ARE BASED ON THE 'RUNWAY 13L-31R' ALIGNMENT UNLESS OTHERWISE NOTED.
- 2. LOCATION OF EXISTING UTILITIES TO BE DETERMINED UPON FURTHER FIELD INVESTIGATION AND UTILITY LOCATOR SERVICES.

#### LEGEND

TRAFFIC PATTERN INDICATOR REMOVAL



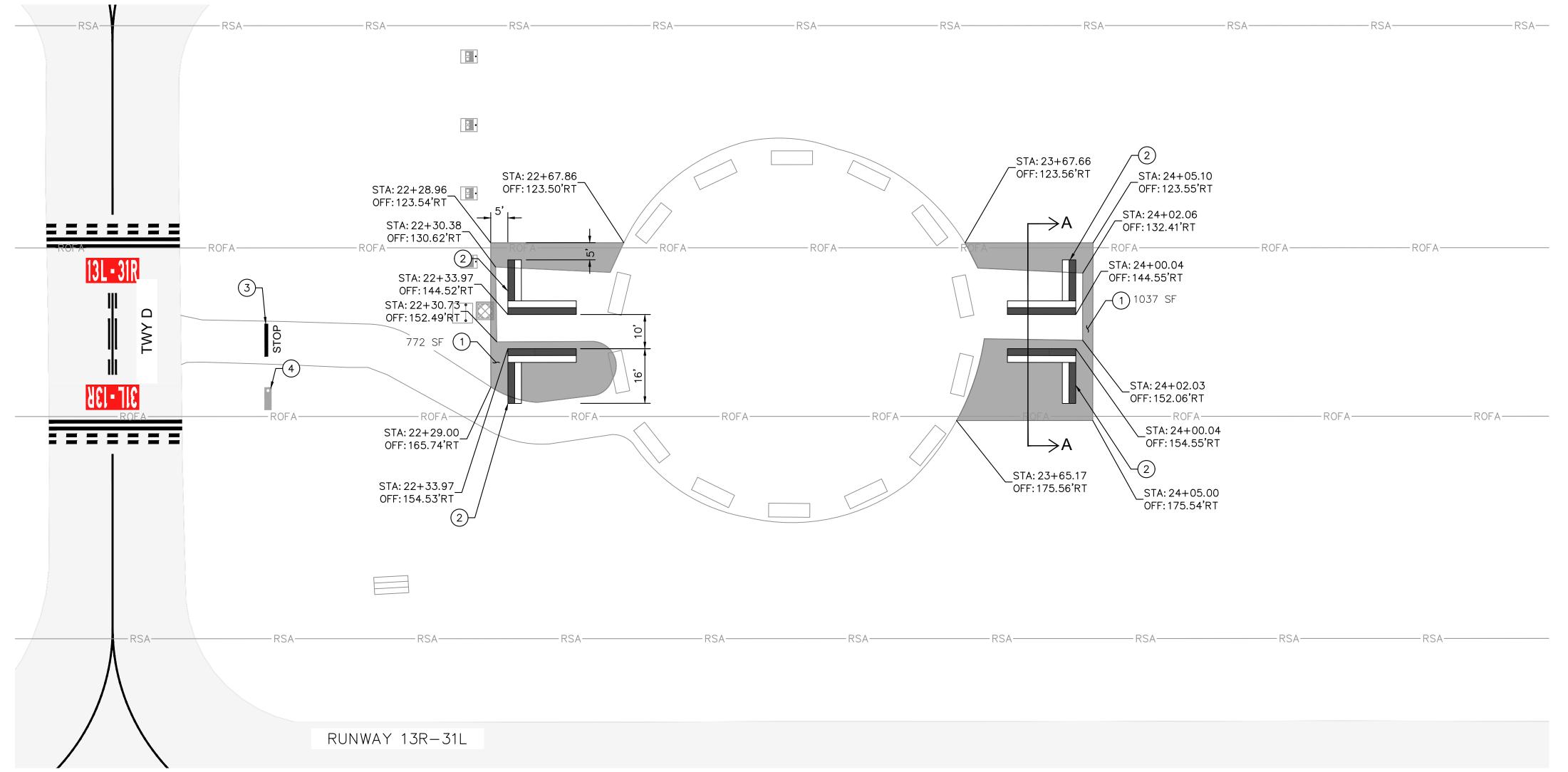
PROPOSED ASPHALT PAVEMENT

#### DEMOLITION NOTES

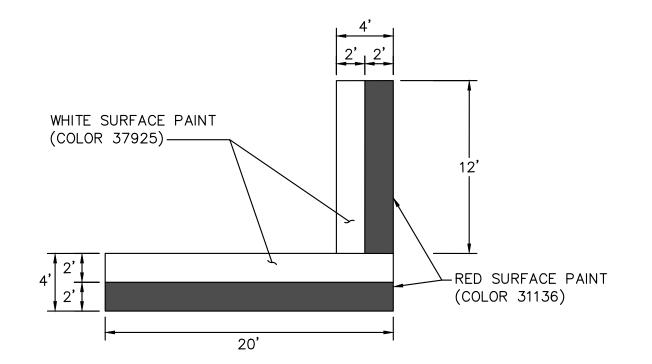
- 1 REMOVE EXISTING TRAFFIC PATTERN INDICATORS
- $\langle 2 \rangle$  protect in place

#### CONSTRUCTION NOTES

- (1) INSTALL NEW ASPHALT PAVEMENT.
- 2 INSTALL NEW SURFACE PAINTED TRAFFIC PATTERN INDICATORS PER AC 150/5340-5D. SEE DETAIL ON THIS SHEET FOR SURFACE PAINT
- (3) INSTALL NEW VSR STOP BAR MARKING PER DETAIL 7, SHEET 44.
- (4) INSTALL NEW SIGNAGE PER SHEET 53.

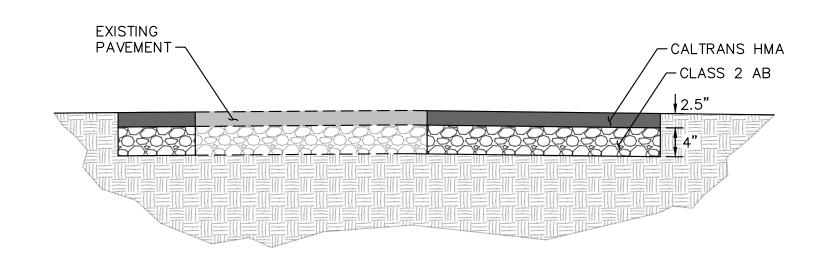






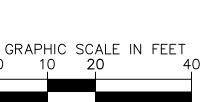
#### PROPOSED SURFACE PAINTED TRAFFIC PATTERN INDICATORS SURFACE PAINT DETAIL

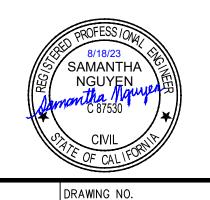
NOT TO SCALE











COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT **REID-HILLVIEW AIRPORT** AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID SUBMITTED:

PROJECT NO.

C1.0

SEGMENTED CIRCLE LAYOUT

SHT NO. 12 OF 68

BY DATE REVISIONS

Kimley»Horn 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

8/18/23 DESIGNED DATE KGH DRAWN 8/18/23 DATE RJH/SN 8/18/23 CHECKED

JAMIL S. SALAS SENIOR CIVIL ENGINEER

DATE

SIGNATURE

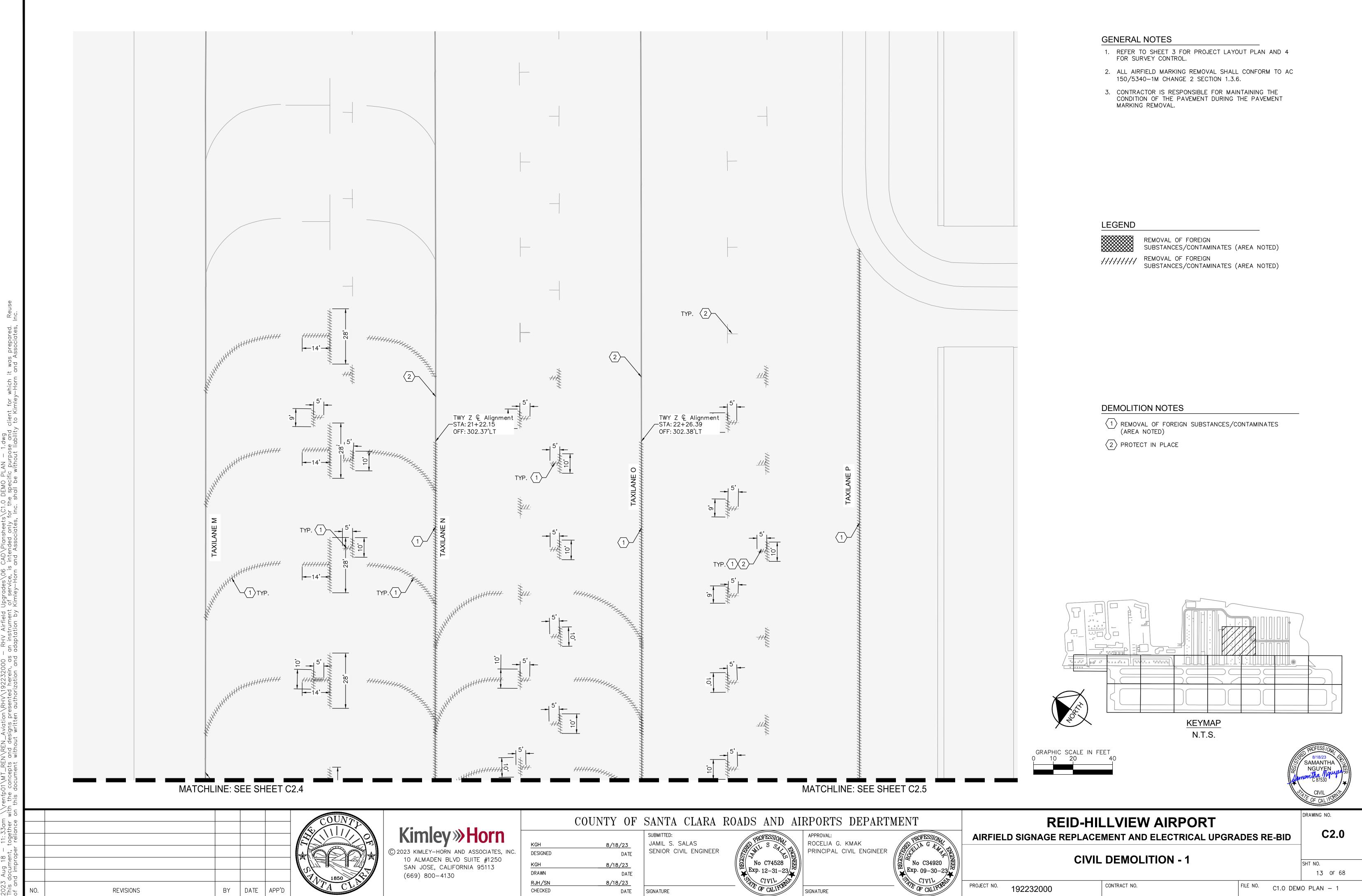
ROCELIA G. KMAK No C74528

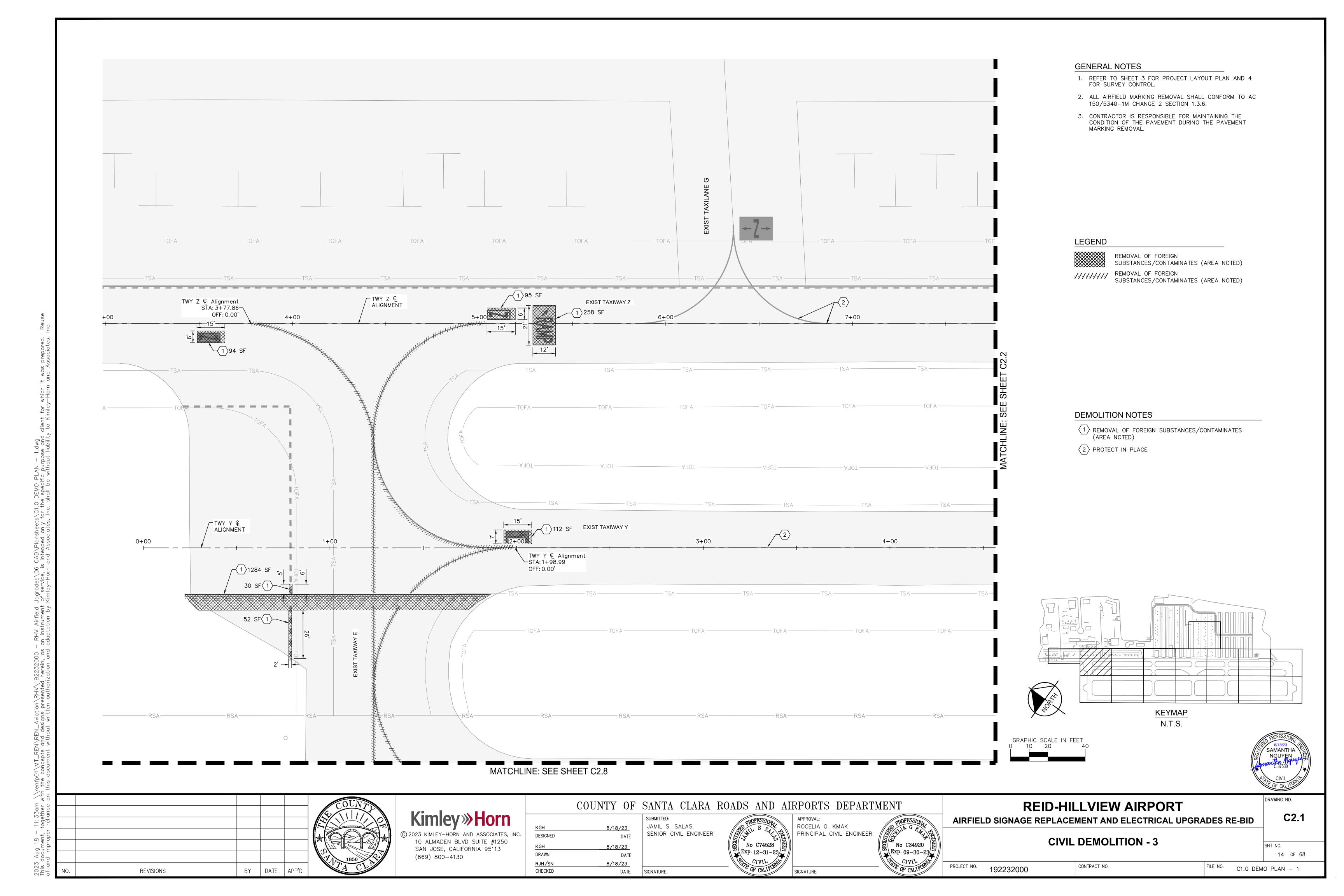
PRINCIPAL CIVIL ENGINEER No C34920 Exp. 09-30-23

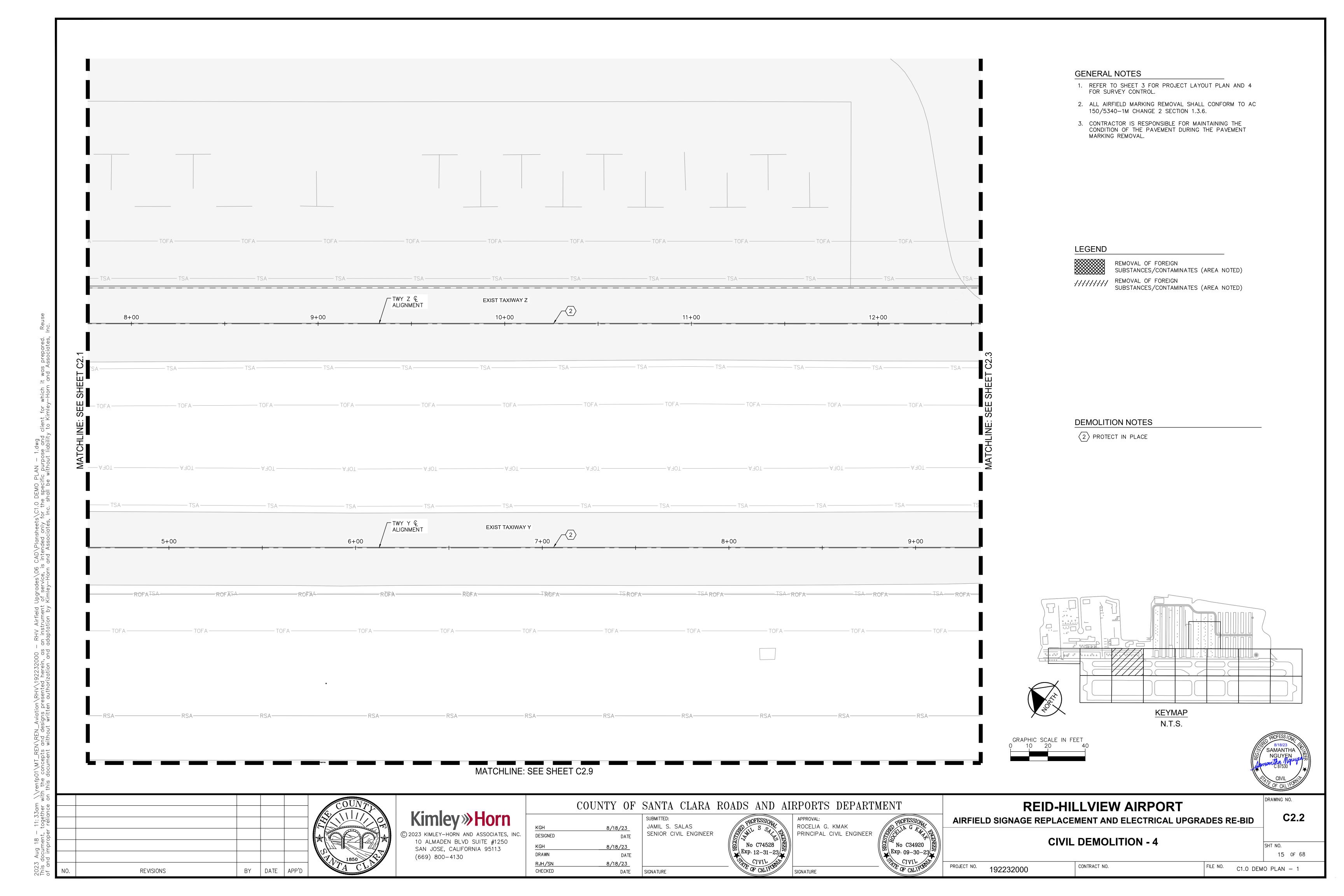
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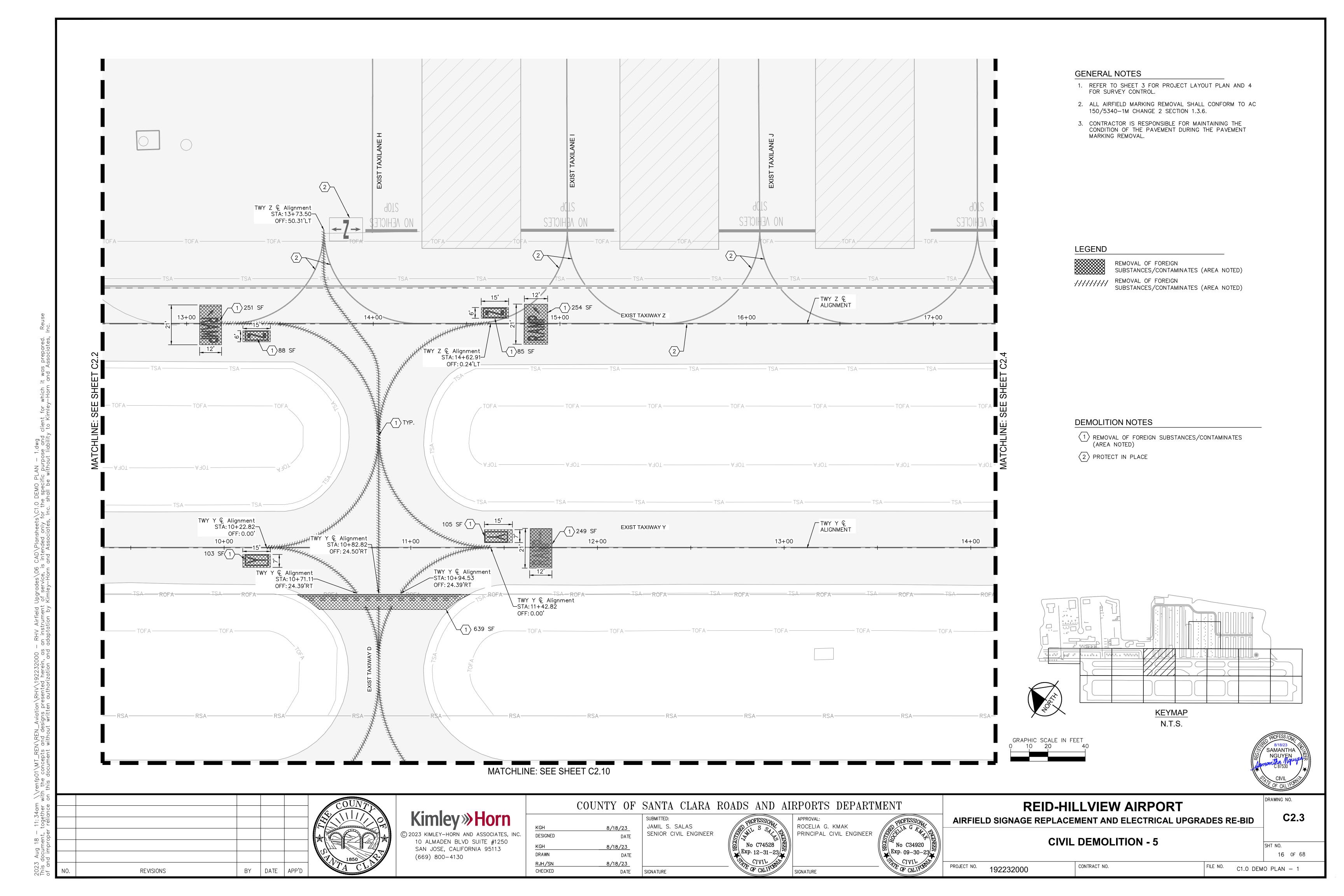
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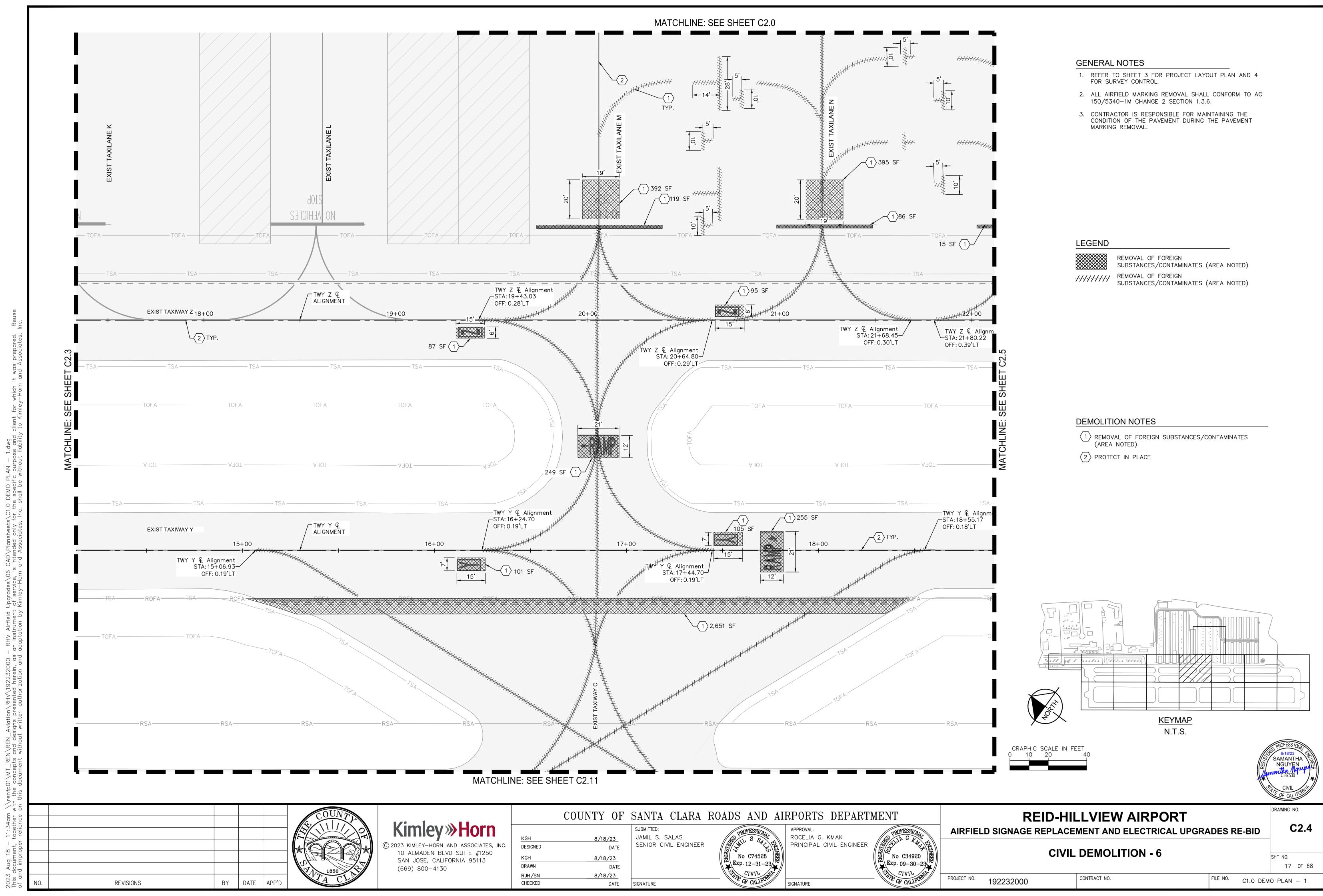
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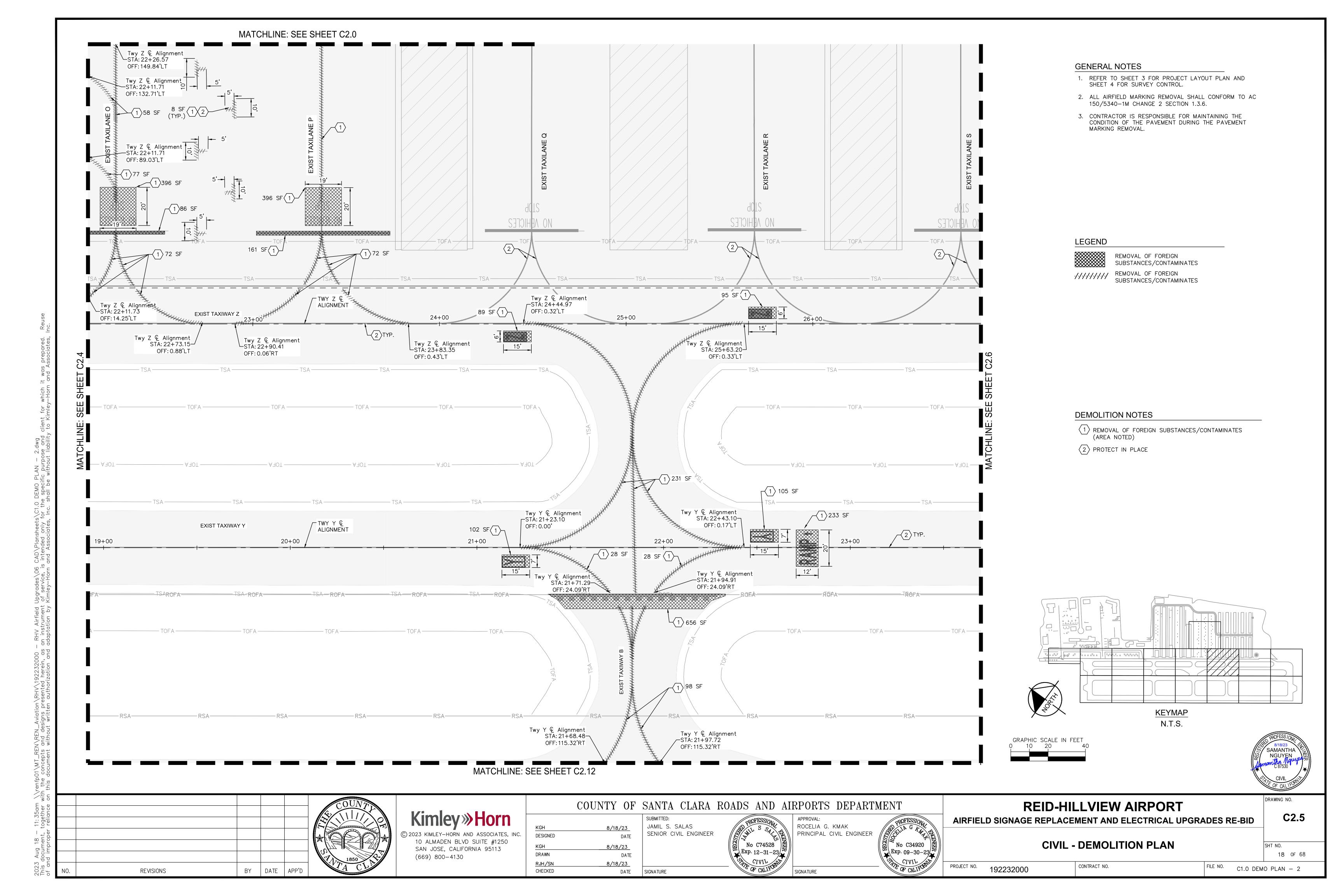


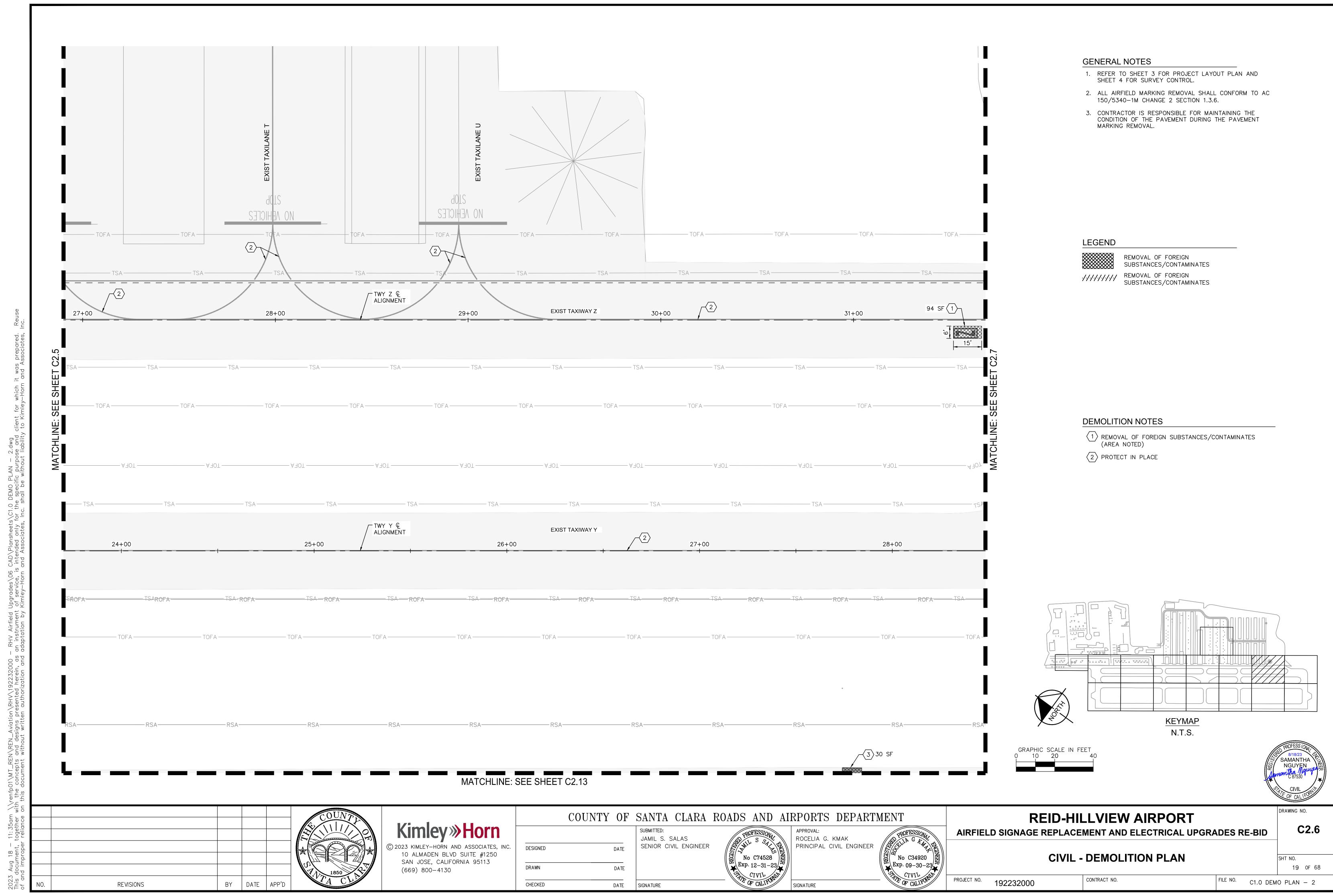


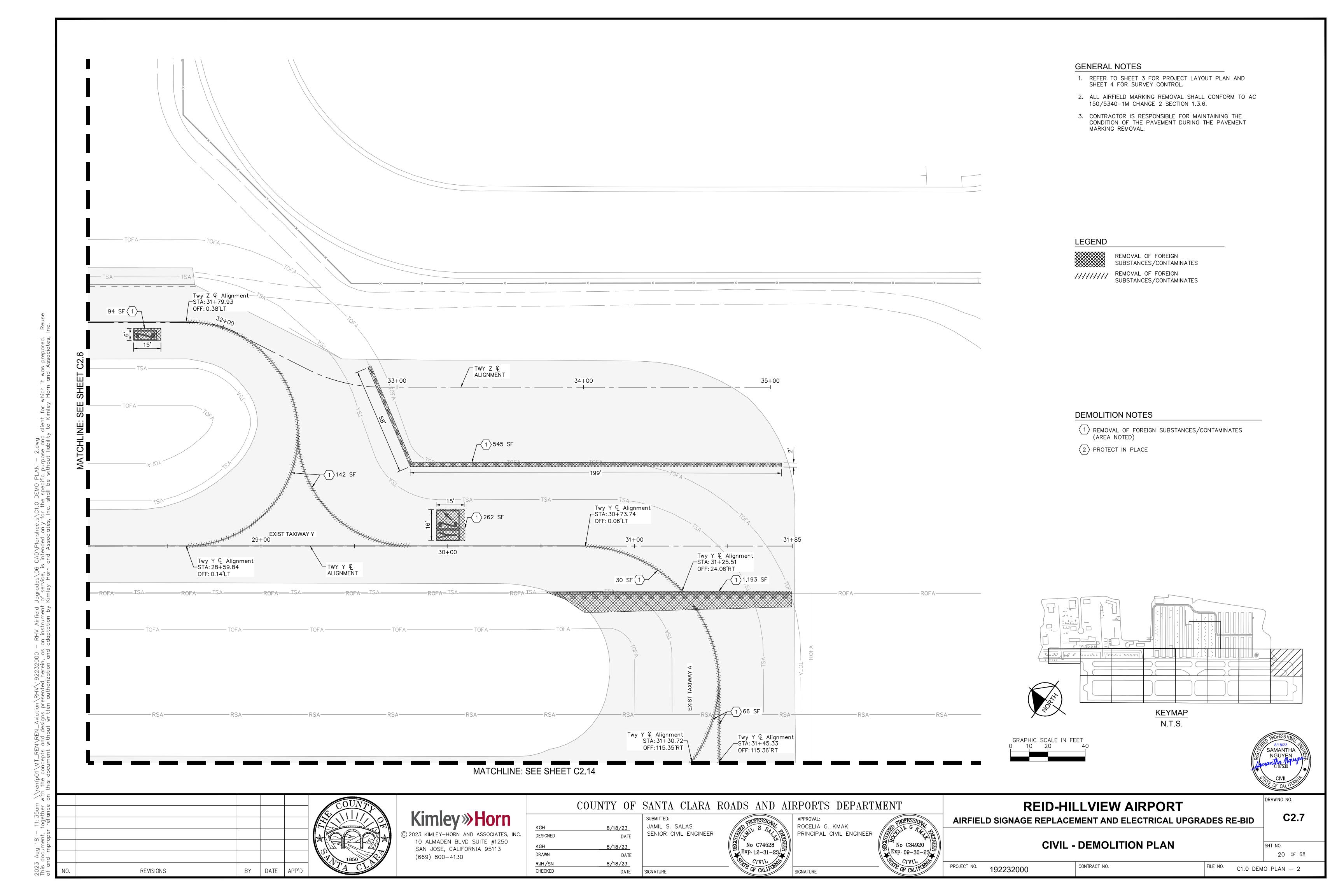


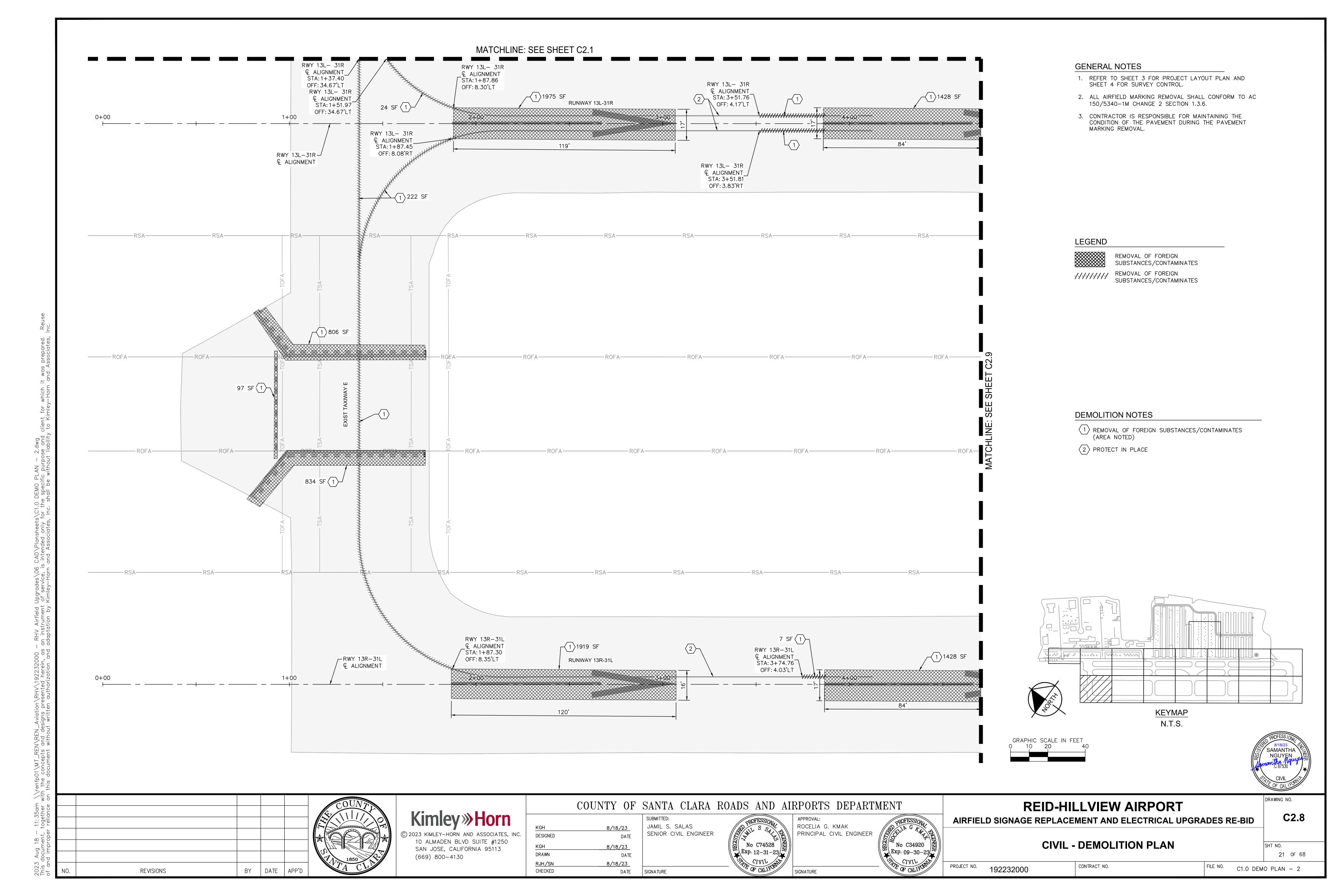


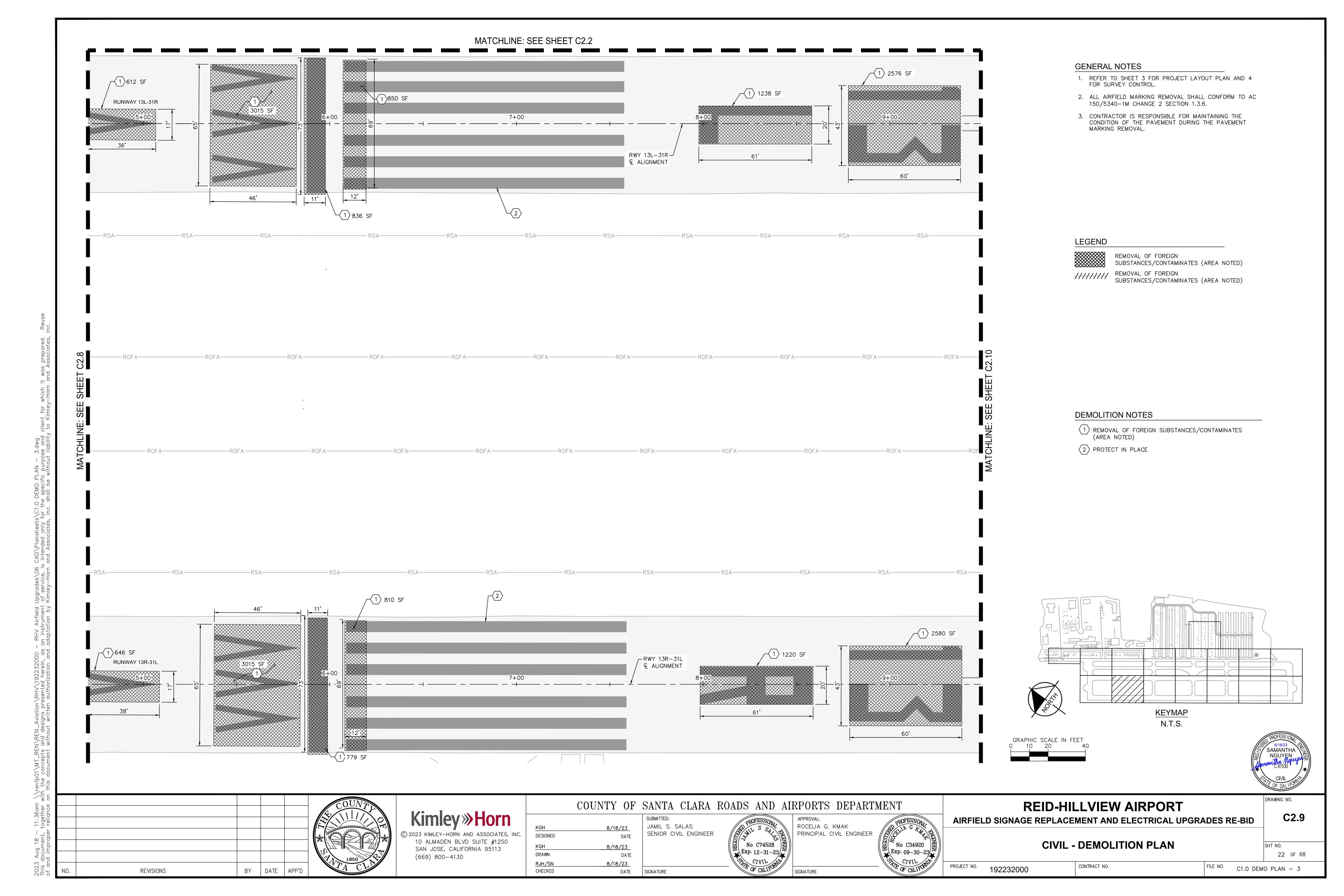


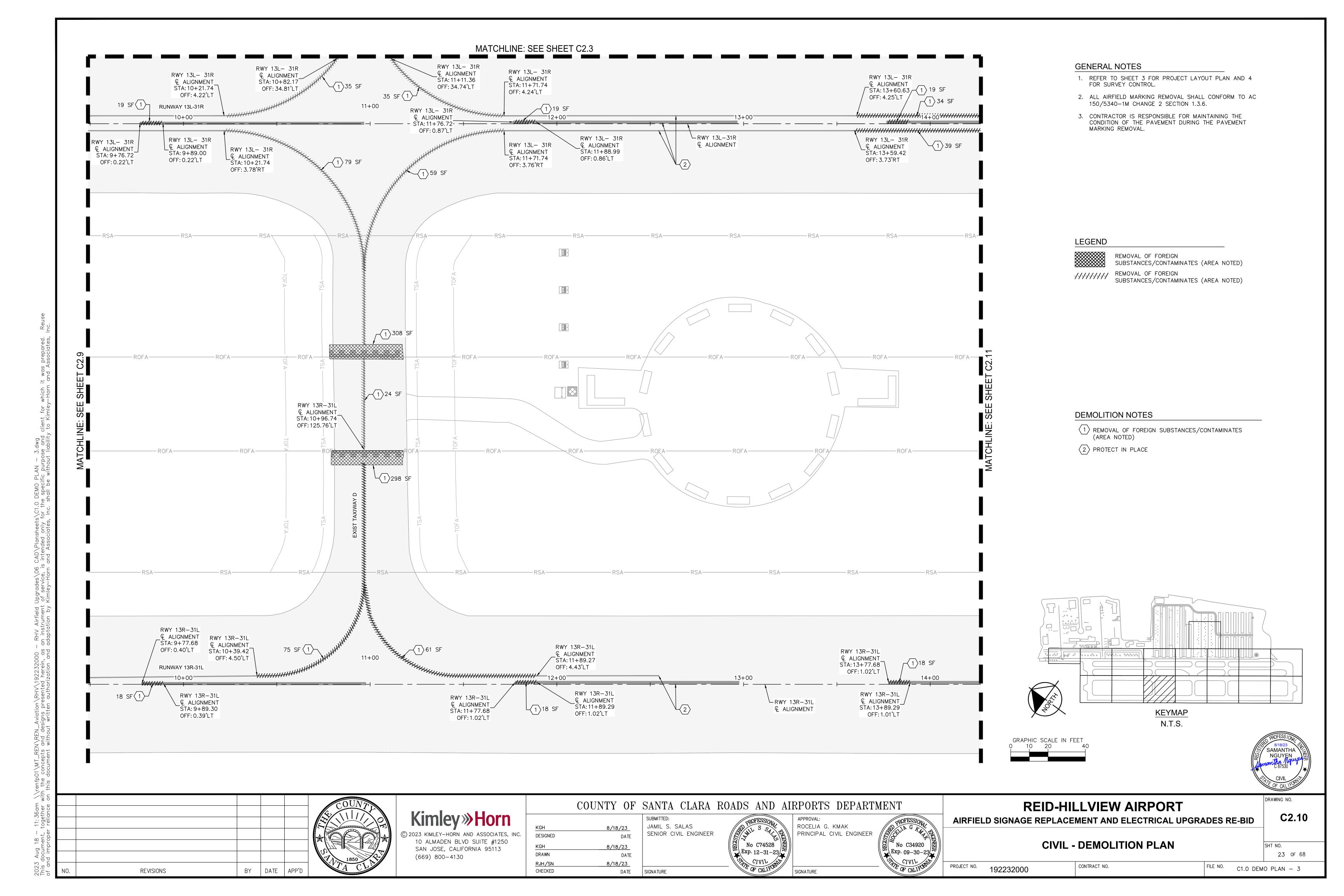


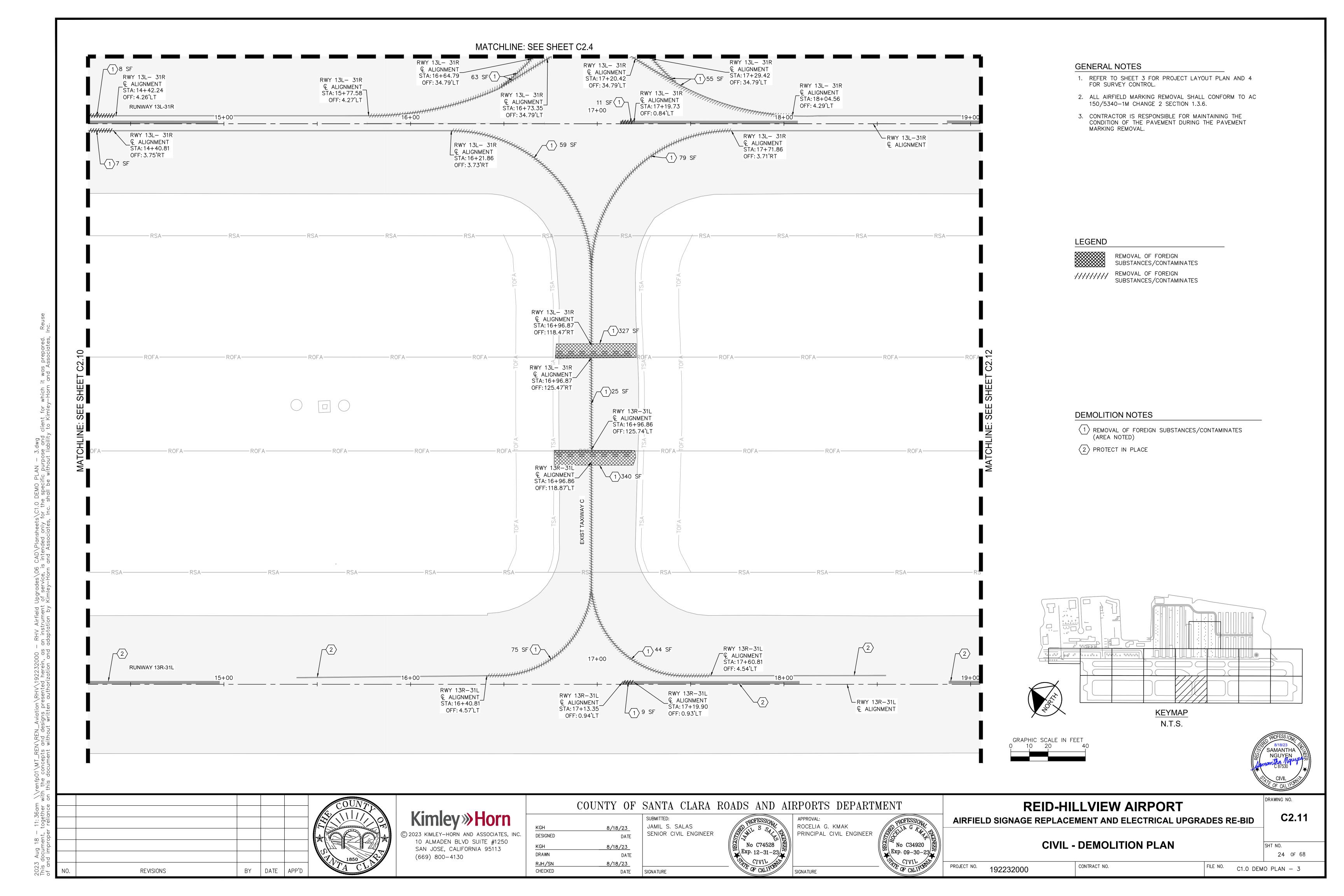


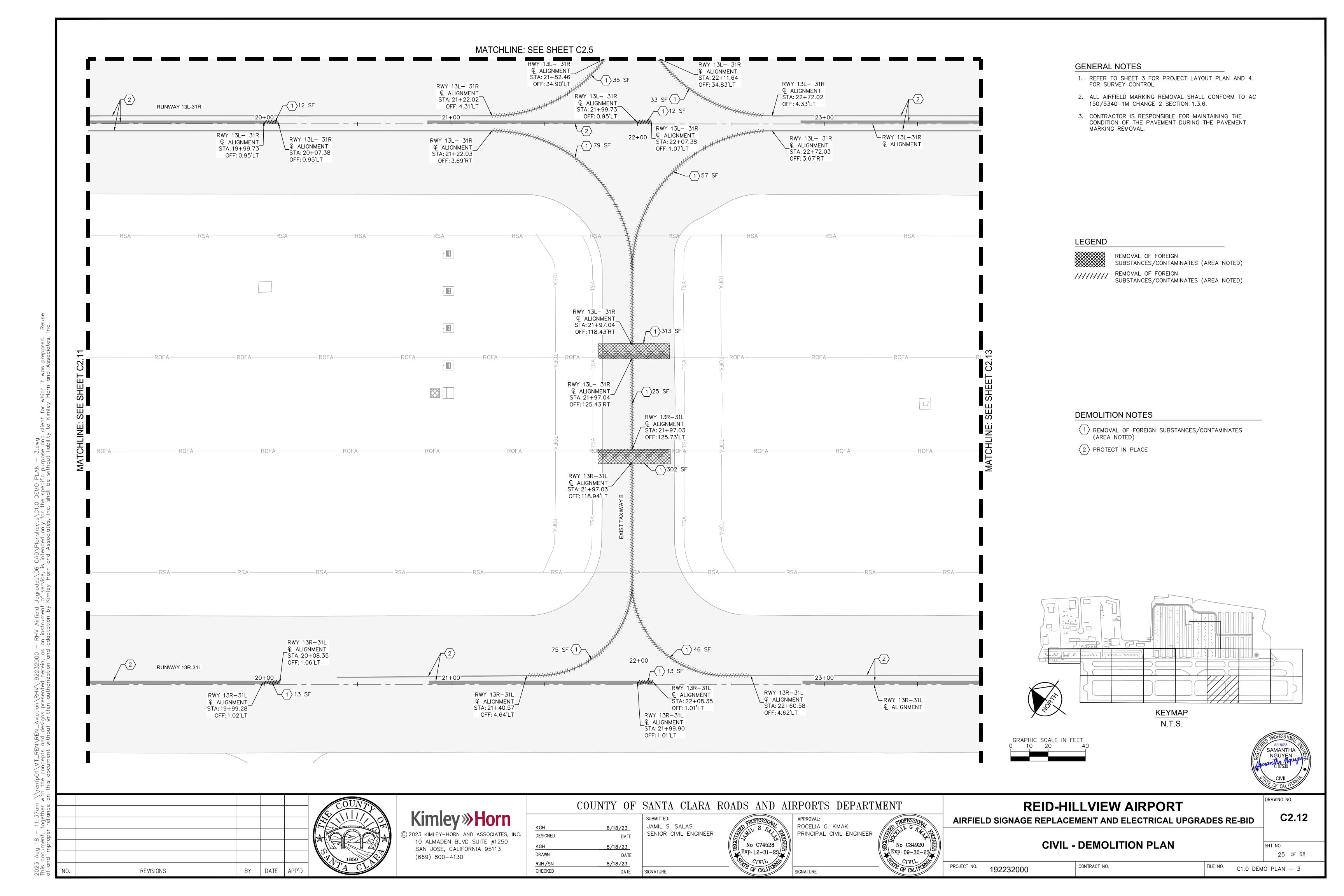


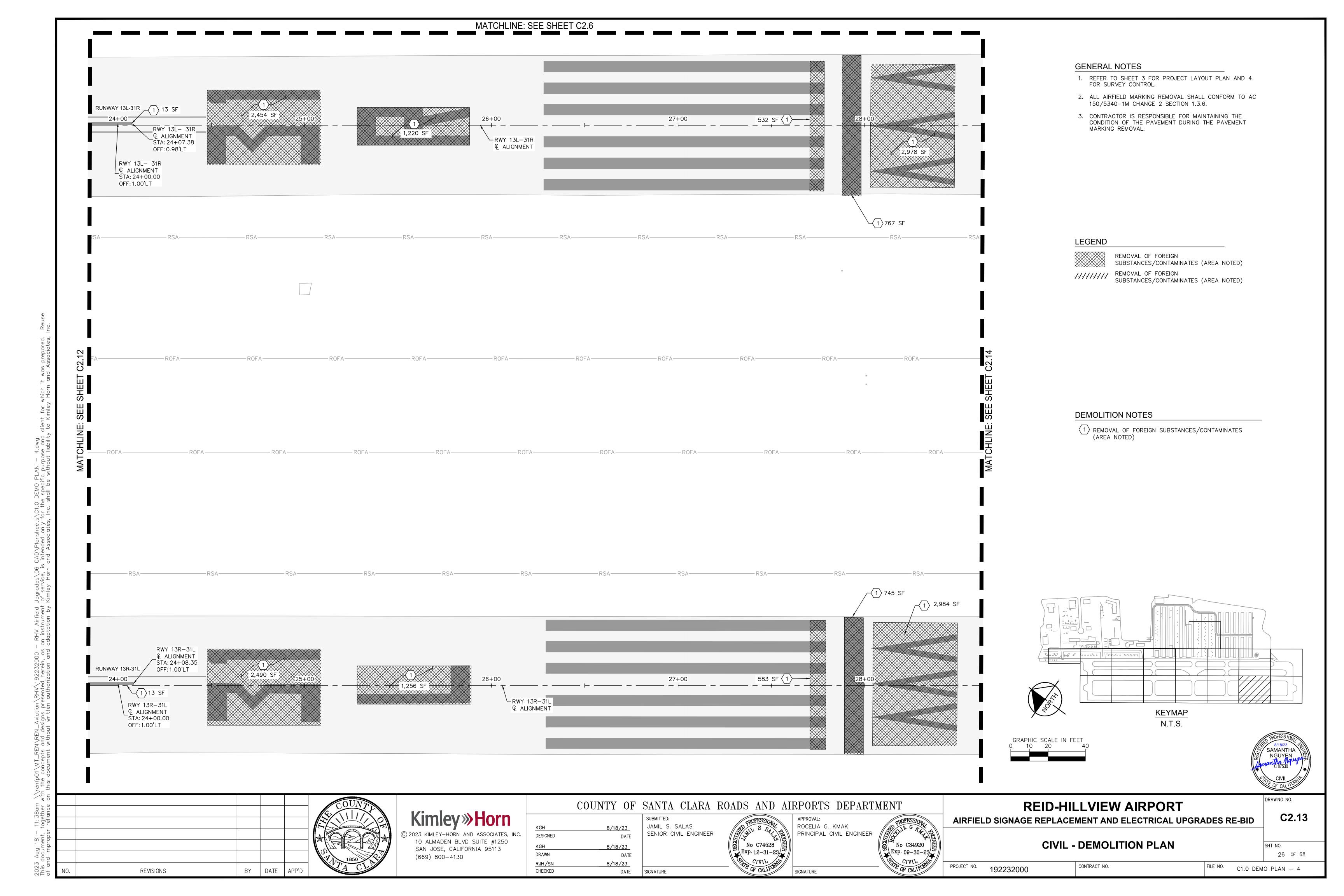


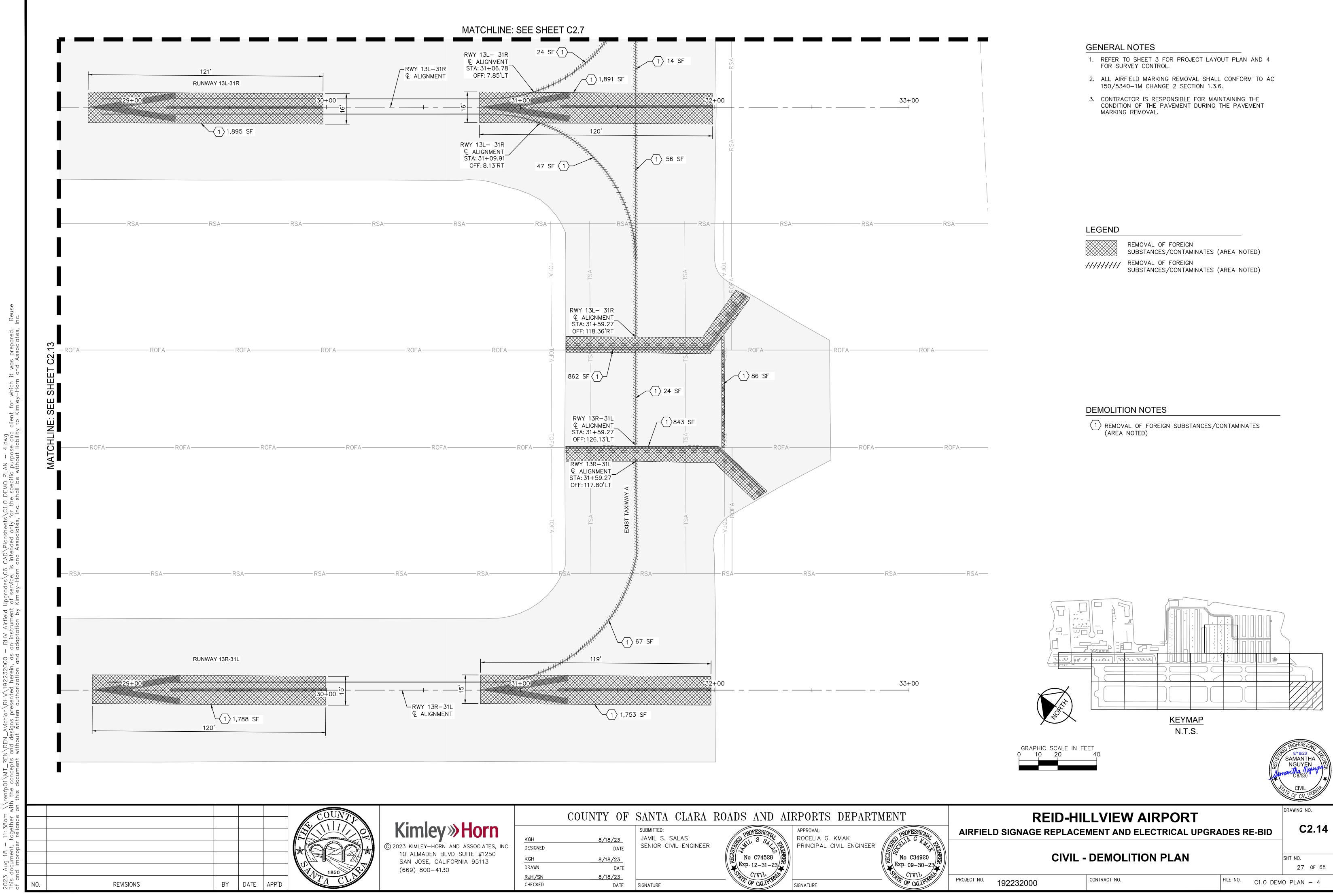




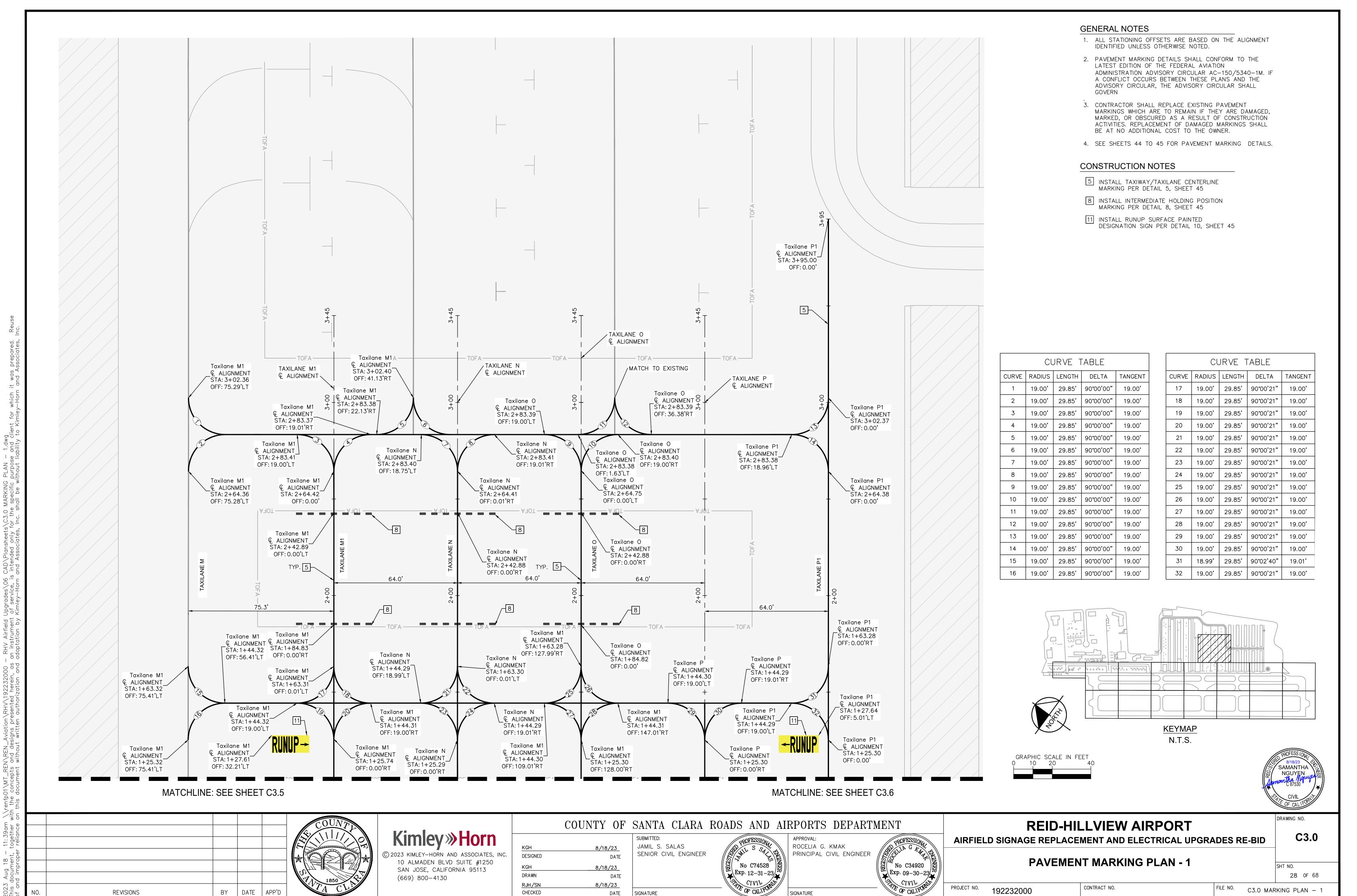




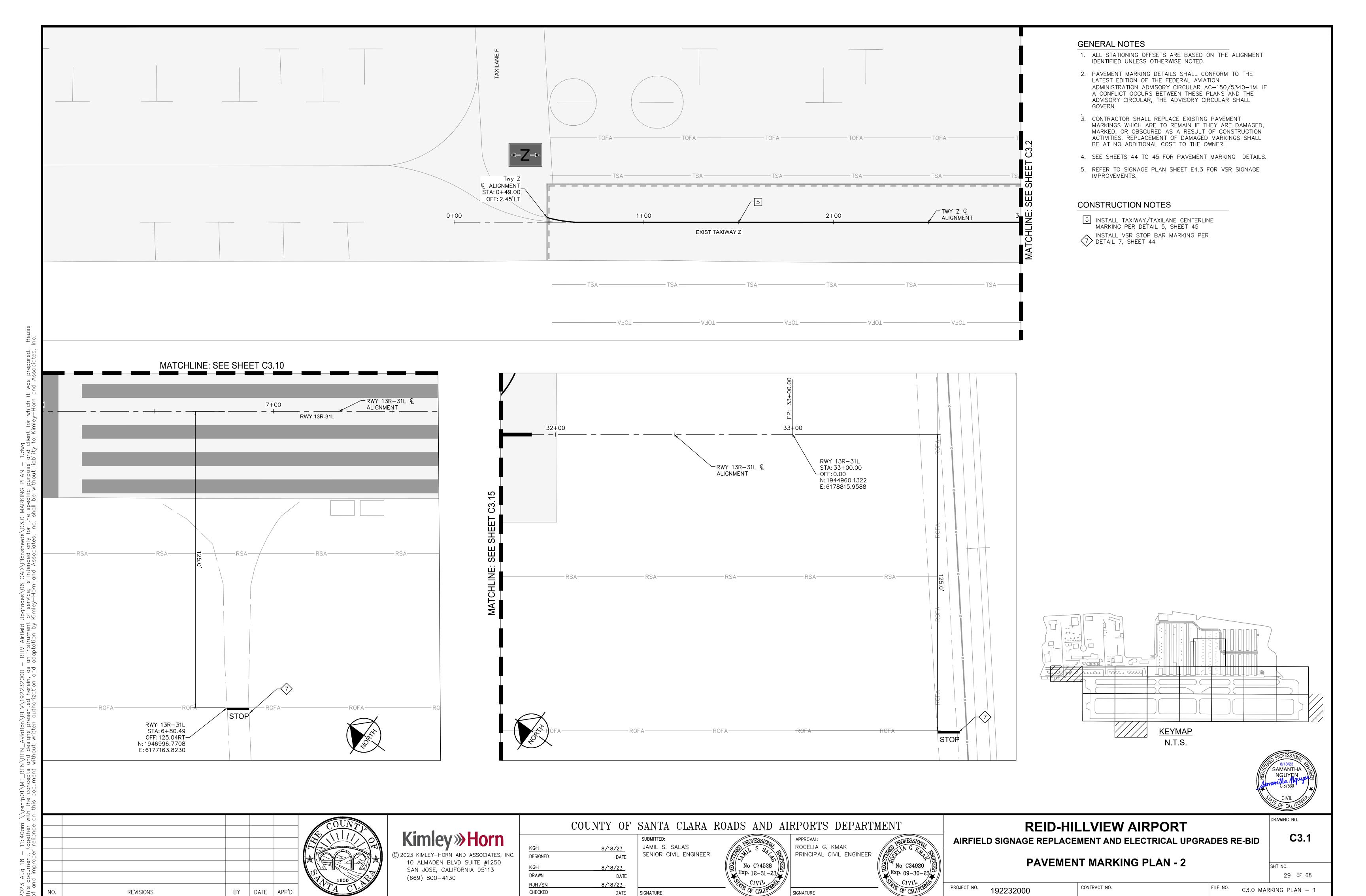


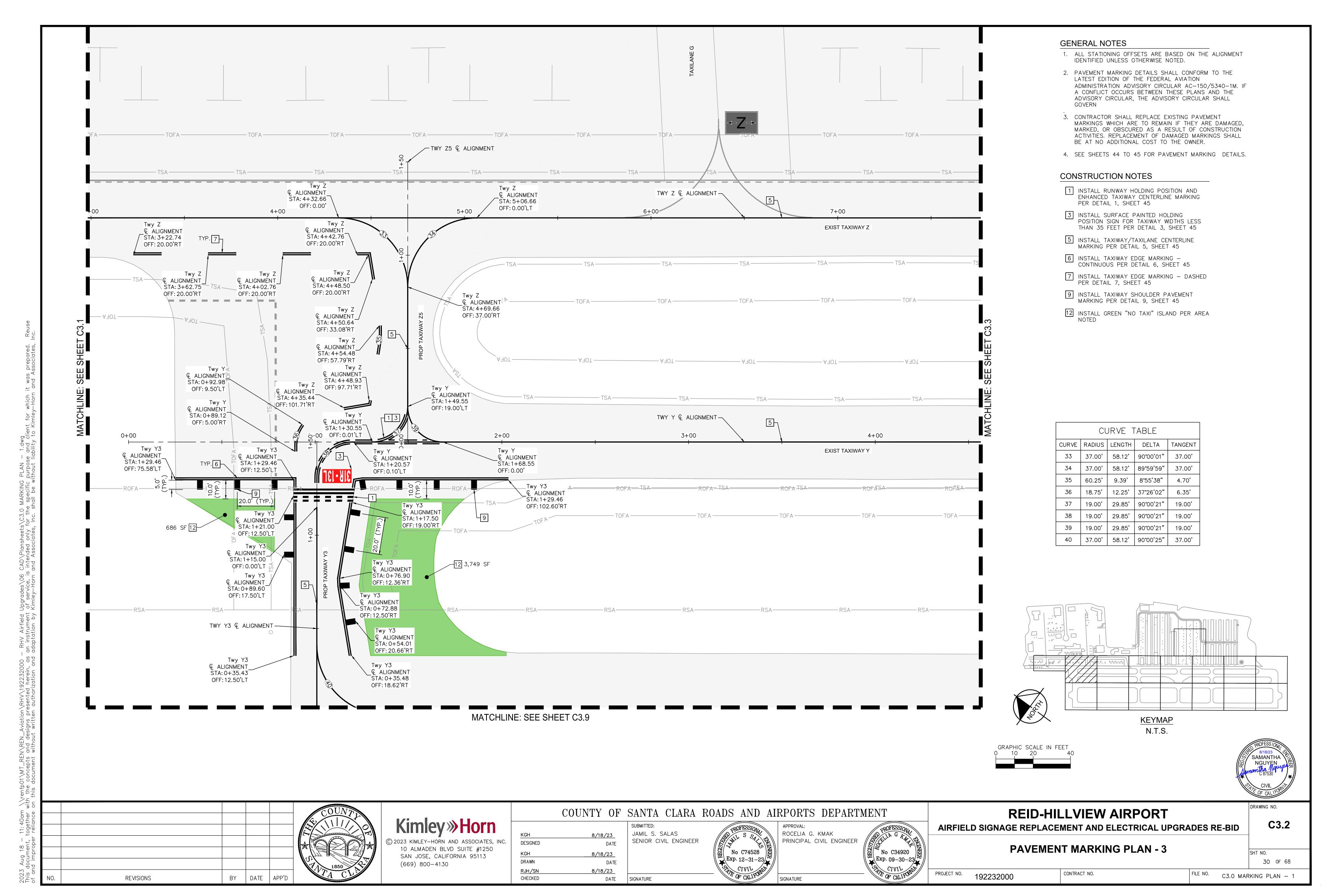


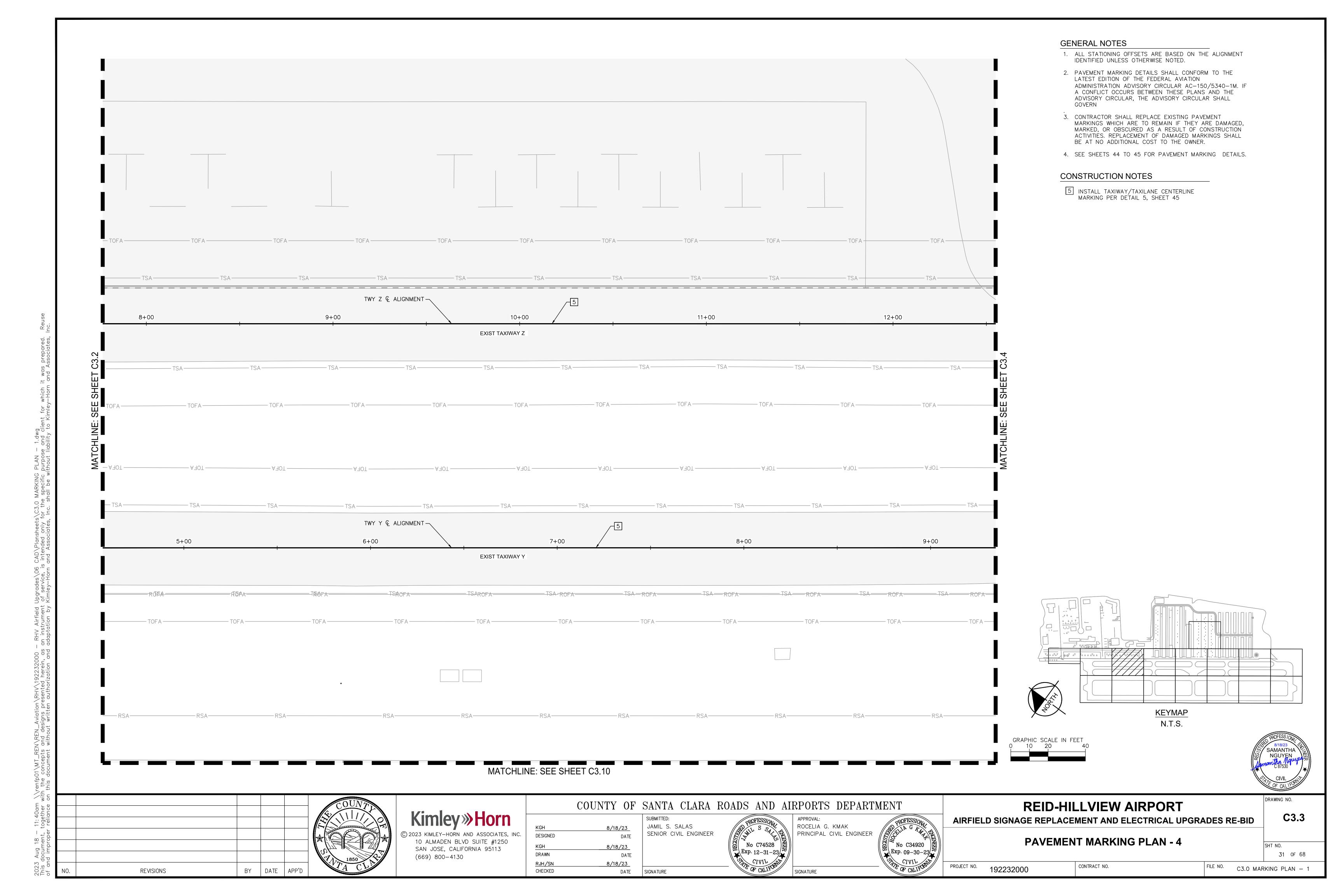
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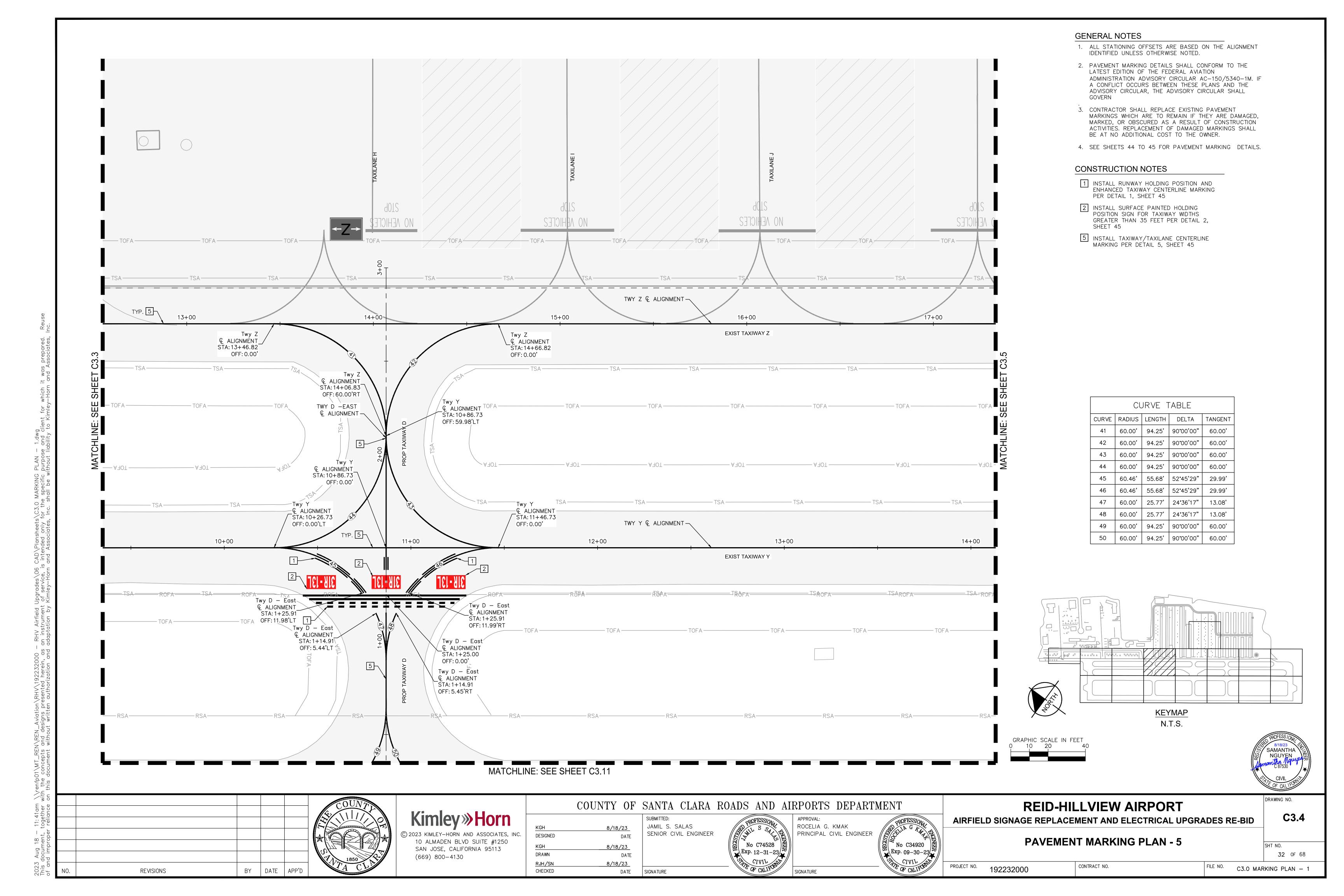


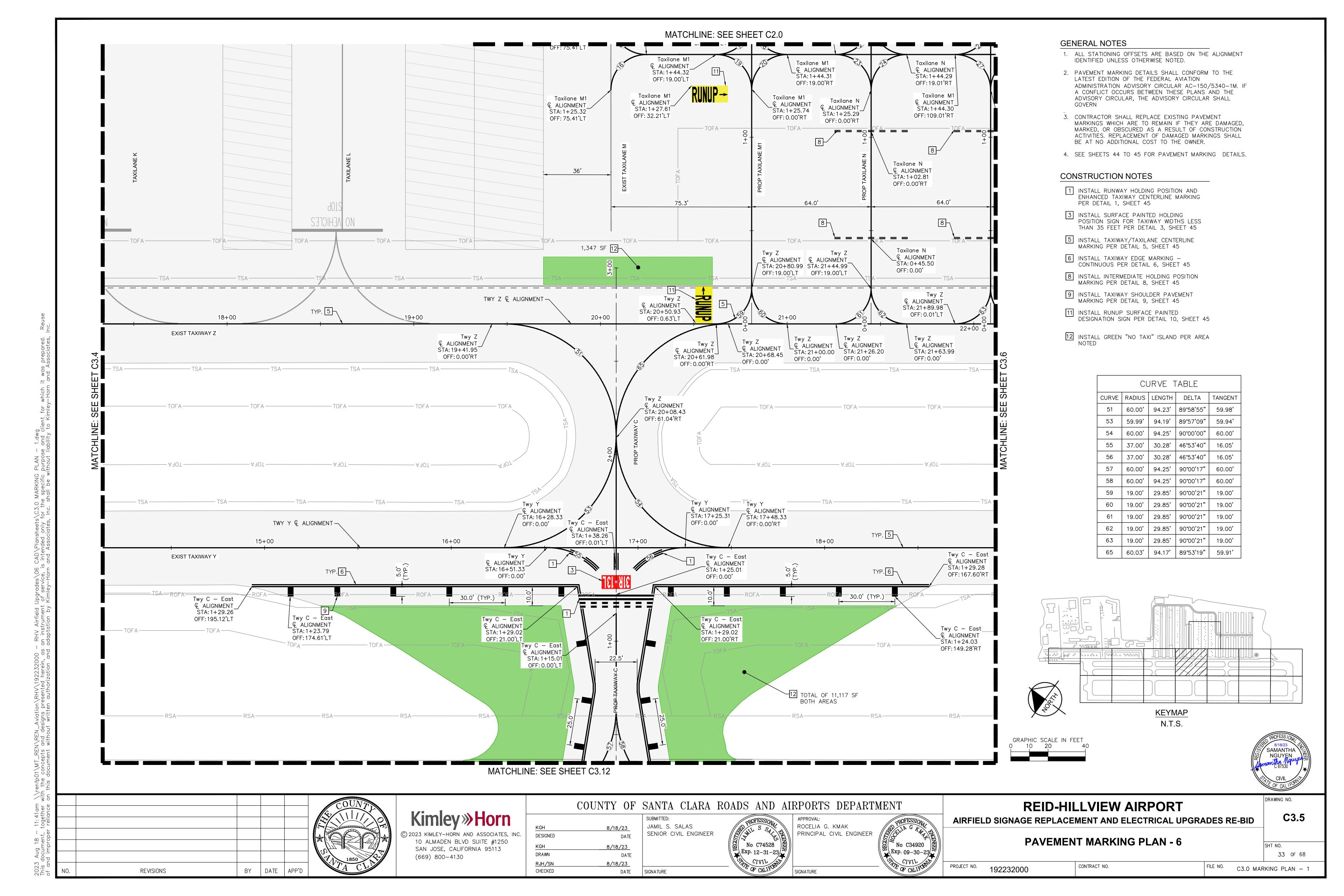
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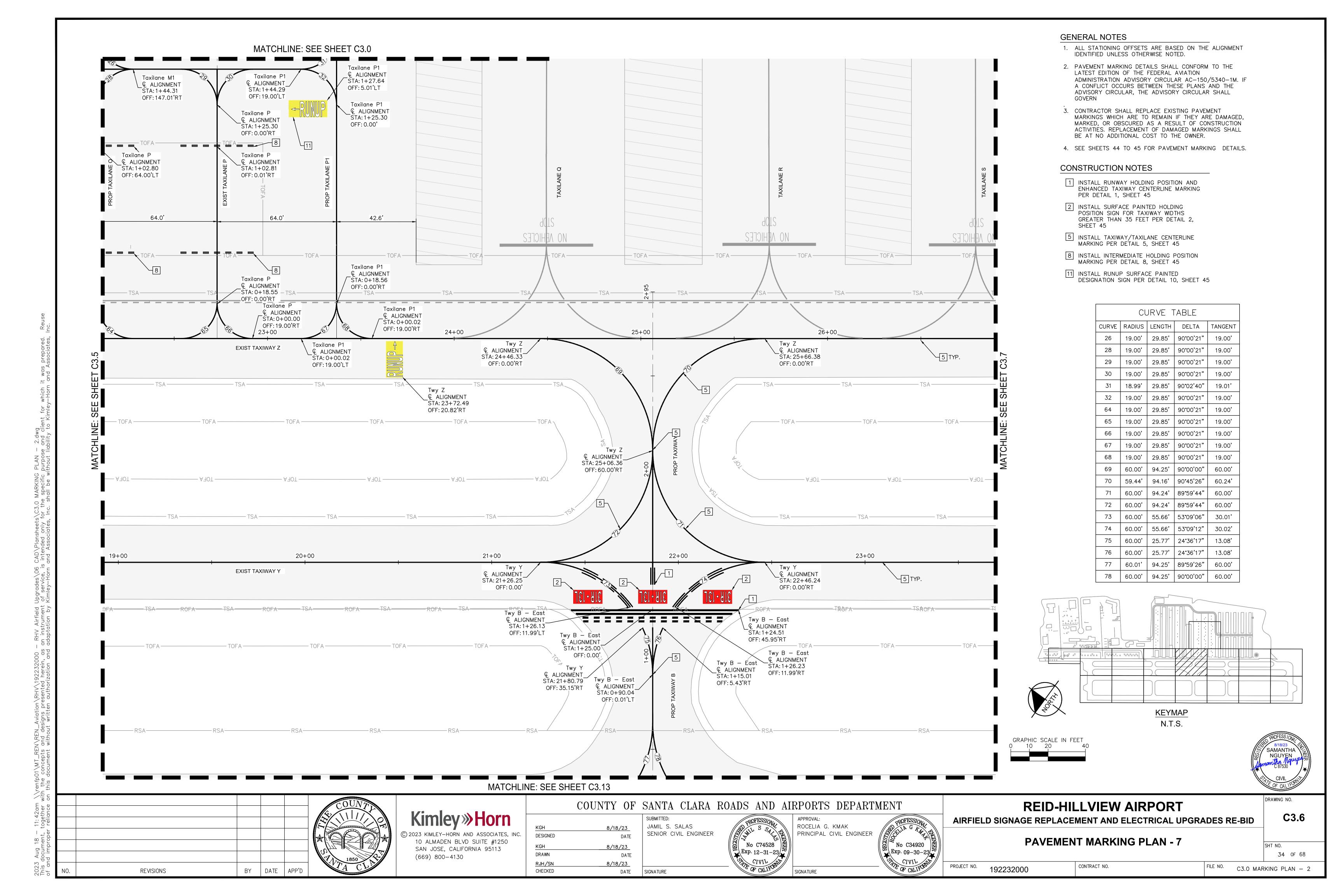


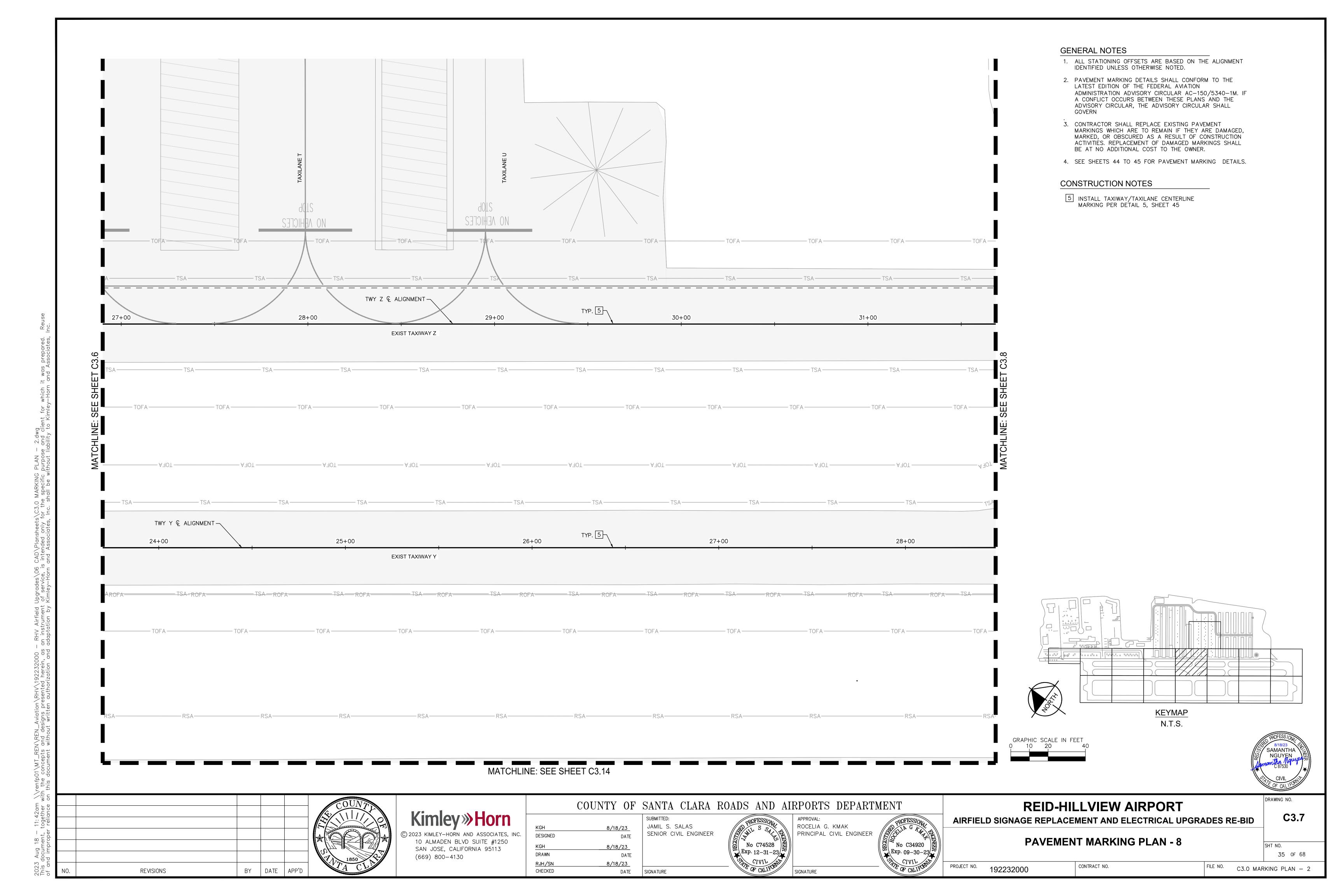


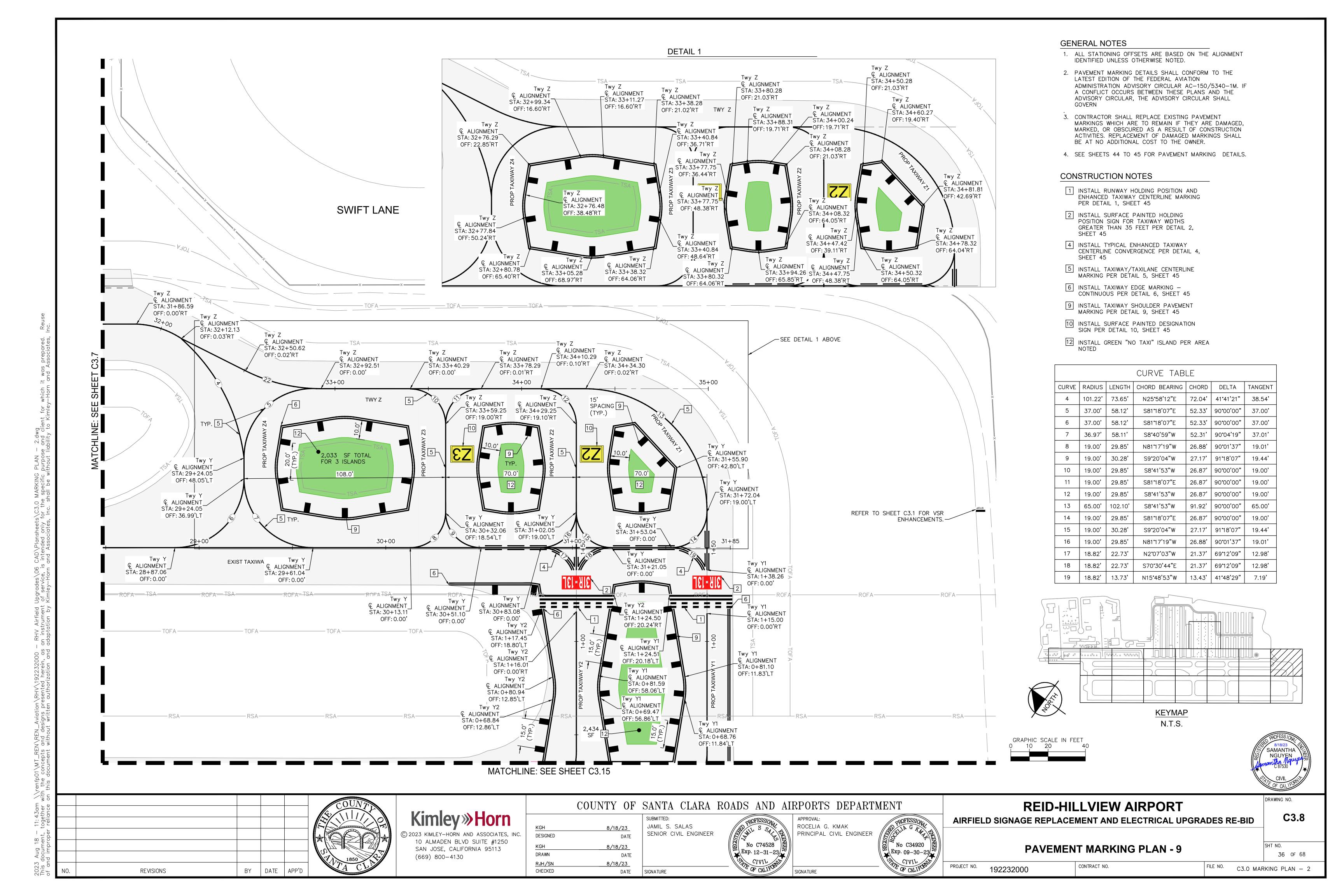


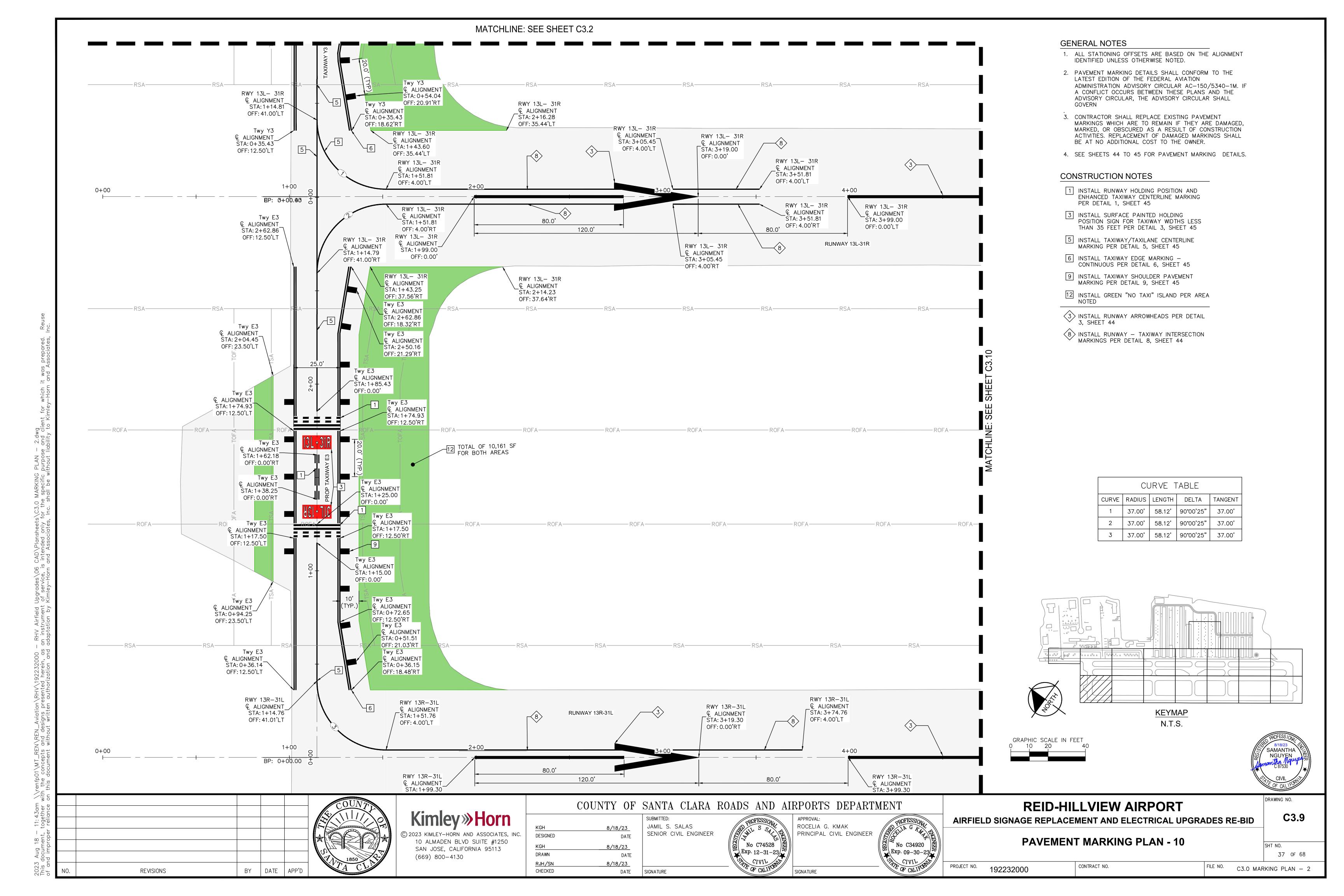


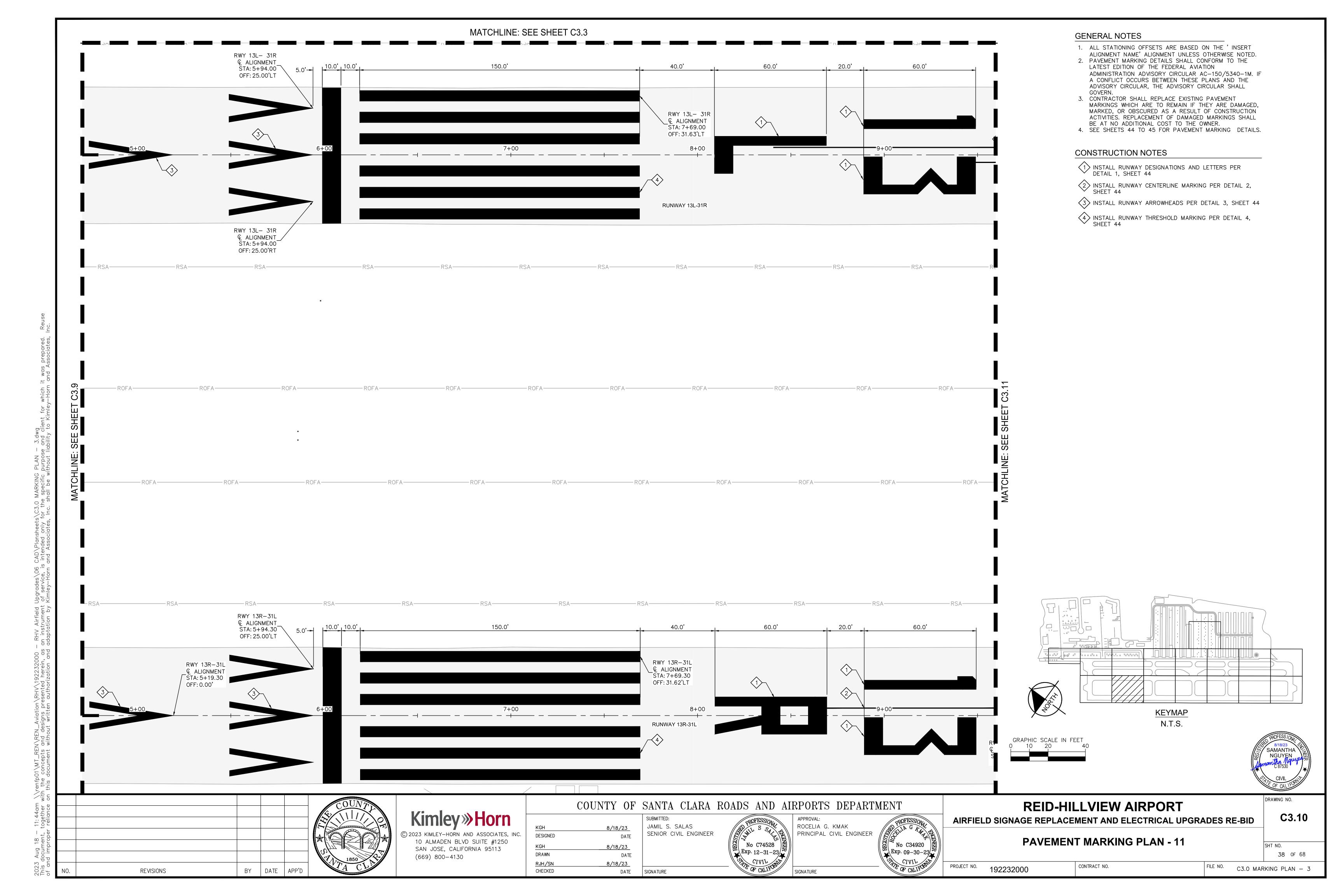


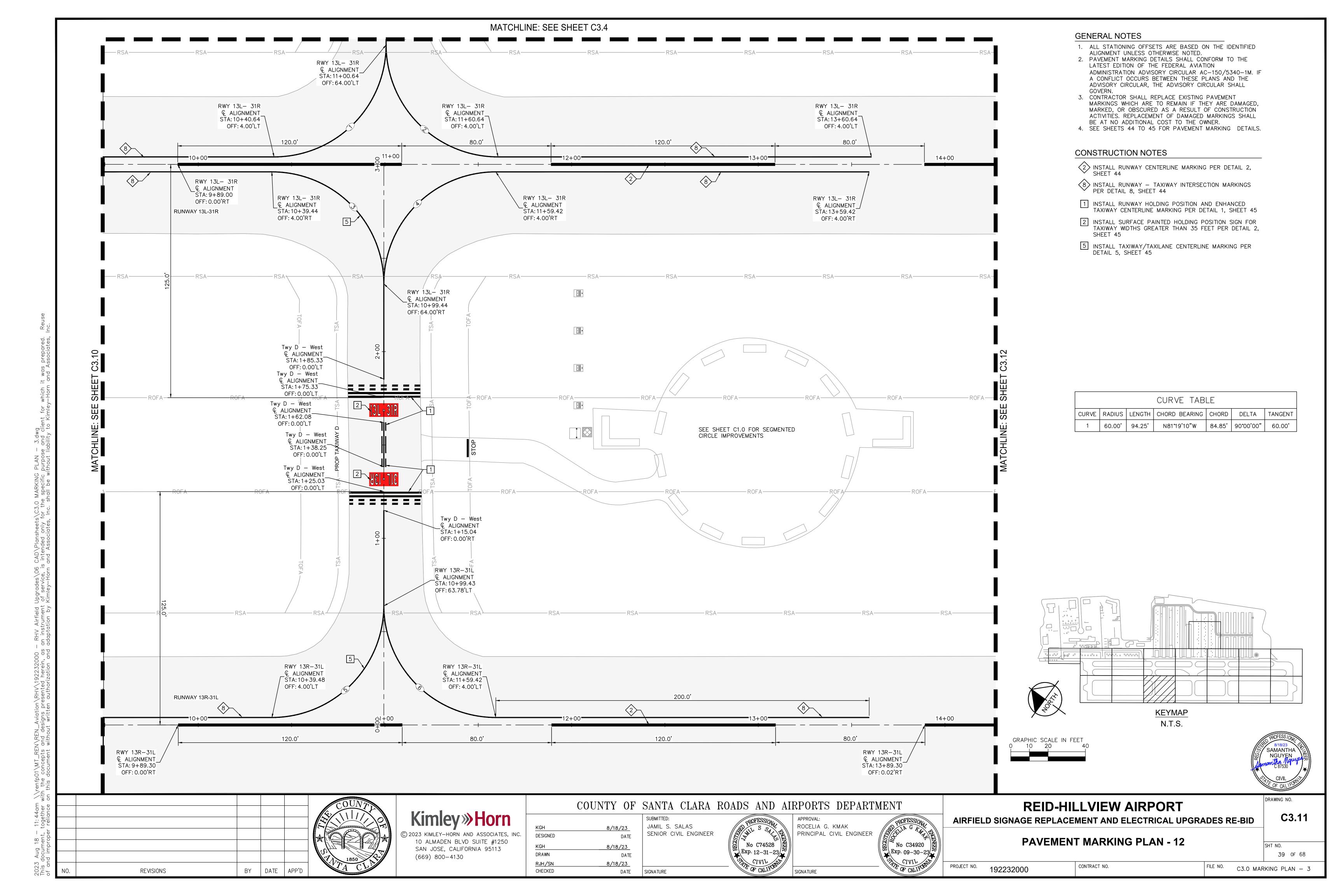


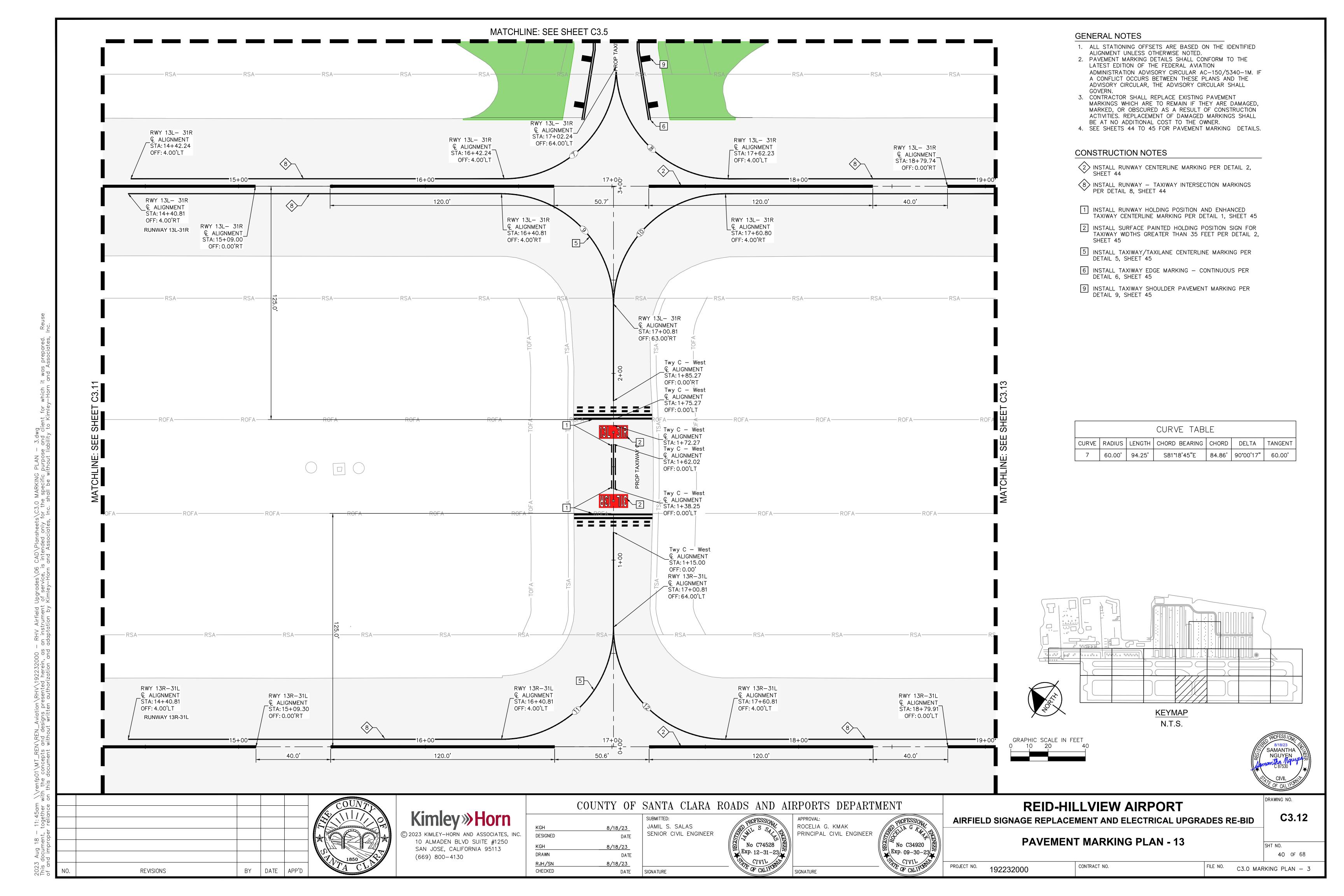


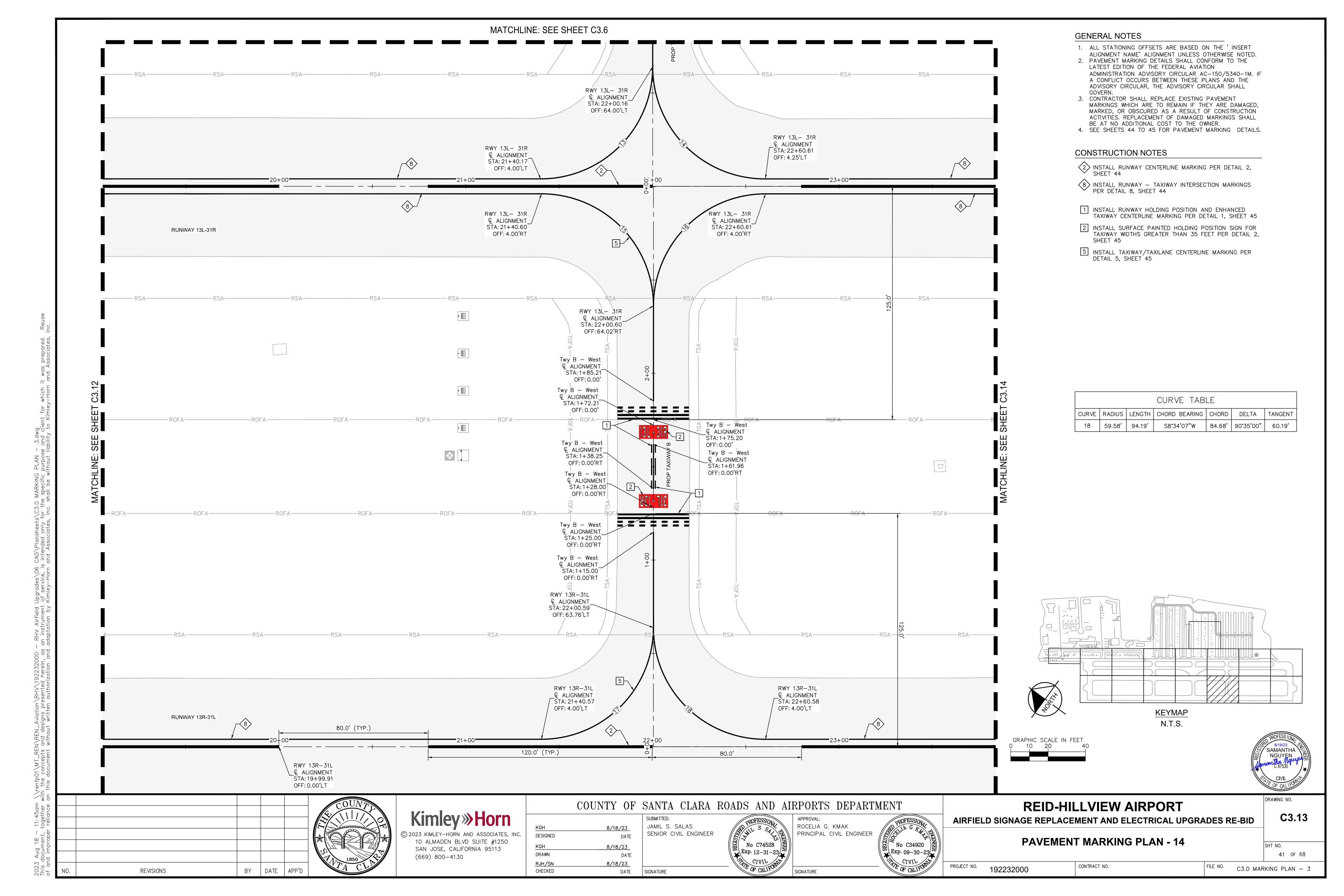


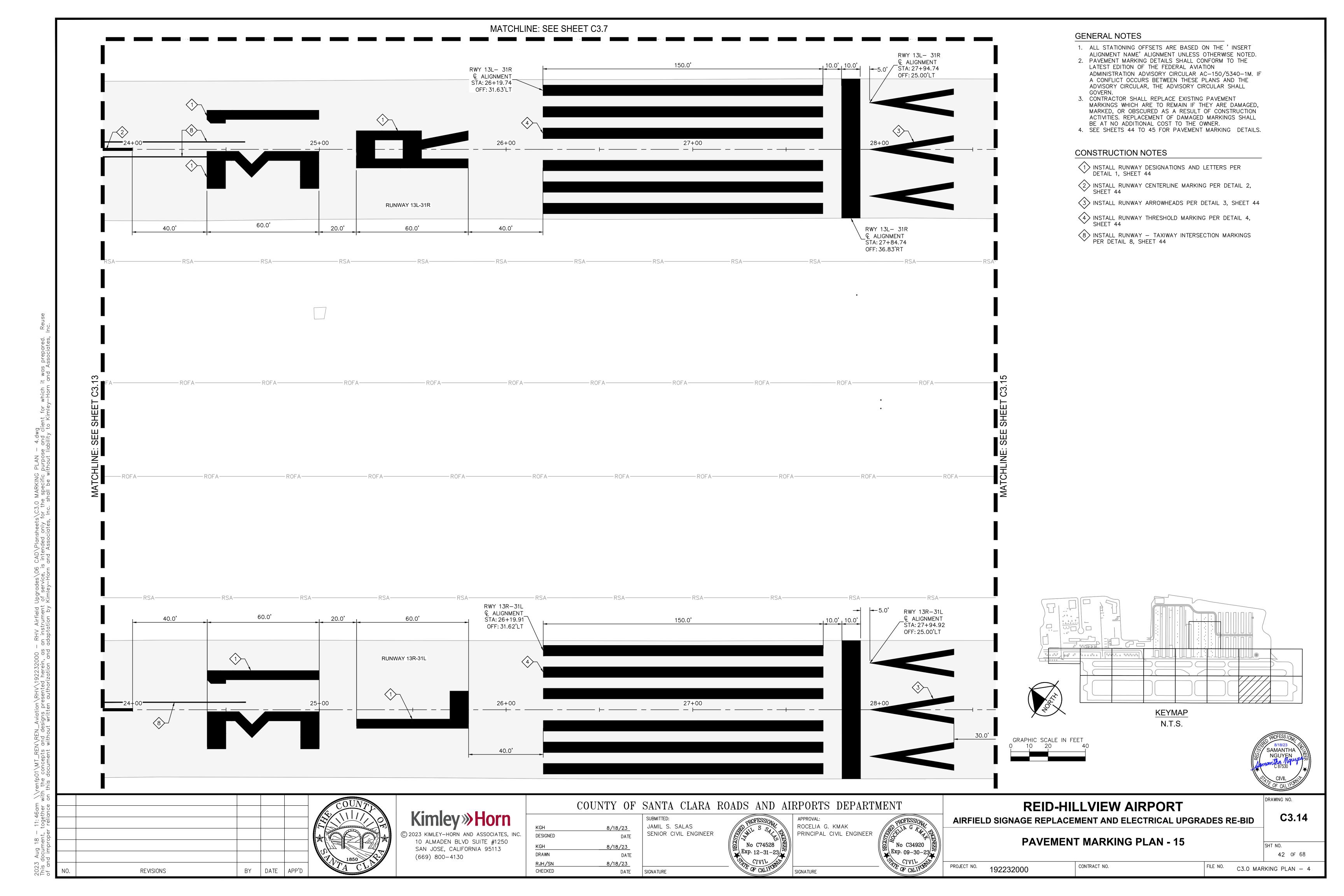


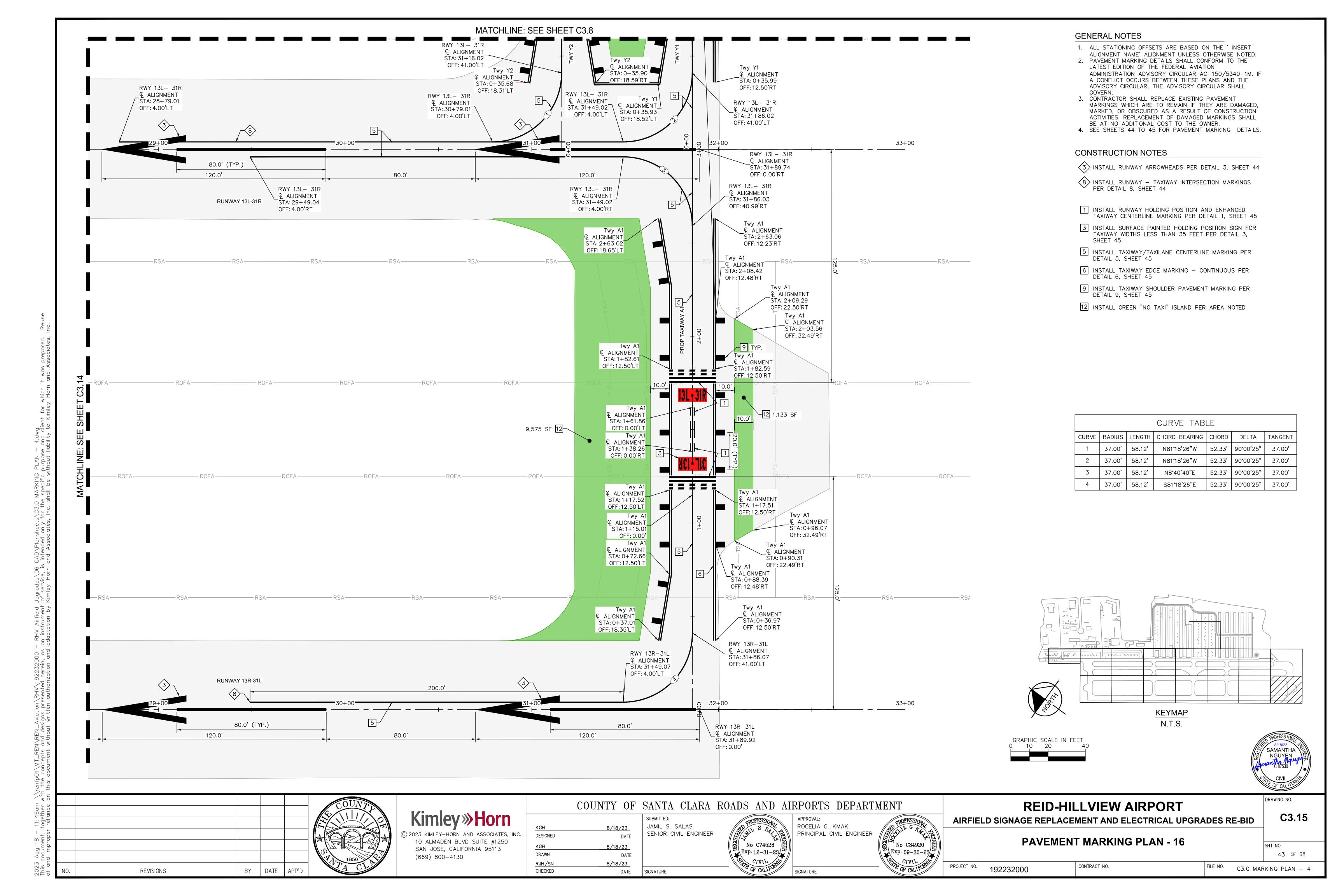


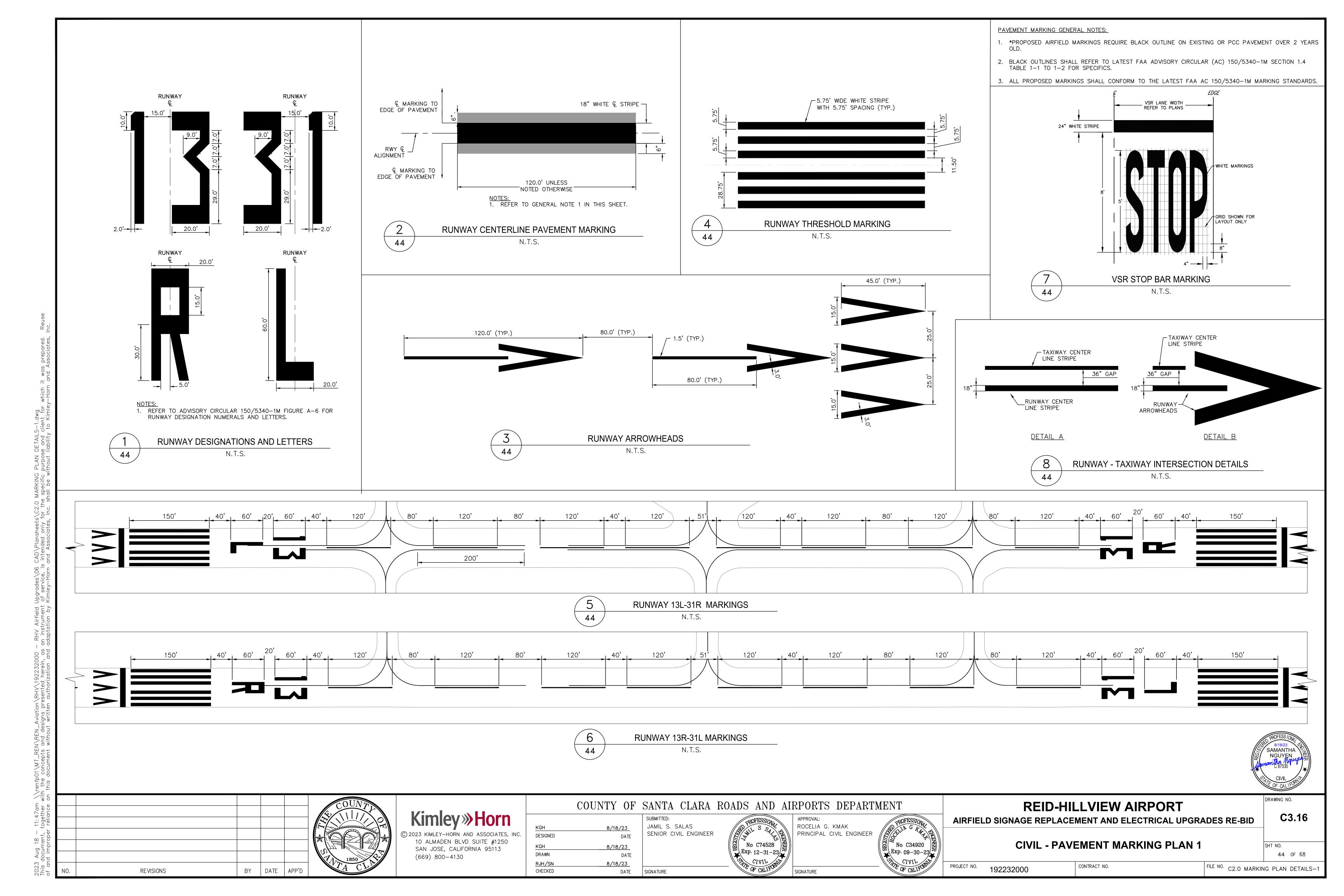


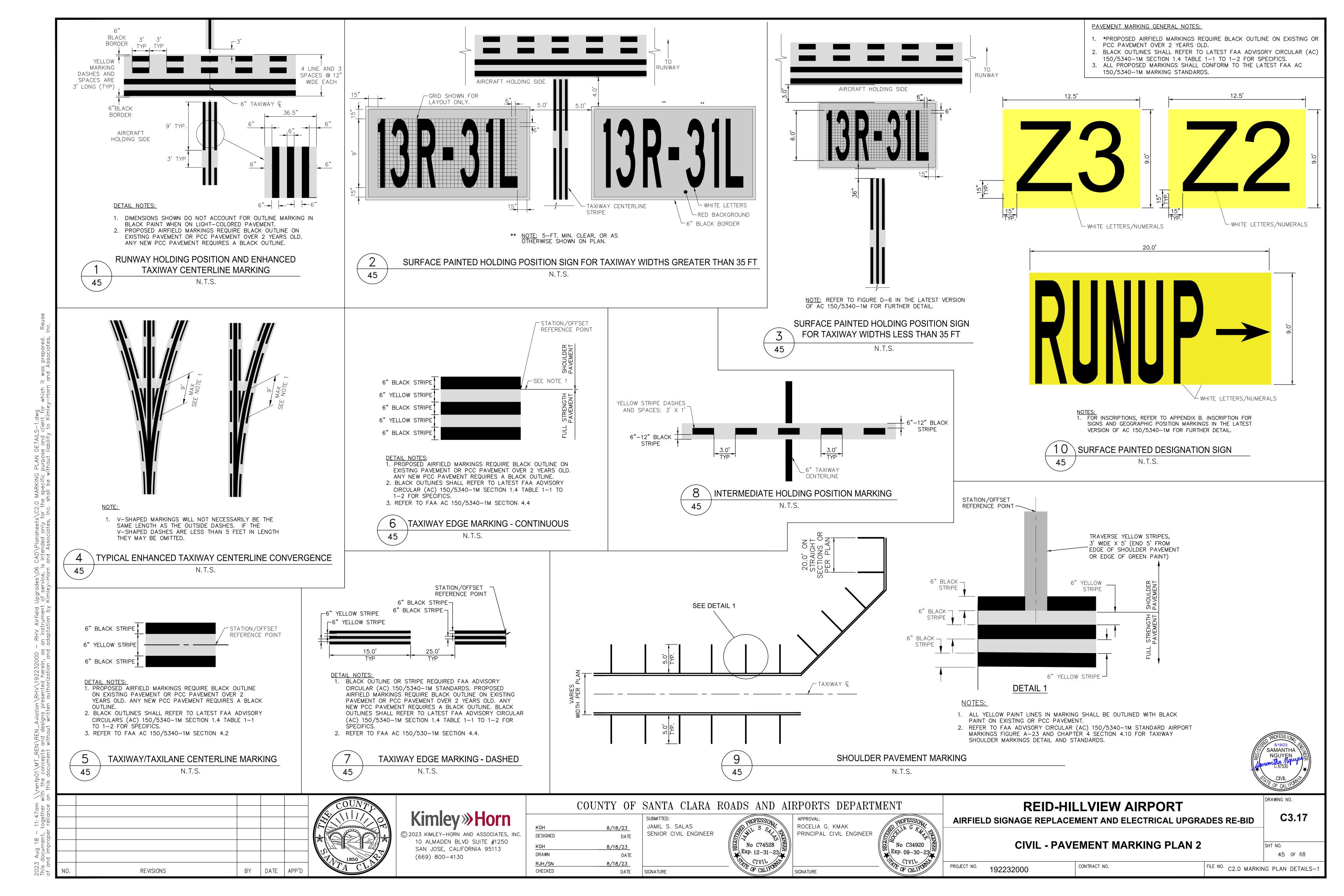


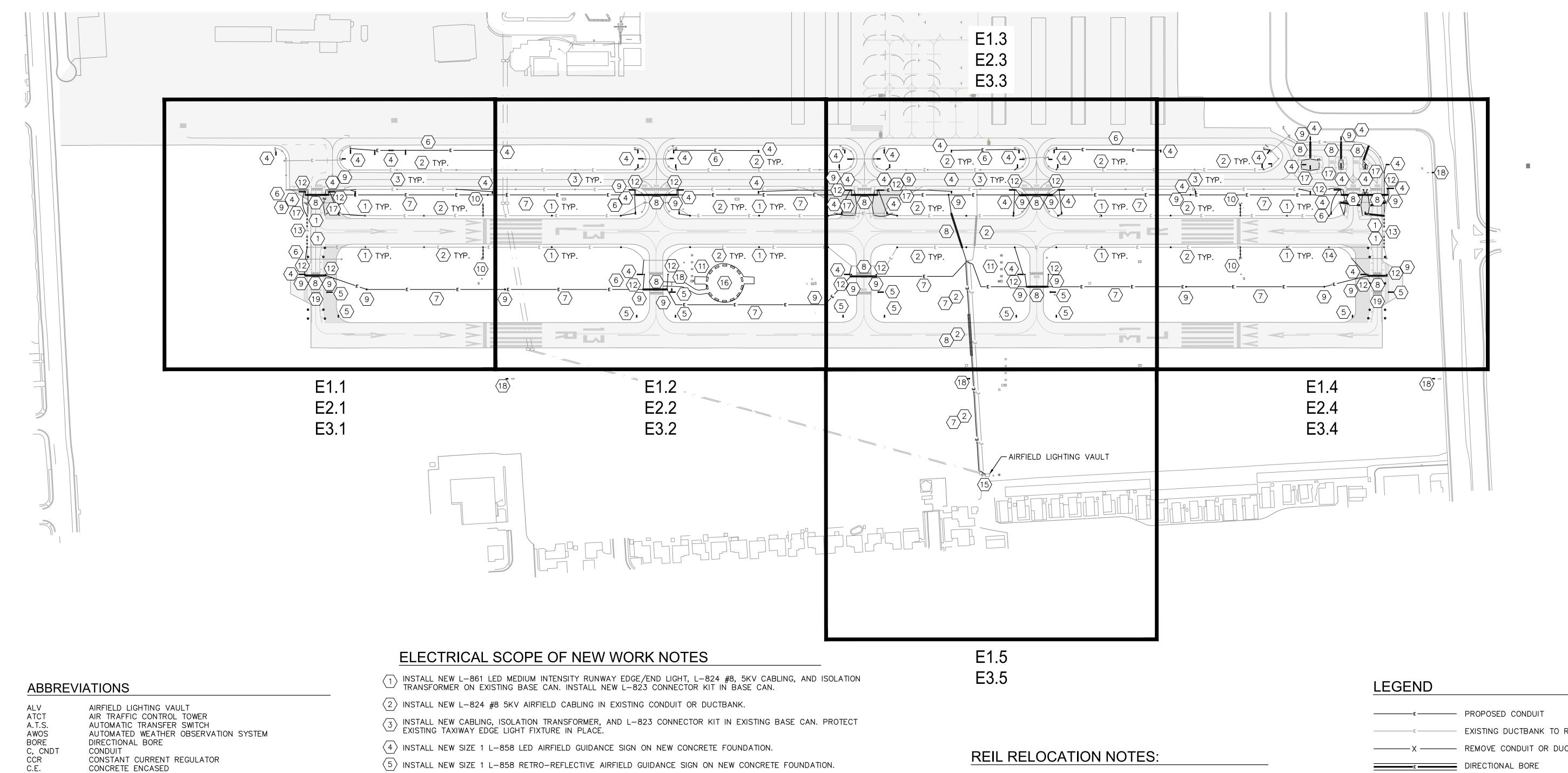












CLSM CONTROLLED LOW STRENGTH MATERIAL P-153

D.B. DIRECT EARTH BURIAL EXISTING EQUIPMENT OR FACILITY

(E) FAA FEDERAL AVIATION ADMINISTRATION

HH HANDHOLE (L-115)

MIRL MEDIUM INTENSITY RUNWAY LIGHTS MITL MEDIUM INTENSITY TAXIWAY LIGHTS

(N) NEW EQUIPMENT OR FACILITY ΡΑΡΙ PRECISION APPROACH PATH INDICATOR

P.O.C. POINT OF CONNECTION (WITH EXISTING CONDUIT OR DUCT) POLYVINYLCHLORIDE: CONDUIT MATERIAL PVC

(R) REMOVE EQUIPMENT OR FACILITY (RR) REMOVE AND REPLACE EQUIPMENT OR FACILITY

ŘDŔ RUNWAY DISTANCE REMAINING REIL RUNWAY END INDICATOR LIGHT

RGL RUNWAY GUARD LIGHT

RSA RUNWAY SAFETY AREA

R/W, RWY RUNWAY

SCHEDULE TSA TAXIWAY SAFETY AREA

T/W, TWY TAXIWAY

TÝP. TYPICAL

U.O.N. UNLESS OTHERWISE NOTED VASI VISUAL APPROACH SLOPE INDICATOR

- $\langle 6 \rangle$  INSTALL NEW 1-2" SCH. 40 PVC DIRECT BURIED CONDUIT AND L-824 #8, 5KV CABLING.
- $\langle 7 \rangle$  INSTALL NEW 1-2" SCH. 40 PVC DIRECT BURIED CONDUIT AND L-824 #8, 5KV CABLING FOR RGL CIRCUIT.
- $\langle 8 \rangle$  INSTALL NEW 1-2" DIRECTIONAL BORE SCH. 80 HDPE CONDUIT AND L-824 #8, 5KV CABLING.
- (9) INSTALL NEW L-867 BASE CAN.
- \$\langle 10 \rangle FAA OWNED AND MAINTAINED REIL TO BE SALVAGED AND REINSTALLED BY CONTRACTOR. SEE REIL NOTES THIS SHEET.
- (11) PROTECT EXISTING FAA OWNED AND MAINTAINED PAPI IN PLACE.
- $\langle 12 \rangle$  install New L-804 Led elevated runway guard light with New Basecan and Concrete Foundation.
- INSTALL NEW L-861E LED ELEVATED RUNWAY END LIGHT AND L-824 #8, 5KV CABLING ON L-867 BASE CAN WITH NEW ISOLATION TRANSFORMER.
- INTERCEPT EXISTING CONDUIT AND INSTALL NEW L-861 LED ELEVATED RUNWAY DISPLACED THRESHOLD EDGE LIGHT AND L-824 #8, 5KV CABLING ON NEW L-867 BASE CAN WITH NEW ISOLATION TRANSFORMER.
- INSTALL (1) NEW 7.5KW, (2) NEW 10KW, AND (1) NEW 15KW FERRORESONANT CCRS IN ALV. USE EXISTING SPARE 50A/2P BREAKER FOR NEW RGL CCR.
- (16) SEE CIVIL SHEET C1.0 FOR SEGMENTED CIRCLE IMPROVEMENTS.
- INSTALL NEW L-861T QUARTZ ELEVATED TAXIWAY EDGE LIGHT AND L-824 #8, 5KV CABLING ON NEW L-867 BASE CAN WITH NEW ISOLATION TRANSFORMER.

DESIGNED

DRAWN

CHECKED

- (18) INSTALL NEW VSR RETROREFLECTIVE SIGN.
- (19) INSTALL NEW RETROREFLECTIVE MARKER.

- 1. FINAL LOCATION OF REIL TO BE COORDINATED WITH FAA AFTER
- AWARD. 2. REFER TO FIGURE A-79 OF FAA ADVISORY CIRCULAR 150/5340-30J FOR TOLERANCES (0-40' OFF RUNWAY EDGE; +30' IN FRONT OF
- THRESHOLD, -100' BEHIND THRESHOLD) 3. THE INTENT IS TO MOVE THE REILS MINIMALLY AS POSSIBLE TO DE-CONFLICT THRESHOLD LIGHT PLACEMENT AND STILL MEET REIL PLACEMENT TOLERANCES. REFER TO FIGURE A-93 OF ADVISORY
- CIRCULAR 150/5340-30J FOR DETAILS. 4. REIL FOUNDATIONS SHALL BE 4,000 PSI CONCRETE WITH DIMENSIONS OF 3' DEEP BY 3' LONG BY 2' WIDTH. FOUNDATIONS SHALL HAVE #4 REBARS ON 12" CENTERS ON ALL FACES AND CORNERS. REFER TO SPECIFICATION 110-14-11 FOR UNDERGROUND POWER CABLE
- REQUIREMENTS FOR AIRPORTS. 5. IF REQUESTED BY CONTRACTOR, RELOCATING REIL WITH EXISTING BASE WILL ALSO BE CONSIDERED IF EXISTING BASE IS DETERMINED TO BE IN SUFFICIENT CONDITION BY THE RPR.

EXISTING DUCTBANK TO REMAIN ----- REMOVE CONDUIT OR DUCTBANK

EXISTING HANDHOLE TO REMAIN R−XX L-861E LED RUNWAY END / THRESHOLD LIGHT

R−XX L-861 LED RUNWAY EDGE LIGHT  $^{\mathsf{T-XX}} \otimes$ L-861T LED TAXIWAY EDGE LIGHT

ELEVATED LED RGL

BC-XX L-867 LED BASE CAN WITH BLANK STEEL COVER  $M-XX \longrightarrow$ 

L-853 RETROREFLECTIVE MARKER

REIL

S-XXAIRFIELD GUIDANCE SIGN WINDCONE

PAPI RGL-XX



# BY DATE APP'D REVISIONS



2021 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

	SUBMITTE
<u>8/18/23</u> DATE	JAMIL SENIOI
8/18/23	
DATE	
8/18/23	

SIGNATURE

DATE

S. SALAS OR CIVIL ENGINEER No C74528 **Exp.** 12−31−23/20

### COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 \Exp. 09-30-23

PROJECT NO.

192232000

### **REID-HILLVIEW AIRPORT**

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

### **ELECTRICAL SCOPE OF WORK**

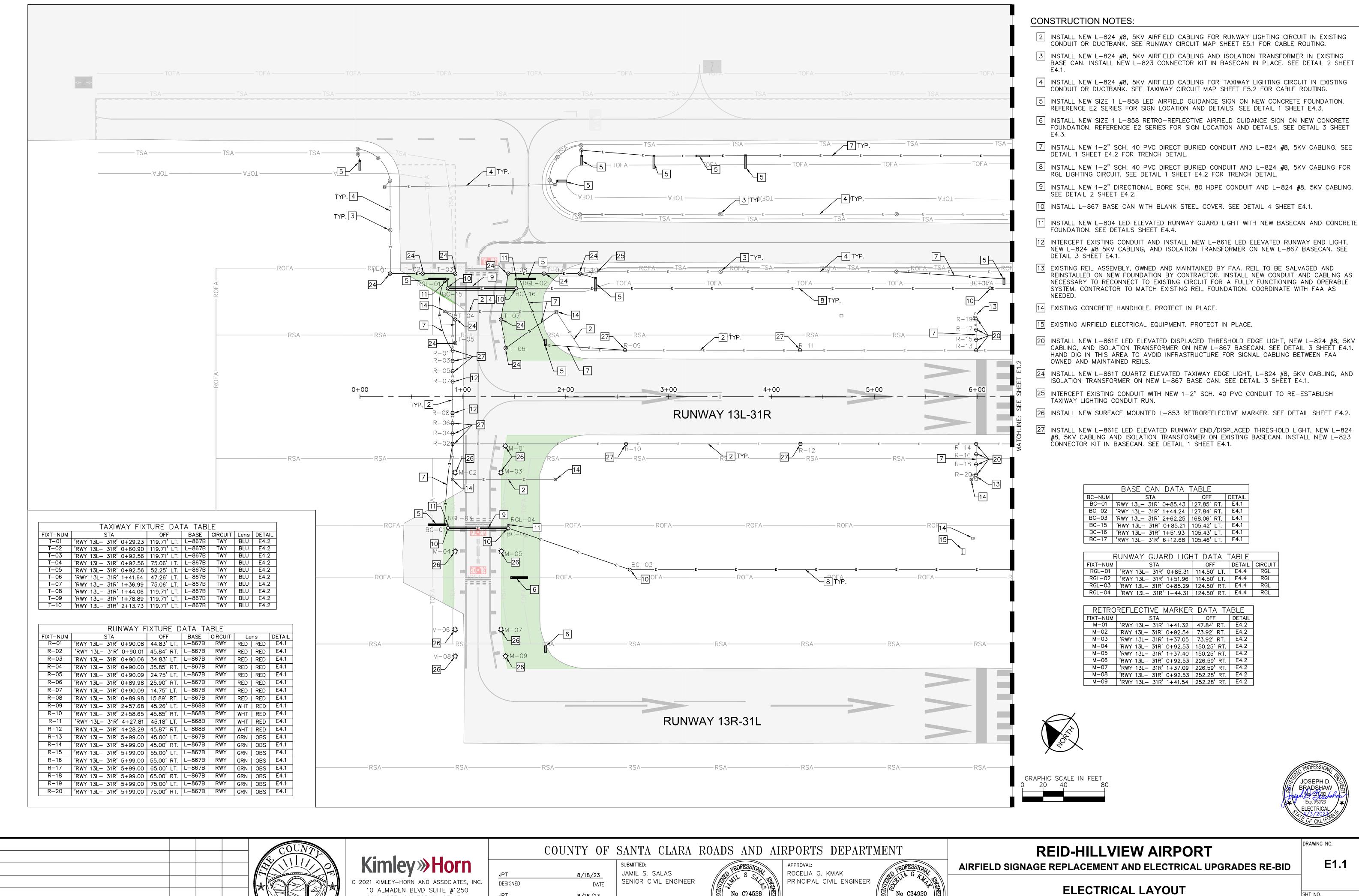
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SHT NO.

DRAWING NO.

FILE NO.

46 OF 68



No C74528

8/18/23

8/18/23

DATE

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SAN JOSE, CALIFORNIA 95113

(669) 800-4130

BY DATE

REVISIONS

No C34920

∧Exp. 09-30-23/₹

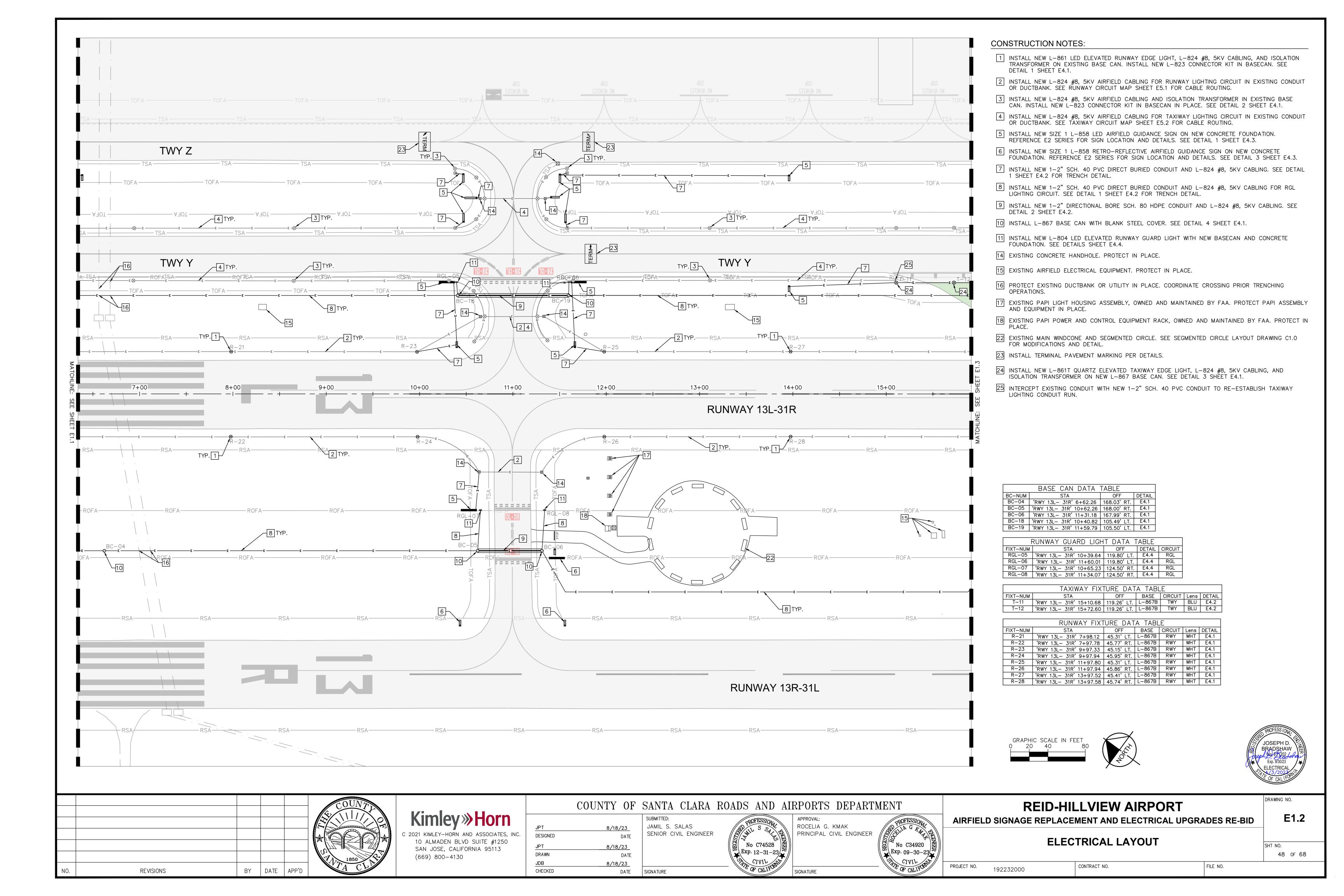
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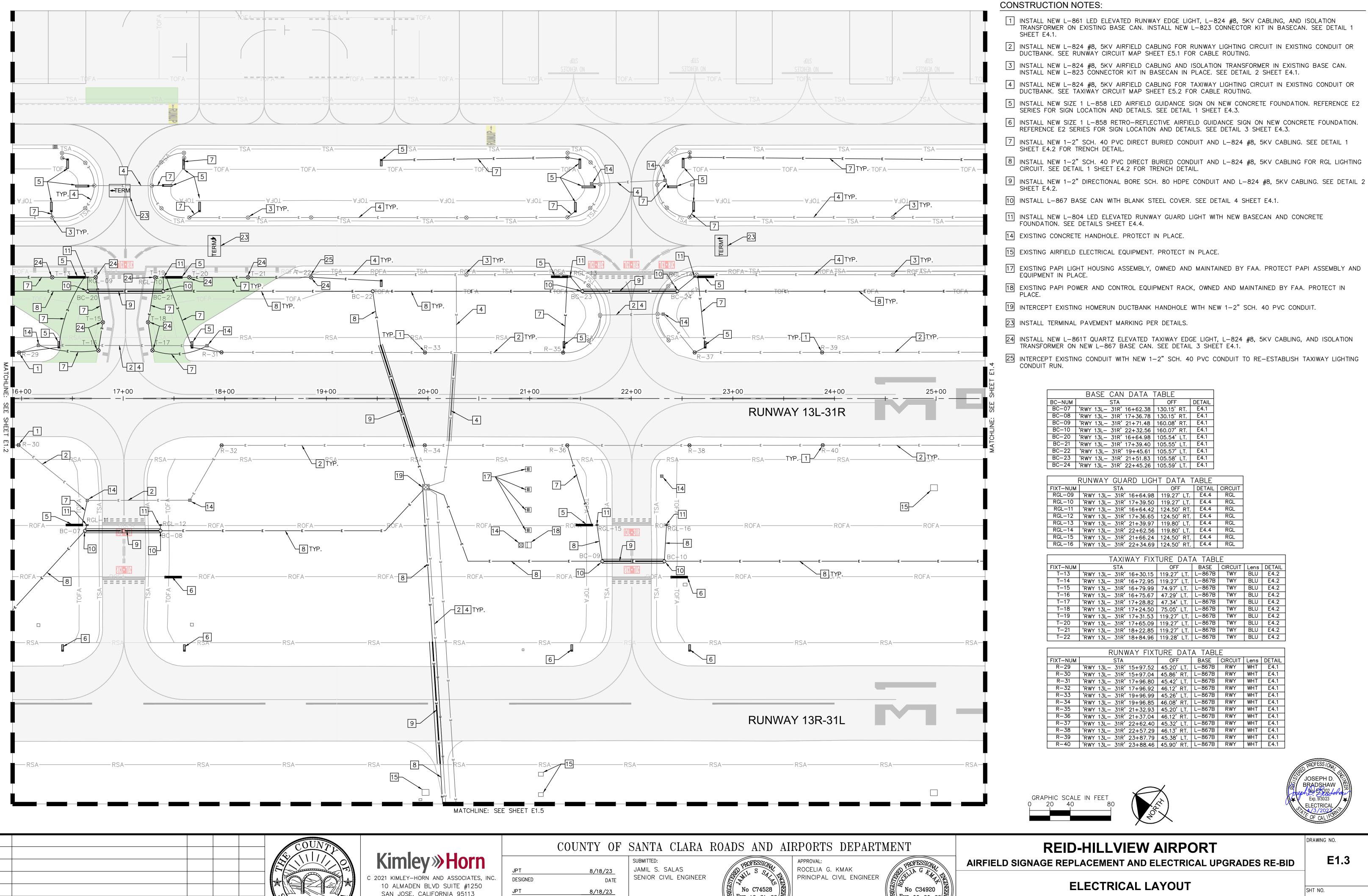
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SHT NO. 47 OF 68

FILE NO.

CONTRACT NO.





√Exp. 09-30-23/≈

PROJECT NO.

192232000

CONTRACT NO.

SAN JOSE, CALIFORNIA 95113

(669) 800 - 4130

BY DATE

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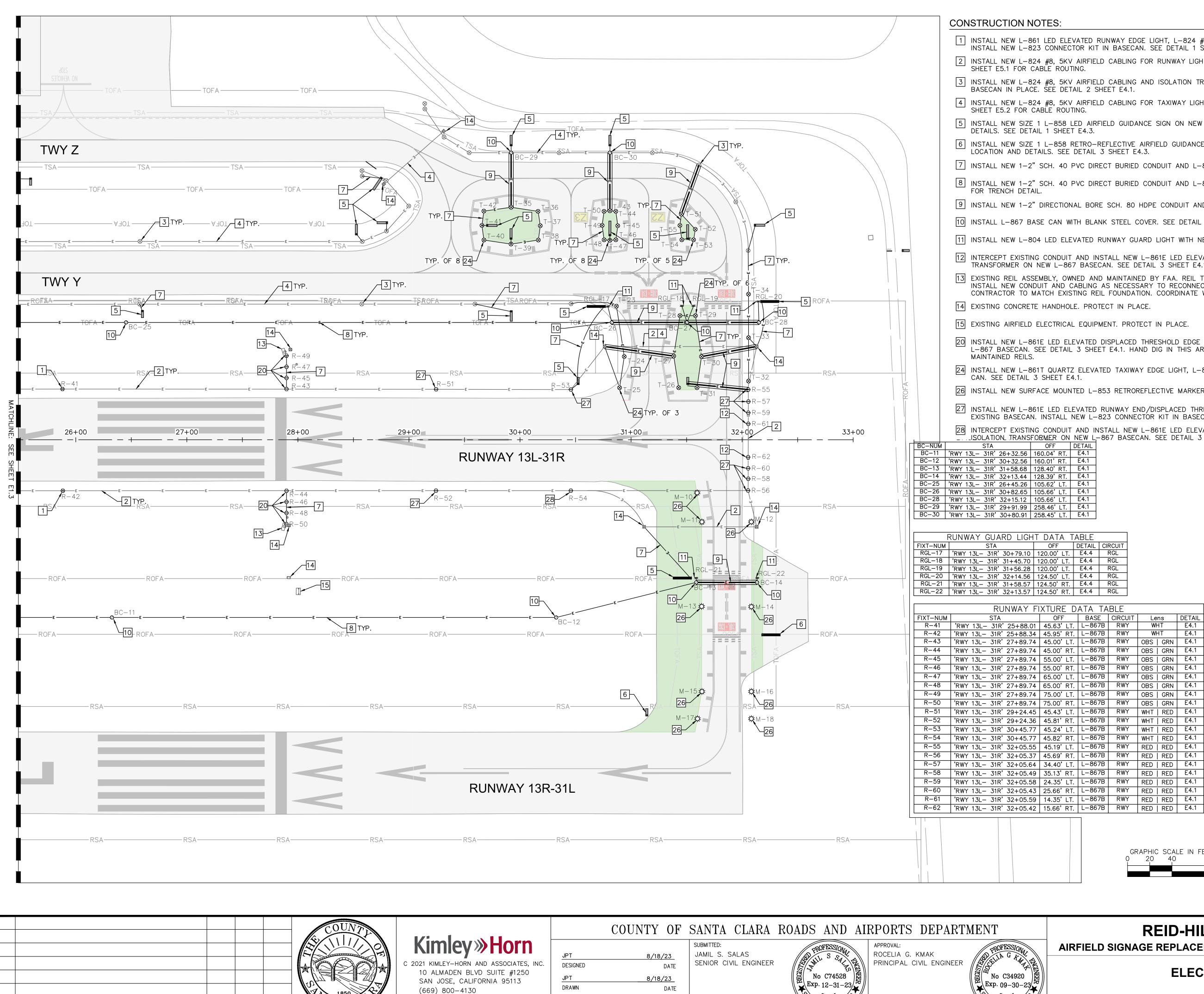
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8/18/23

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49 OF 68



BY DATE

REVISIONS

8/18/23

DATE

SIGNATURE

CHECKED

### **CONSTRUCTION NOTES:**

- 1 INSTALL NEW L-861 LED ELEVATED RUNWAY EDGE LIGHT, L-824 #8, 5KV CABLING, AND ISOLATION TRANSFORMER ON EXISTING BASE CAN. INSTALL NEW L-823 CONNECTOR KIT IN BASECAN. SEE DETAIL 1 SHEET E4.1.
- 2 INSTALL NEW L-824 #8, 5KV AIRFIELD CABLING FOR RUNWAY LIGHTING CIRCUIT IN EXISTING CONDUIT OR DUCTBANK. SEE RUNWAY CIRCUIT MAP SHEET E5.1 FOR CABLE ROUTING.
- 3 INSTALL NEW L-824 #8, 5KV AIRFIELD CABLING AND ISOLATION TRANSFORMER IN EXISTING BASE CAN. INSTALL NEW L-823 CONNECTOR KIT IN BASECAN IN PLACE. SEE DETAIL 2 SHEET E4.1.
- 4 INSTALL NEW L-824 #8, 5KV AIRFIELD CABLING FOR TAXIWAY LIGHTING CIRCUIT IN EXISTING CONDUIT OR DUCTBANK. SEE TAXIWAY CIRCUIT MAP SHEET E5.2 FOR CABLE ROUTING.
- 5 INSTALL NEW SIZE 1 L-858 LED AIRFIELD GUIDANCE SIGN ON NEW CONCRETE FOUNDATION. REFERENCE E2 SERIES FOR SIGN LOCATION AND
- 6 INSTALL NEW SIZE 1 L-858 RETRO-REFLECTIVE AIRFIELD GUIDANCE SIGN ON NEW CONCRETE FOUNDATION. REFERENCE E2 SERIES FOR SIGN LOCATION AND DETAILS. SEE DETAIL 3 SHEET E4.3.
- 7 INSTALL NEW 1-2" SCH. 40 PVC DIRECT BURIED CONDUIT AND L-824 #8, 5KV CABLING. SEE DETAIL 1 SHEET E4.2 FOR TRENCH DETAIL.
- 8 INSTALL NEW 1-2" SCH. 40 PVC DIRECT BURIED CONDUIT AND L-824 #8, 5KV CABLING FOR RGL LIGHTING CIRCUIT. SEE DETAIL 1 SHEET E4.2
- 9 INSTALL NEW 1-2" DIRECTIONAL BORE SCH. 80 HDPE CONDUIT AND L-824 #8, 5KV CABLING. SEE DETAIL 2 SHEET E4.2.
- 10 INSTALL L-867 BASE CAN WITH BLANK STEEL COVER. SEE DETAIL 4 SHEET E4.1.
- 11 INSTALL NEW L-804 LED ELEVATED RUNWAY GUARD LIGHT WITH NEW BASECAN AND CONCRETE FOUNDATION. SEE DETAILS SHEET E4.4.
- 12 INTERCEPT EXISTING CONDUIT AND INSTALL NEW L-861E LED ELEVATED RUNWAY END LIGHT, NEW L-824 #8 5KV CABLING, AND ISOLATION TRANSFORMER ON NEW L-867 BASECAN. SEE DETAIL 3 SHEET E4.1.
- EXISTING REIL ASSEMBLY, OWNED AND MAINTAINED BY FAA. REIL TO BE SALVAGED AND REINSTALLED ON NEW FOUNDATION BY CONTRACTOR. INSTALL NEW CONDUIT AND CABLING AS NECESSARY TO RECONNECT TO EXISTING CIRCUIT FOR A FULLY FUNCTIONING AND OPERABLE SYSTEM. CONTRACTOR TO MATCH EXISTING REIL FOUNDATION. COORDINATE WITH FAA AS NEEDED.
- 14 EXISTING CONCRETE HANDHOLE. PROTECT IN PLACE.
- 15 EXISTING AIRFIELD ELECTRICAL EQUIPMENT. PROTECT IN PLACE.
- INSTALL NEW L-861E LED ELEVATED DISPLACED THRESHOLD EDGE LIGHT, NEW L-824 #8, 5KV CABLING, AND ISOLATION TRANSFORMER ON NEW L-867 BASECAN. SEE DETAIL 3 SHEET E4.1. HAND DIG IN THIS AREA TO AVOID INFRASTRUCTURE FOR SIGNAL CABLING BETWEEN FAA OWNED AND MAINTAINED REILS.
- 24 INSTALL NEW L-861T QUARTZ ELEVATED TAXIWAY EDGE LIGHT, L-824 #8, 5KV CABLING, AND ISOLATION TRANSFORMER ON NEW L-867 BASE CAN. SEE DETAIL 3 SHEET E4.1.
- 26 INSTALL NEW SURFACE MOUNTED L-853 RETROREFLECTIVE MARKER. SEE DETAIL SHEET E4.2.
- 27 INSTALL NEW L-861E LED ELEVATED RUNWAY END/DISPLACED THRESHOLD LIGHT, NEW L-824 #8, 5KV CABLING AND ISOLATION TRANSFORMER ON EXISTING BASECAN. INSTALL NEW L-823 CONNECTOR KIT IN BASECAN. SEE DETAIL 1 SHEET E4.1.

ш	RC-NOW	SIA	UFF	DETAIL
$\  \ $	BC-11	'RWY 13L- 31R' 26+32.56	160.04' RT.	E4.1
$\  \ $	BC-12	'RWY 13L- 31R' 30+32.56	160.01' RT.	E4.1
Ш	BC-13	'RWY 13L- 31R' 31+58.68	128.40' RT.	E4.1
IJ	BC-14	'RWY 13L- 31R' 32+13.44	128.39' RT.	E4.1
	BC-25	'RWY 13L- 31R' 26+45.26	105.62' LT.	E4.1
3	BC-26	'RWY 13L- 31R' 30+82.65	105.66' LT.	E4.1
	BC-28	'RWY 13L- 31R' 32+15.12	105.66' LT.	E4.1
	BC-29	'RWY 13L- 31R' 29+91.99	258.46' LT.	E4.1
	BC-30	'RWY 13L- 31R' 30+80.91	258.45' LT.	E4.1
	•		•	

RETRO	REFLECTIVE MARKER	DATA TA	BLE
FIXT-NUM	STA	OFF	DETAIL
M-10	'RWY 13L- 31R' 31+59.52	47.73' RT.	E4.2
M-11	'RWY 13L- 31R' 31+63.81	73.95' RT.	E4.2
M-12	'RWY 13L- 31R' 32+08.31	73.95' RT.	E4.2
M-13	'RWY 13L- 31R' 31+63.58	150.00' RT.	E4.2
M-14	'RWY 13L- 31R' 32+08.33	150.00' RT.	E4.2
M-15	'RWY 13L- 31R' 31+63.86	226.65' RT.	E4.2
M-16	'RWY 13L- 31R' 32+08.36	226.65' RT.	E4.2
M-17	'RWY 13L- 31R' 31+59.86	251.15' RT.	E4.2
M-18	'RWY 13L- 31R' 32+08.37	251.15' RT.	E4.2
	·	•	

RUNWAY GUARD LIGHT DATA TABLE				
FIXT-NUM	STA	OFF	DETAIL	CIRCUIT
RGL-17	'RWY 13L- 31R' 30+79.10	120.00' LT.	E4.4	RGL
RGL-18	'RWY 13L- 31R' 31+45.70	120.00' LT.	E4.4	RGL
RGL-19	'RWY 13L- 31R' 31+56.28	120.00' LT.	E4.4	RGL
RGL-20	'RWY 13L- 31R' 32+14.56	124.50' LT.	E4.4	RGL
RGL-21	'RWY 13L- 31R' 31+58.57	124.50' RT.	E4.4	RGL
RGL-22	'RWY 13L- 31R' 32+13.57	124.50' RT.	E4.4	RGL

	RUNWAY FI	IXTURE D	ATA TA	BLE		
FIXT-NUM	STA	OFF	BASE	CIRCUIT	Lens	DETAIL
R-41	'RWY 13L- 31R' 25+88.01	45.63' LT.	L-867B	RWY	WHT	E4.1
R-42	'RWY 13L- 31R' 25+88.34	45.95' RT.	L-867B	RWY	WHT	E4.1
R-43	'RWY 13L- 31R' 27+89.74	45.00' LT.	L-867B	RWY	OBS   GRN	E4.1
R-44	'RWY 13L- 31R' 27+89.74	45.00' RT.	L-867B	RWY	OBS   GRN	E4.1
R-45	'RWY 13L- 31R' 27+89.74	55.00' LT.	L-867B	RWY	OBS   GRN	E4.1
R-46	'RWY 13L- 31R' 27+89.74	55.00' RT.	L-867B	RWY	OBS   GRN	E4.1
R-47	'RWY 13L- 31R' 27+89.74	65.00' LT.	L-867B	RWY	OBS   GRN	E4.1
R-48	'RWY 13L- 31R' 27+89.74	65.00' RT.	L-867B	RWY	OBS   GRN	E4.1
R-49	'RWY 13L- 31R' 27+89.74	75.00' LT.	L-867B	RWY	OBS   GRN	E4.1
R-50	'RWY 13L- 31R' 27+89.74	75.00' RT.	L-867B	RWY	OBS   GRN	E4.1
R-51	'RWY 13L- 31R' 29+24.45	45.43' LT.	L-867B	RWY	WHT   RED	E4.1
R-52	'RWY 13L- 31R' 29+24.36	45.81' RT.	L-867B	RWY	WHT   RED	E4.1
R-53	'RWY 13L- 31R' 30+45.77	45.24' LT.	L-867B	RWY	WHT   RED	E4.1
R-54	'RWY 13L- 31R' 30+45.77	45.82' RT.	L-867B	RWY	WHT   RED	E4.1
R-55	'RWY 13L- 31R' 32+05.55	45.19' LT.	L-867B	RWY	RED   RED	E4.1
R-56	'RWY 13L- 31R' 32+05.37	45.69' RT.	L-867B	RWY	RED   RED	E4.1
R-57	'RWY 13L- 31R' 32+05.64	34.40' LT.	L-867B	RWY	RED   RED	E4.1
R-58	'RWY 13L- 31R' 32+05.49	35.13' RT.	L-867B	RWY	RED   RED	E4.1
R-59	'RWY 13L- 31R' 32+05.58	24.35' LT.	L-867B	RWY	RED   RED	E4.1
R-60	'RWY 13L- 31R' 32+05.43	25.66' RT.	L-867B	RWY	RED   RED	E4.1
R-61	'RWY 13L- 31R' 32+05.59	14.35' LT.	L-867B	RWY	RED   RED	E4.1

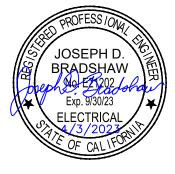
IX I — NOW	317	011	5	CIICOTT	Lelia	ואושע
T-23	'RWY 13L- 31R' 30+86.84	120.30' LT.	L-867B	TWY	BLU	E4.2
T-24	'RWY 13L- 31R' 30+93.88	74.92' LT.	L-867B	TWY	BLU	E4.2
T-25	'RWY 13L- 31R' 30+89.66	46.90' LT.	L-867B	TWY	BLU	E4.2
T-26	'RWY 13L- 31R' 31+42.86	46.91' LT.	L-867B	TWY	BLU	E4.2
T-27	'RWY 13L- 31R' 31+38.40	75.39' LT.	L-867B	TWY	BLU	E4.2
T-28	'RWY 13L- 31R' 31+43.99	111.76' LT.	L-867B	TWY	BLU	E4.2
T-29	'RWY 13L- 31R' 31+57.98	111.76' LT.	L-867B	TWY	BLU	E4.2
T-30	'RWY 13L- 31R' 31+63.38	74.92' LT.	L-867B	TWY	BLU	E4.2
T-31	'RWY 13L- 31R' 31+59.41	46.91'LT.	L-867B	TWY	BLU	E4.2
T-32	'RWY 13L- 31R' 32+05.76	52.84' LT.	L-867B	TWY	BLU	E4.2
T-33	'RWY 13L- 31R' 32+05.75	91.96'LT.	L-867B	TWY	BLU	E4.2
T-34	'RWY 13L- 31R' 32+05.74	131.08' LT.	L-867B	TWY	BLU	E4.2
T-35	'RWY 13L- 31R' 29+91.98	209.27' LT.	L-867B	TWY	BLU	E4.2
T-36	'RWY 13L- 31R' 30+16.25	205.66' LT.	L-867B	TWY	BLU	E4.2
T-37	'RWY 13L- 31R' 30+18.25	192.72' LT.	L-867B	TWY	BLU	E4.2
T-38	'RWY 13L- 31R' 30+16.50	179.83' LT.	L-867B	TWY	BLU	E4.2
T-39	'RWY 13L- 31R' 29+91.91	176.17' LT.	L-867B	TWY	BLU	E4.2
T-40	'RWY 13L- 31R' 29+67.49	179.81' LT.	L-867B	TWY	BLU	E4.2
T-41	'RWY 13L- 31R' 29+65.70	192.98' LT.	L-867B	TWY	BLU	E4.2
T-42	'RWY 13L- 31R' 29+67.45	205.88' LT.	L-867B	TWY	BLU	E4.2
T-43	'RWY 13L- 31R' 30+80.96	206.16' LT.	L-867B	TWY	BLU	E4.2
T-44	'RWY 13L- 31R' 30+86.25	205.65' LT.	L-867B	TWY	BLU	E4.2
T-45	'RWY 13L- 31R' 30+88.00	192.72' LT.	L-867B	TWY	BLU	E4.2
T-46	'RWY 13L- 31R' 30+86.29	180.05' LT.	L-867B	TWY	BLU	E4.2
T-47	'RWY 13L- 31R' 30+80.94	179.54' LT.	L-867B	TWY	BLU	E4.2
T-48	'RWY 13L- 31R' 30+75.71	180.04' LT.	L-867B	TWY	BLU	E4.2
T-49	'RWY 13L- 31R' 30+73.95	192.98' LT.	L-867B	TWY	BLU	E4.2
T-50	'RWY 13L- 31R' 30+75.66	205.65' LT.	L-867B	TWY	BLU	E4.2
T-51	'RWY 13L- 31R' 31+45.25	203.10' LT.	L-867B	TWY	BLU	E4.2
T-52	'RWY 13L- 31R' 31+57.83	189.49' LT.	L-867B	TWY	BLU	E4.2
T-53	'RWY 13L- 31R' 31+56.29	180.06' LT.	L-867B	TWY	BLU	E4.2
T-54	'RWY 13L- 31R' 31+45.71	180.04' LT.	L-867B	TWY	BLU	E4.2
T-55	'RWY 13L- 31R' 31+43.95	192.98' LT.	L-867B	TWY	BLU	E4.2

TAXIWAY FIXTURE DATA TABLE

OFF BASE CIRCUIT Lens DETAI







**REID-HILLVIEW AIRPORT** 

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

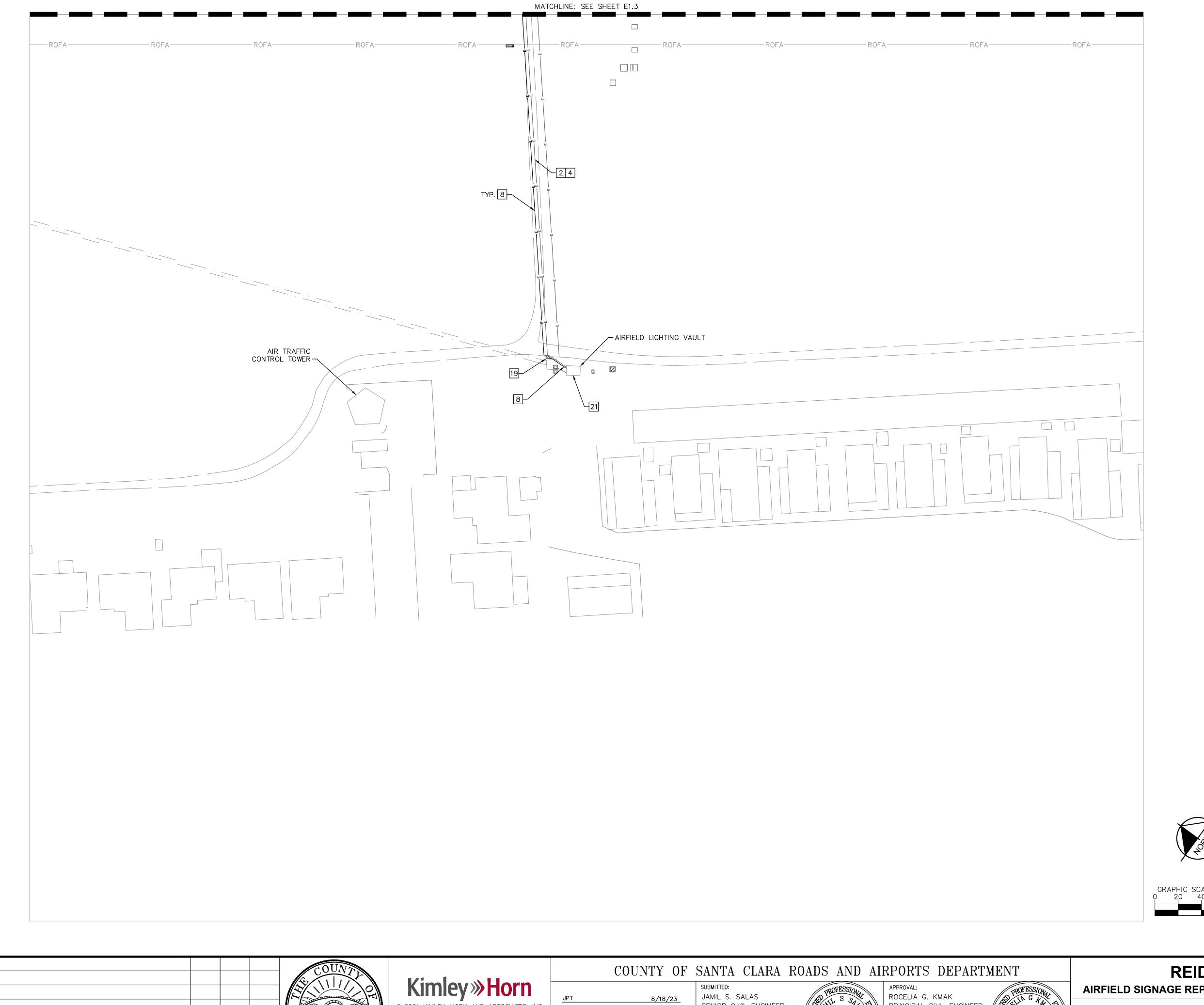
E1.4

DRAWING NO.

**ELECTRICAL LAYOUT** 

50 OF 68

CONTRACT NO. FILE NO. PROJECT NO. 192232000



CONSTRUCTION NOTES:

- 2 INSTALL NEW L-824 #8, 5KV AIRFIELD CABLING FOR RUNWAY LIGHTING CIRCUIT IN EXISTING CONDUIT OR DUCTBANK. SEE RUNWAY CIRCUIT MAP SHEET E5.1 FOR CABLE ROUTING.
- 4 INSTALL NEW L-824 #8, 5KV AIRFIELD CABLING FOR TAXIWAY LIGHTING CIRCUIT IN EXISTING CONDUIT OR DUCTBANK. SEE TAXIWAY CIRCUIT MAP SHEET E5.2 FOR CABLE ROUTING.
- 8 INSTALL NEW 1-2" SCH. 40 PVC DIRECT BURIED CONDUIT AND L-824 #8, 5KV CABLING FOR RGL LIGHTING CIRCUIT. SEE DETAIL 1 SHEET E4.2 FOR TRENCH DETAIL.
- 19 INTERCEPT EXISTING HOMERUN DUCTBANK HANDHOLE WITH NEW 1-2" SCH. 40 PVC CONDUIT.
- 21 INSTALL (1) NEW 7.5KW, (2) NEW 10KW, AND (1) NEW 15KW FERRORESONANT CONSTANT CURRENT REGULATORS IN EXISTING AIRFIELD LIGHTING VAULT. SEE VAULT LAYOUT DETAILS SHEET E4.5.



DRAWING NO.

BY DATE

REVISIONS

JPT DESIGNED 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 JPT DRAWN 8/18/23 (669) 800-4130 JDB CHECKED 8/18/23

	SUBMITTED:	OROFESSION.
8/23	JAMIL S. SALAS	SPILS SA
DATE	SENIOR CIVIL ENGINEER	S A S
8/23		[일 No C74528]
DATE		Exp. 12-31-23/
8/23		CIVIL
DATE	SIGNATURE	OF CALIFOR

ROCELIA G. KMAK
PRINCIPAL CIVIL ENGINEER

### **REID-HILLVIEW AIRPORT**

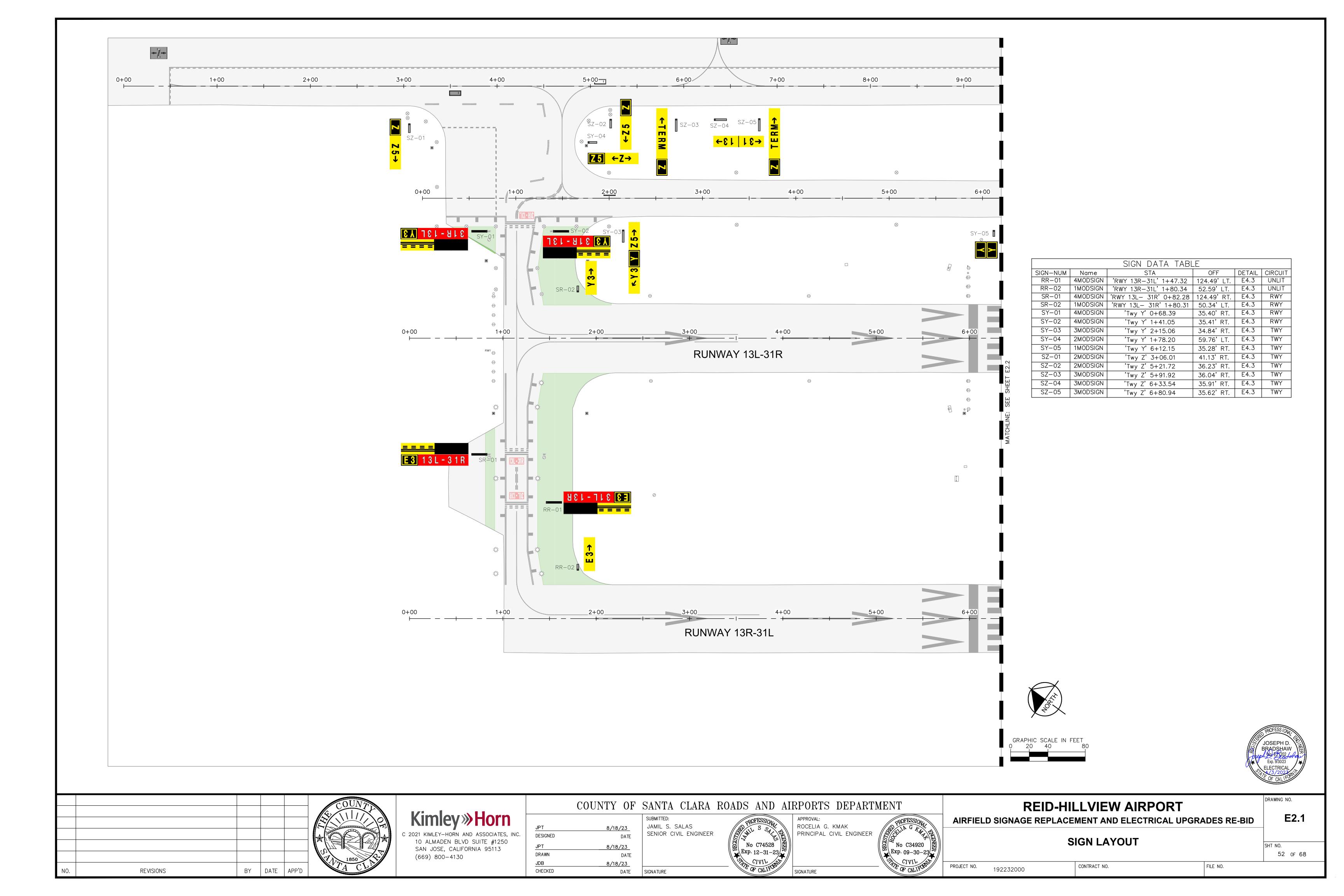
AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

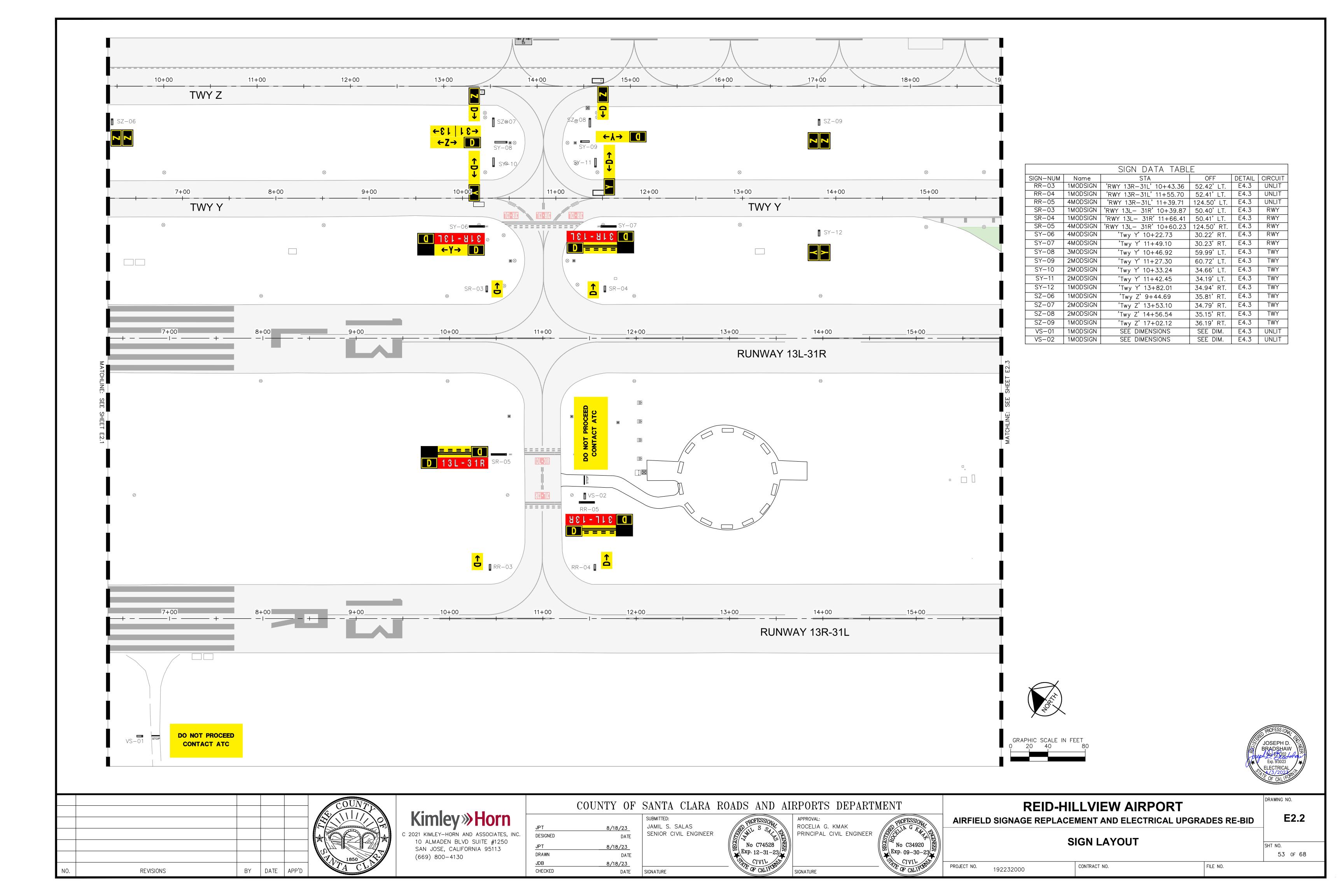
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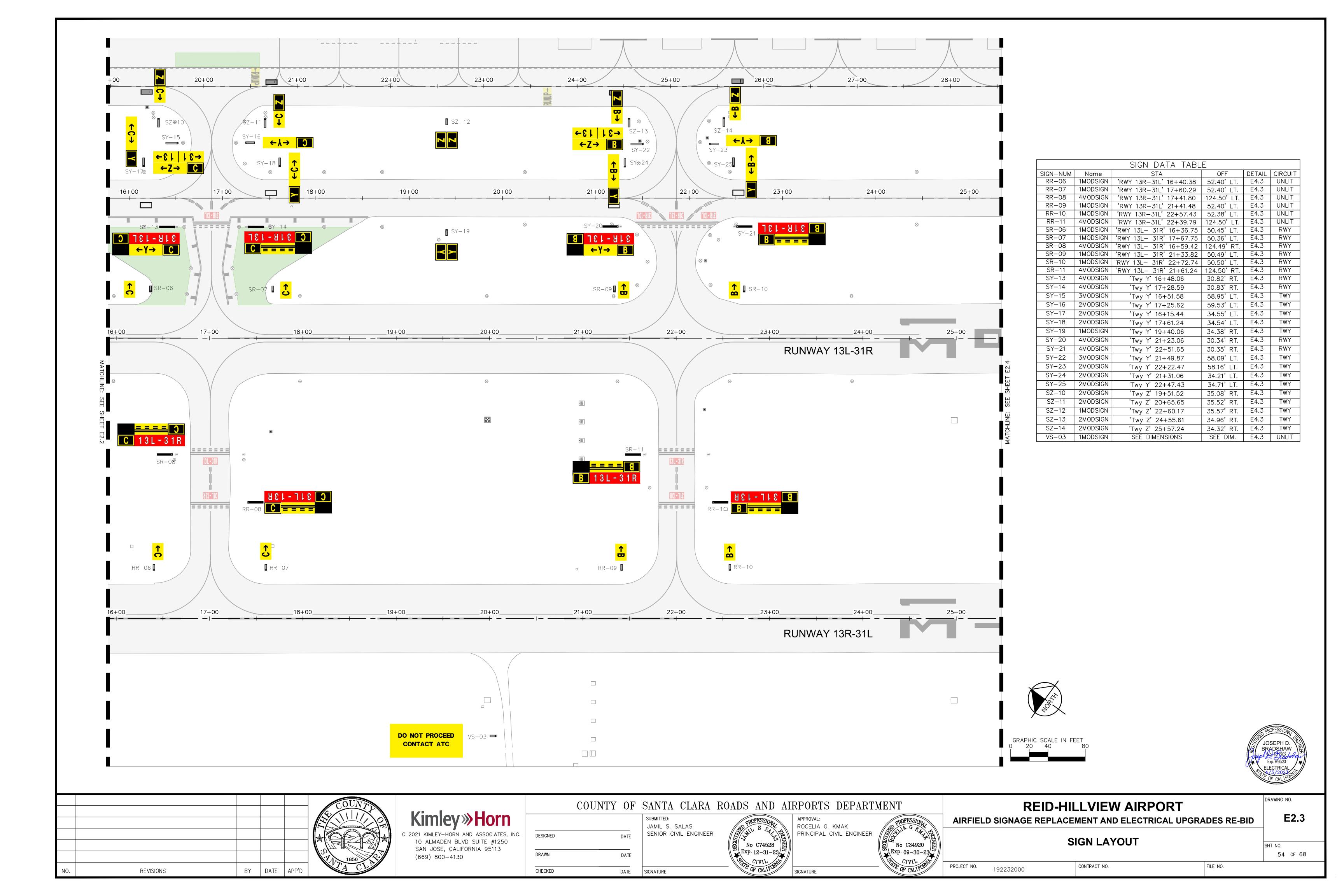
**ELECTRICAL LAYOUT** 

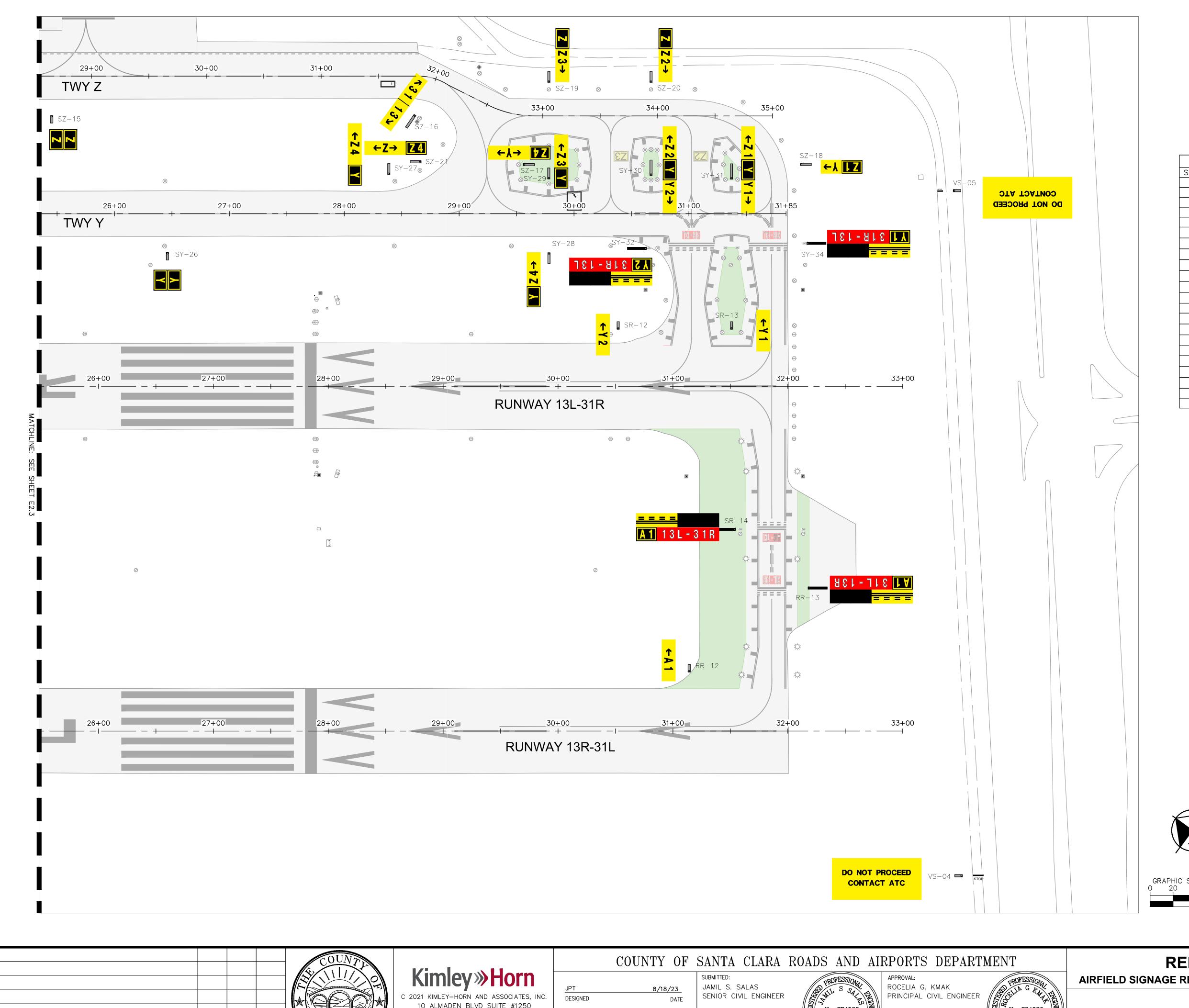
51 OF 68

FILE NO. PROJECT NO. CONTRACT NO. 192232000

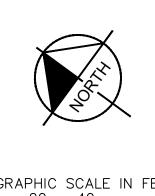








	SIGN DATA TABLE				
SIGN-NUM	Name	STA	OFF	DETAIL	CIRCUIT
RR-12	1MODSIGN	'RWY 13R-31L' 31+14.18	52.32' LT.	E4.3	UNLIT
RR-13	4MODSIGN	'RWY 13R-31L' 32+18.59	124.50' LT.	E4.3	UNLIT
SR-12	1MODSIGN	'RWY 13L- 31R' 30+52.33	50.56' LT.	E4.3	RWY
SR-13	1MODSIGN	'RWY 13L- 31R' 31+51.05	50.57' LT.	E4.3	RWY
SR-14	4MODSIGN	'RWY 13L- 31R' 31+53.57	124.50' RT.	E4.3	RWY
SY-26	1MODSIGN	'Twy Y' 26+46.11	34.61' RT.	E4.3	TWY
SY-27	2MODSIGN	'Twy Y' 28+38.75	35.03' LT.	E4.3	TWY
SY-28	2MODSIGN	'Twy Y' 29+78.18	34.85' RT.	E4.3	TWY
SY-29	2MODSIGN	'Twy Y' 29+78.00	31.18' LT.	E4.3	TWY
SY-30	3MODSIGN	'Twy Y' 30+67.03	34.28' LT.	E4.3	TWY
SY-31	3MODSIGN	'Twy Y' 31+37.03	31.03' LT.	E4.3	TWY
SY-32	4MODSIGN	'Twy Y' 30+62.19	30.21' RT.	E4.3	RWY
SY-34	4MODSIGN	'RWY 13L- 31R' 32+17.49	124.49' LT.	E4.3	RWY
SZ-15	1MODSIGN	'Twy Z' 28+65.51	35.18' RT.	E4.3	TWY
SZ-16	3MODSIGN	'Twy Z' 31+81.17	34.58' RT.	E4.3	TWY
SZ-17	2MODSIGN	'Twy Z' 32+86.75	42.69' RT.	E4.3	TWY
SZ-18	2MODSIGN	'RWY 13L- 31R' 32+11.84	193.19' LT.	E4.3	TWY
SZ-19	2MODSIGN	'Twy Z' 33+05.30	30.09' LT.	E4.3	TWY
SZ-20	2MODSIGN	'Twy Z' 33+94.27	30.10' LT.	E4.3	TWY
SZ-21	2MODSIGN	'Twy Z' 32+20.24	70.70' RT.	E4.3	TWY
VS-04	1MODSIGN	SEE DIMENSIONS	SEE DIM.	E4.3	UNLIT
VS-05	1MODSIGN	SEE DIMENSIONS	SEE DIM.	E4.3	UNLIT





DRAWING NO.

BY DATE

REVISIONS

2021 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113

(669) 800-4130

JPT	8/18/23
DESIGNED	DATE
JPT	8/18/23
DRAWN	DATE
JDB	8/18/23
CHECKED	DATE

SIGNATURE

PROJECT NO.

192232000

## **REID-HILLVIEW AIRPORT**

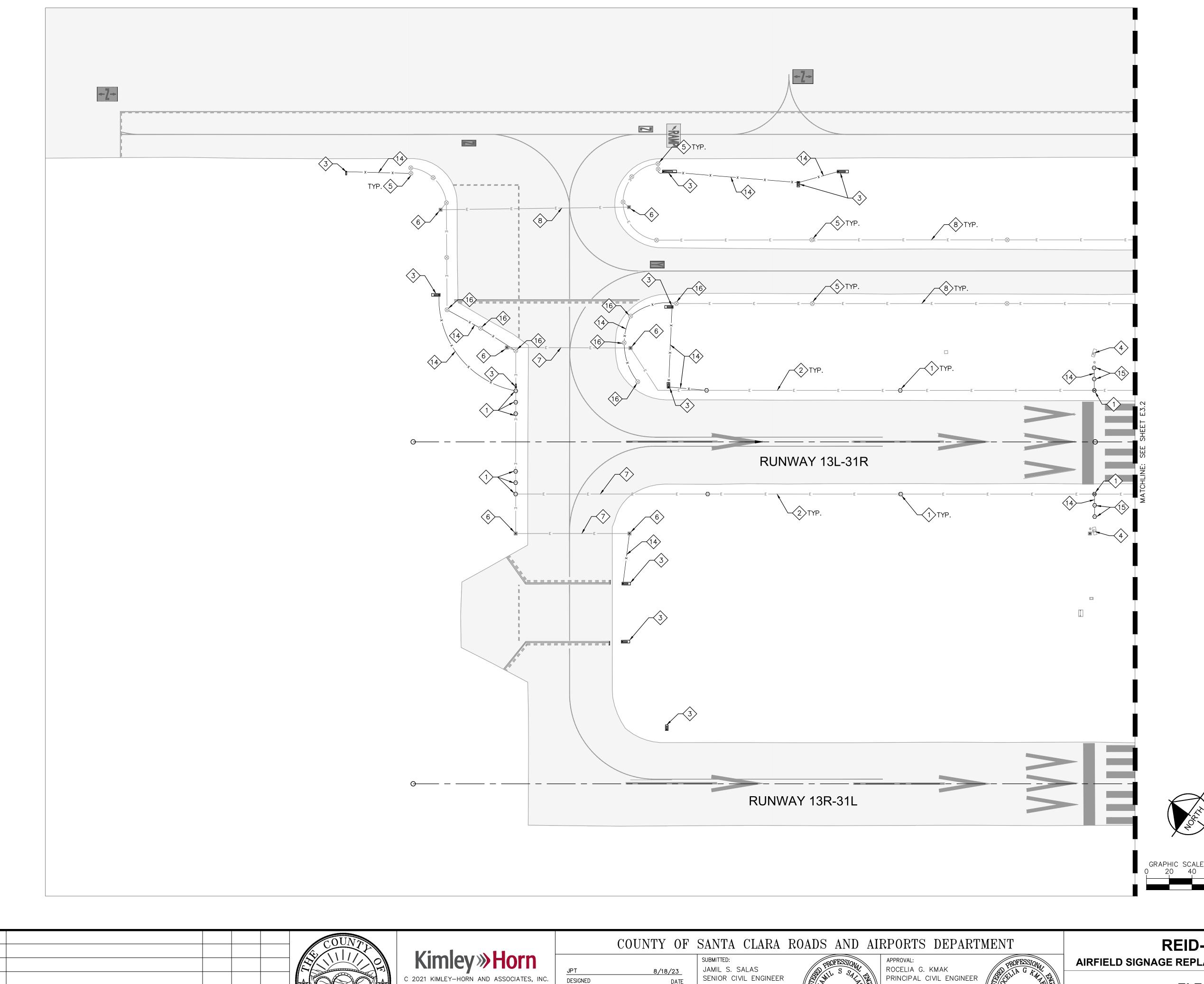
CONTRACT NO.

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

**SIGN LAYOUT** 

55 OF 68

**E2.4** 



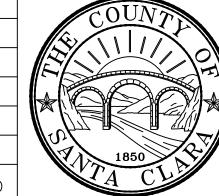
### **DEMOLITION NOTES**

- 1 REMOVE EXISTING RUNWAY EDGE/END/DISPLACED THRESHOLD LIGHT, ISOLÁTION TRANSFORMER, AND CABLING. PROTECT EXISTING BASE CAN IN PLACE FOR
- 2 EXISTING CONDUIT. REMOVE EXISTING RUNWAY CIRCUIT CABLING AND PROTECT CONDUIT IN PLACE FOR REUSE. MANDREL EXISTING CONDUIT BEFORE INSTALLING NEW CABLE.
- REMOVE EXISTING AIRFIELD GUIDANCE SIGN AND CONCRETE FOUNDATION. BACKFILL VOIDS OF DEMOLISHED SIGN BASES WITH CALTRANS CLASS II AGGREGATE BASE PER SPECIFICATION SECTION 110-7.
- 4 EXISTING REIL, OWNED AND MAINTAINED BY THE FAA. REIL TO BE SALVAGED AND REINSTALLED ON NEW FOUNDATION BY CONTRACTOR. INSTALL NEW CONDUIT AND CABLING AS NECESSARY TO RECONNECT TO EXISTING CIRCUIT FOR A FULLY FUNCTIONING AND OPERABLE SYSTEM. CONTRACTOR TO MATCH EXISTING REIL FOUNDATION. COORDINATE WITH FAA AS NEEDED.
- 5 EXISTING TAXIWAY EDGE LIGHT. REMOVE EXISTING ISOLATION TRANSFORMER AND CABLING. PROTECT EXISTING LIGHT FIXTURE AND BASE CAN IN PLACE.
- 6 EXISTING HANDHOLE. PROTECT IN PLACE.
- (7) EXISTING CONDUIT CROSSING. REMOVE EXISTING RUNWAY AND/OR TAXIWAY CIRCUIT CABLING AND PROTECT CONDUIT IN PLACE FOR REUSE. MANDREL EXISTING CONDUIT BEFORE INSTALLING NEW CABLE.
- 8 EXISTING CONDUIT. REMOVE EXISTING TAXIWAY CIRCUIT CABLING AND PROTECT CONDUIT IN PLACE FOR REUSE. MANDREL EXISTING CONDUIT BEFORE INSTALLING NEW
- REMOVE EXISTING CONDUIT AND AIRFIELD CABLING. BACKFILL VOIDS OF DEMOLISHED CONDUIT PER SPECIFICATION SECTION 110-5.
- (15) REMOVE EXISTING RUNWAY EDGE/END LIGHT, BASE CAN, ISOLATION TRANSFORMER AND CABLING.
- (16) REMOVE EXISTING TAXIWAY EDGE LIGHT, BASE CAN, ISOLATION TRANSFORMER AND CABLING.



DRAWING NO.

BY DATE REVISIONS



10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 DRAWN (669) 800-4130 CHECKED

8/18/23_ DATE	JAMIL S SENIOR
8/18/23_ DATE	
8/18/23	
DATE	SIGNATURE

PROJECT NO.

192232000

### **REID-HILLVIEW AIRPORT**

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

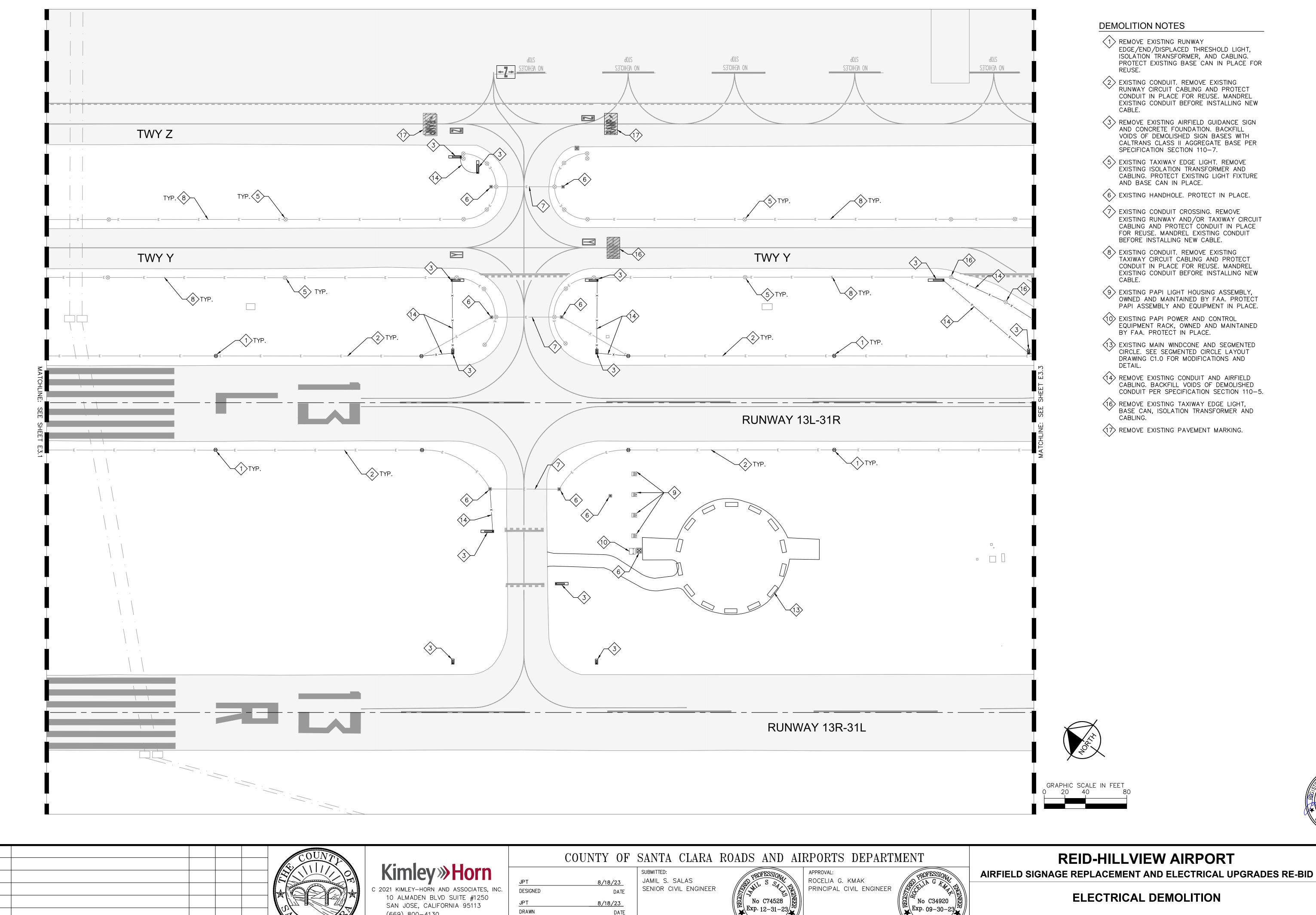
CONTRACT NO.

**ELECTRICAL DEMOLITION** 

FILE NO.

56 OF 68

E3.1



(669) 800-4130

BY DATE

REVISIONS

JDB CHECKED

8/18/23

DATE SIGNATURE

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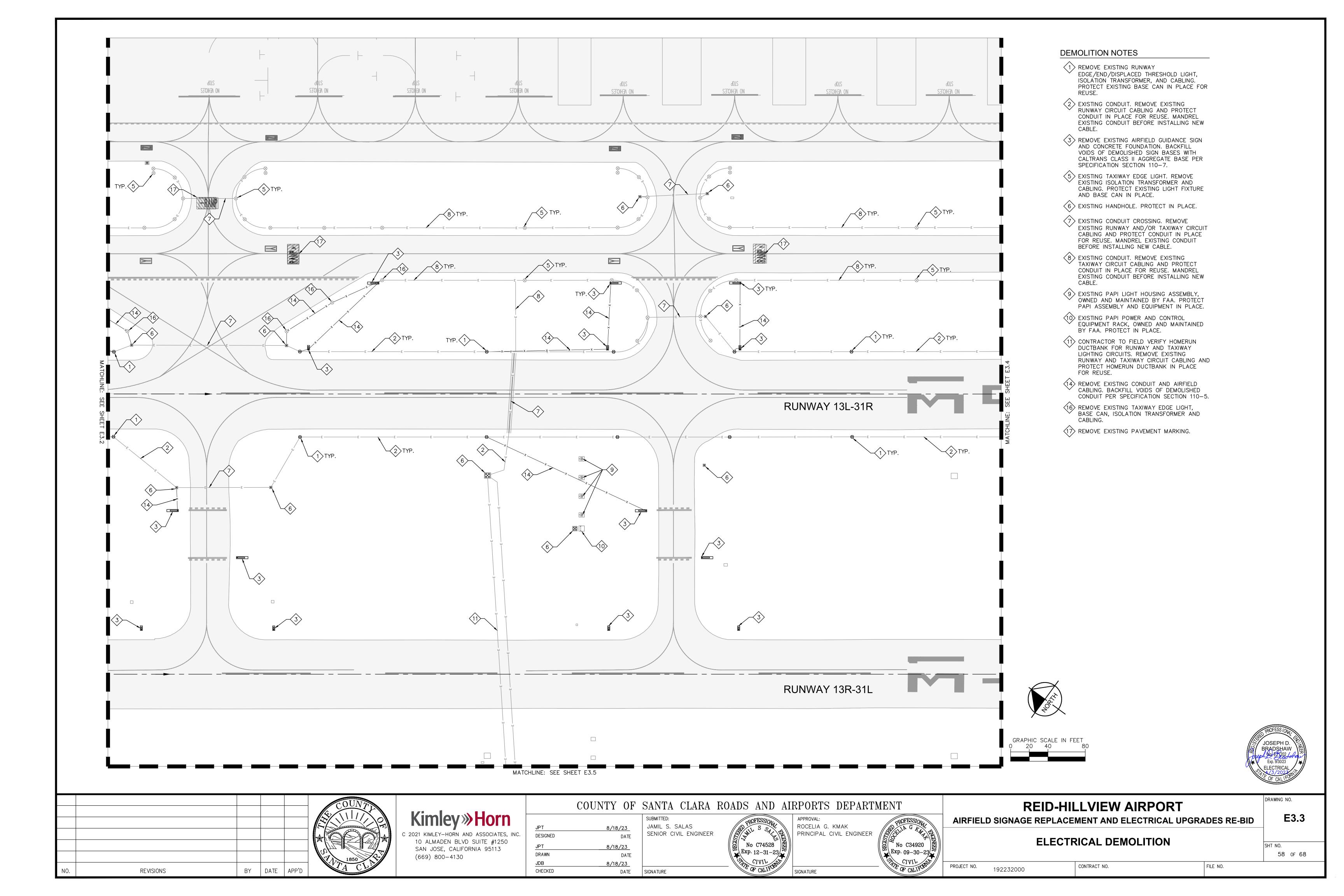
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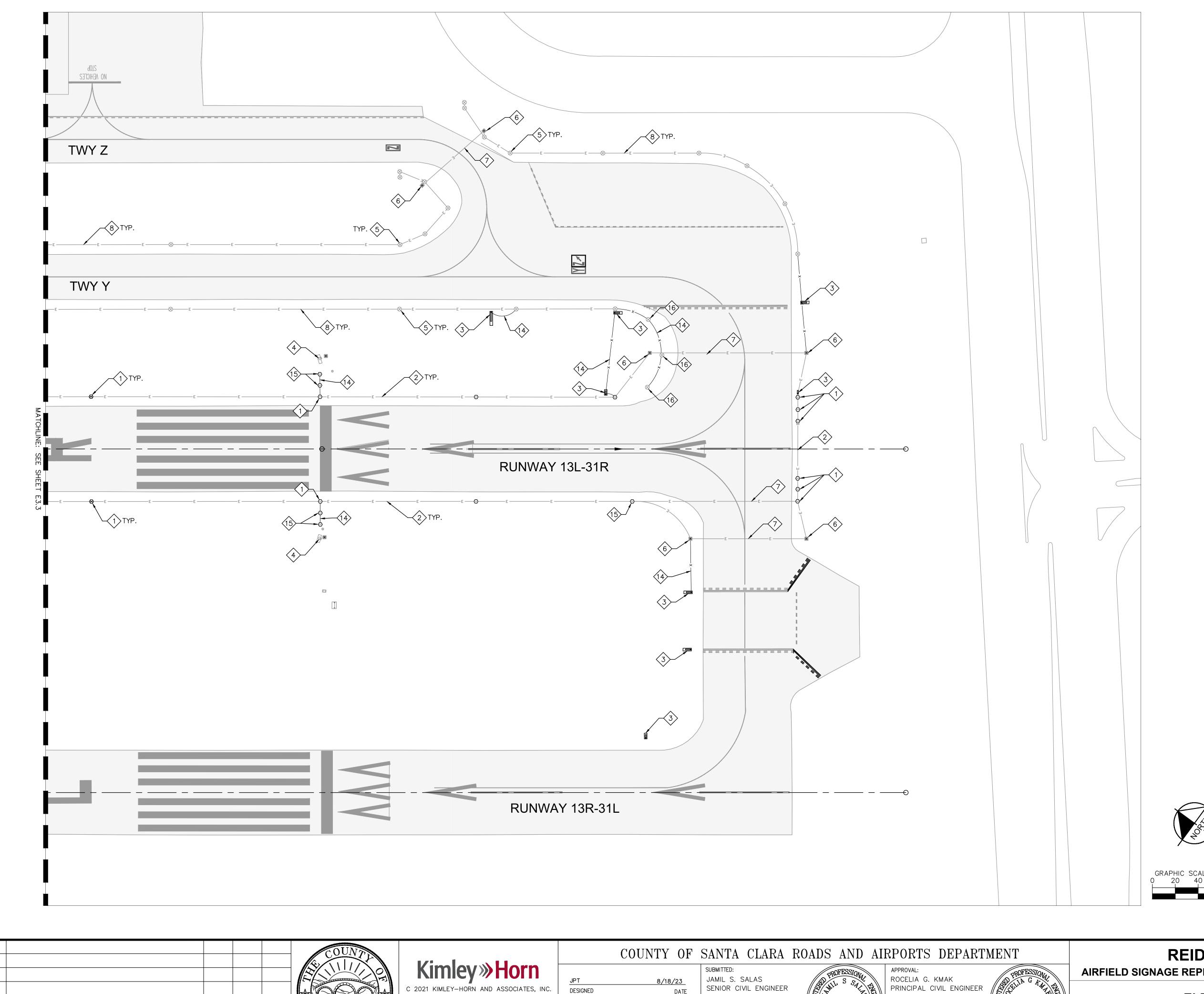
CONTRACT NO.

PROJECT NO.

192232000

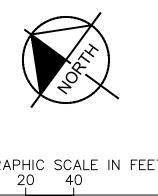
57 OF 68





### **DEMOLITION NOTES**

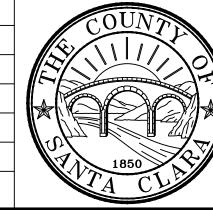
- 1> REMOVE EXISTING RUNWAY EDGE/END/DISPLACED THRESHOLD LIGHT, ISOLÁTION TRANSFORMER, AND CABLING. PROTECT EXISTING BASE CAN IN PLACE FOR
- 2 EXISTING CONDUIT. REMOVE EXISTING RUNWAY CIRCUIT CABLING AND PROTECT CONDUIT IN PLACE FOR REUSE. MANDREL EXISTING CONDUIT BEFORE INSTALLING NEW CABLE.
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- REMOVE EXISTING CONDUIT AND AIRFIELD CABLING. BACKFILL VOIDS OF DEMOLISHED CONDUIT PER SPECIFICATION SECTION 110-5.
- REMOVE EXISTING RUNWAY EDGE/END LIGHT, BASE CAN, ISOLATION TRANSFORMER AND CABLING.
- REMOVE EXISTING TAXIWAY EDGE LIGHT, BASE CAN, ISOLATION TRANSFORMER AND CABLING.





DRAWING NO.

BY DATE REVISIONS



10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

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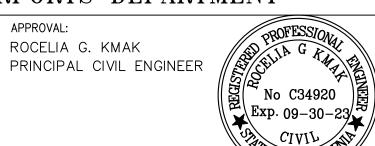
8/18/23

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JPT DRAWN

JDB CHECKED



PROJECT NO.

192232000

### **REID-HILLVIEW AIRPORT**

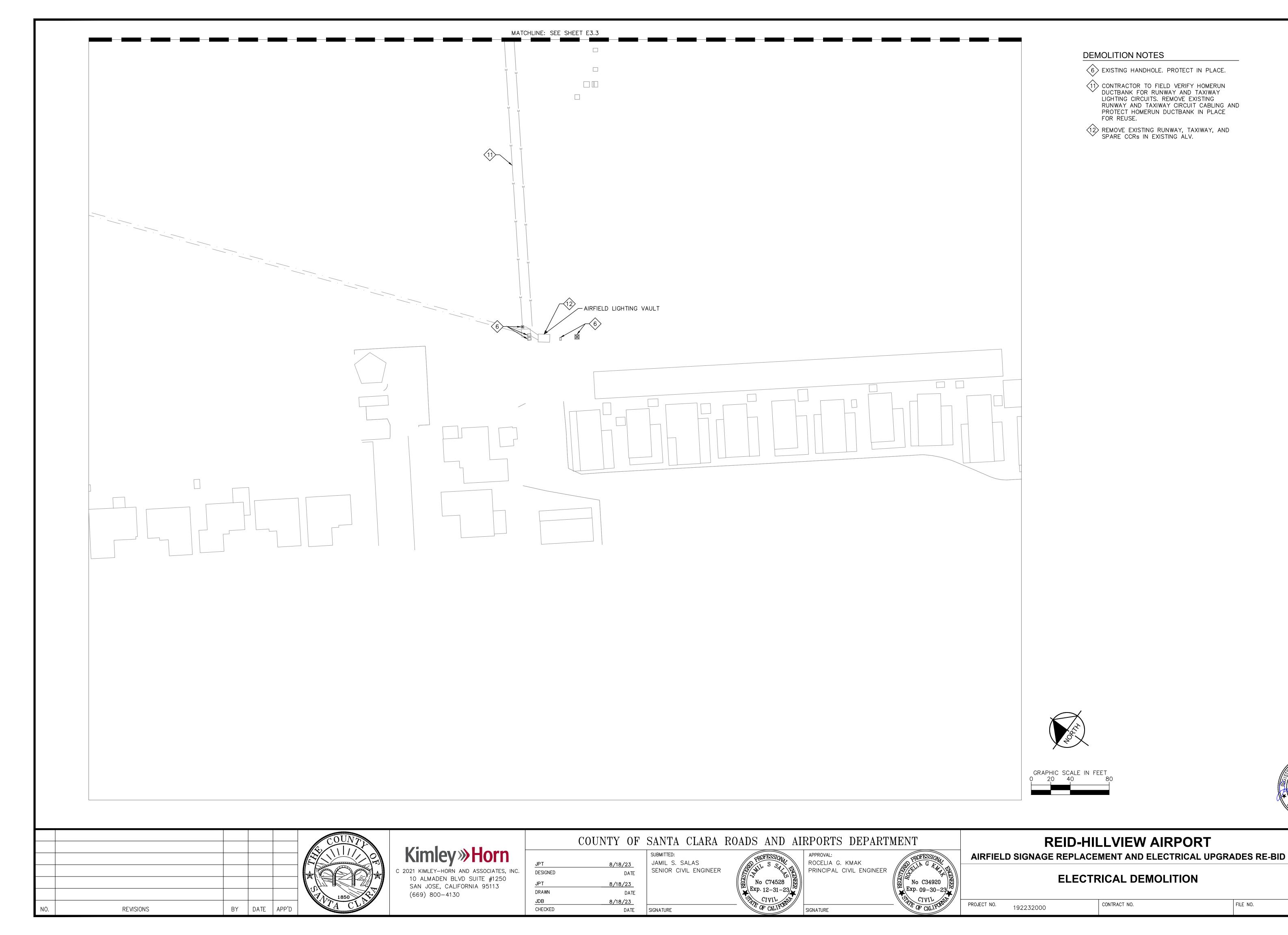
AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

CONTRACT NO.

**ELECTRICAL DEMOLITION** 

59 OF 68

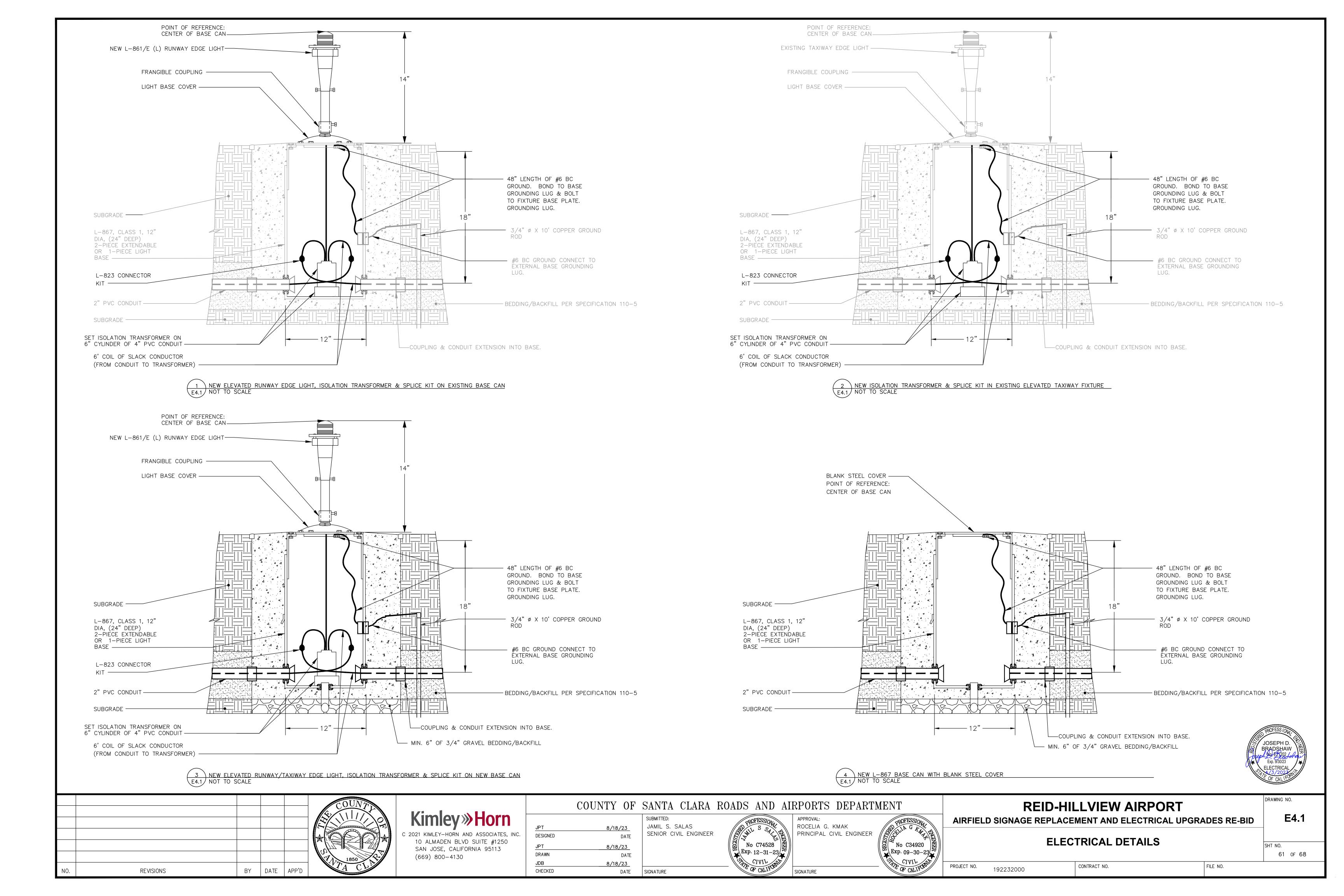
E3.4



60 OF 68

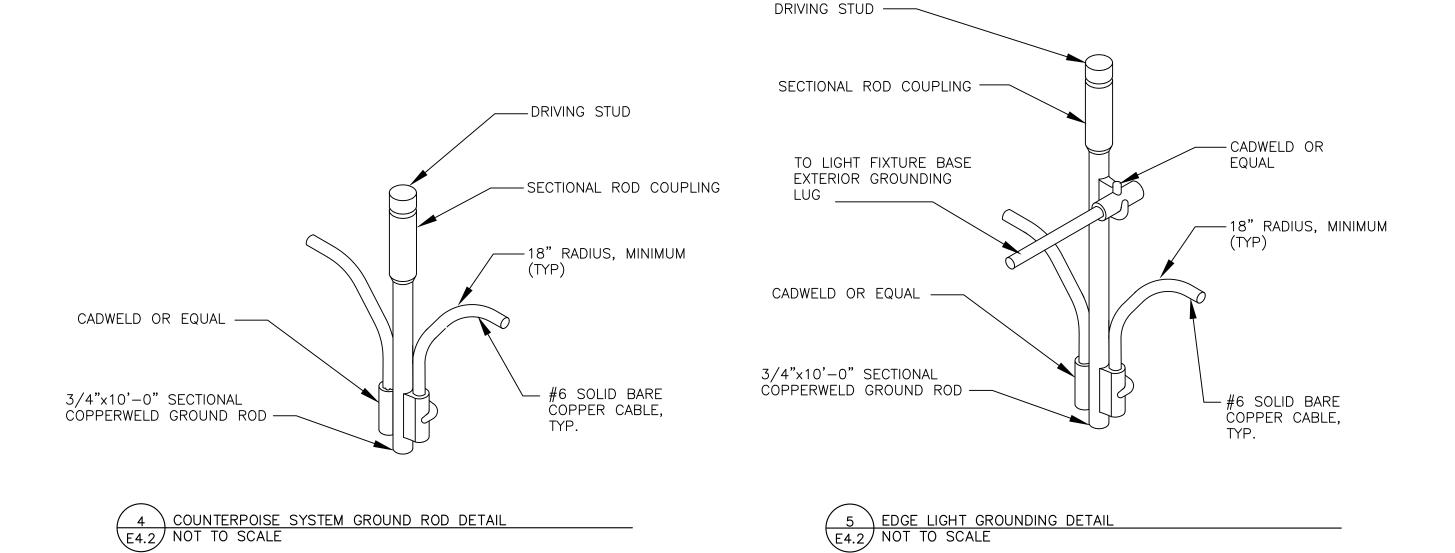
DRAWING NO.

E3.5



WIDTH AS REQUIRED FINISHED GRADE COUNTERPOISE (TYP) BACKFILL (NOTE 5) SEE NOTE 4.-WARNING TAPE (NOTE 3) \_ 24" (MIN) BACKFILL (NOTE 5) #6 B.C. A: 1-2"C (AS REQ'D) SCH. 40 PVC--NATIVE MATERIAL

\_\_CADWELD OR EQUAL COUNTERPOISE SYSTEM GROUNDING DETAIL NOT TO SCALE



BLUE RETROREFLECTIVE

1 \ TYPICAL TRENCH SECTION — DIRECT BURY (D.B.) E4.2 NOT TO SCALE

CONDUIT/DUCT & CABLE INSTALLATION NOTES:

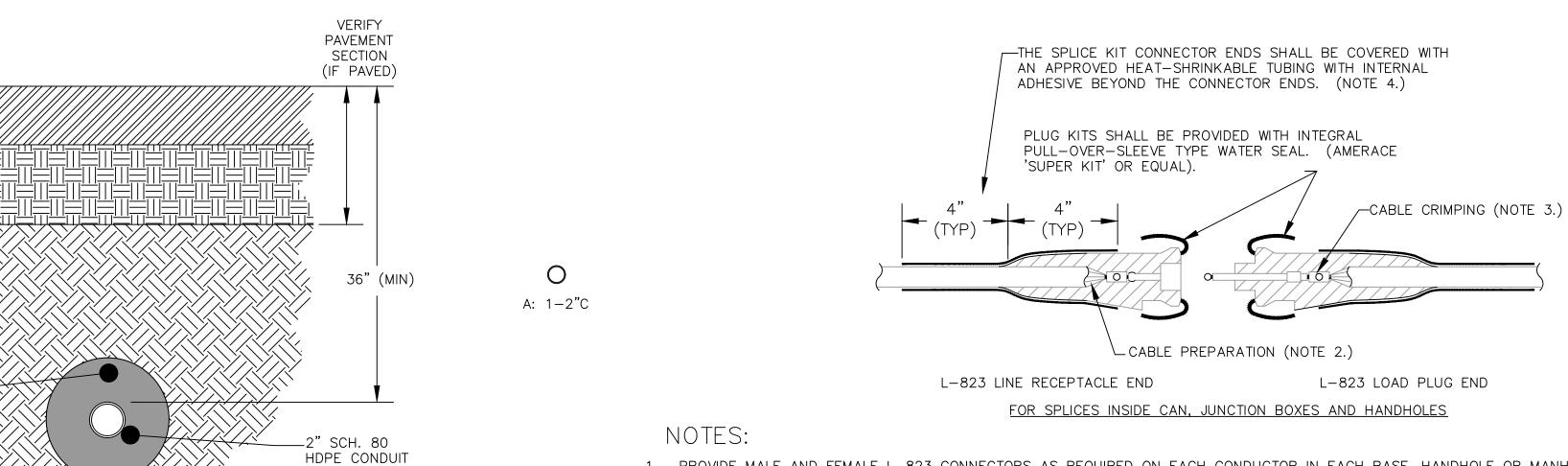
(E)AC PAVEMENT

(E)BASE COURSE

(E)SUB BASE

#6 B.C. COUNTERPOISE

- 1. SEE PLANS FOR REQUIRED DUCT SECTIONS. ALL OF THE SECTIONS SHOWN ON THIS DRAWING MAY NOT BE USED ON THIS PROJECT.
- 2. PROVIDE TRACER WIRES AND MULE TAPE IN ALL (NEW) UNUSED CONDUITS. PLUG CONDUIT ENDS IN HANDHOLES OR MANHOLES.
- 3. WARNING TAPE REQUIRED IN ALL AREAS. WHERE IN CONFLICT WITH THE COUNTERPOISE, INSTALL THE TAPE DIRECTLY ABOVE THE COUNTERPOISE.
- 4. U.O.N. INSTALL A #6 BARE COPPER (BC) COUNTERPOISE CONDUCTOR WITH EACH DUCT ASSEMBLY AND EACH SINGLE CONDUIT. EXOTHERMICALLY WELD TO GROUND RODS PER SPECS. DO NOT CONNECT TO BASES OR GROUNDING SYSTEM. COUNTERPOISE CONDUCTOR TO BE PULLED EXTERIOR TO DIRECTIONAL BORE.
- 5. BACKFILL-D.B. APPLICATION: FILL FROM WARNING TAPE TO BOTTOM OF PAVEMENT WITH P-152. NATIVE BACKFILL USED UP TO WARNING TAPE.
- 6. REFER TO SPECIFICATION ITEM 110-13 FOR CONDUIT/DUCT SPACER SYSTEM.

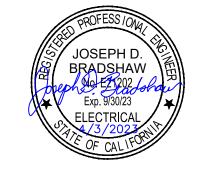


MARKER-BLUE WRAP 24" POST POST BLUE RETROREFLECTIVE MATERIAL FULL LENGTH ADHESIVE BASE. ATTACH SURFACE MOUNT PER MANUFACTURE RECOMMENDATIONS TAXIWAY RETROREFLECTIVE MARKER SHALL BE 'SAFE HIT AIRPORT MARKER' OR APPROVED EQUAL AND MUST COMPLY WITH FAA STAKE MOUNT ADVISORY CIRCULAR 150/5345-39D SPECIFICATION FOR L-853, RUNWAY AND TAXIWAY RETROREFLECTIVE MARKERS.

RETROREFLECTIVE MARKER DETAIL

- 1. PROVIDE MALE AND FEMALE L-823 CONNECTORS AS REQUIRED ON EACH CONDUCTOR IN EACH BASE, HANDHOLE OR MANHOLE IN THE FIELD CIRCUIT. THERE SHALL BE NO SPLICES BETWEEN LIGHTS, ONLY IN BASES OR HANDHOLES. AS MUCH AS PRACTICAL, HOMERUN CIRCUITS FROM THE FIELD CIRCUIT CONNECTION INTO THE MAIN DUCT SYSTEM BACK TO THE AIRFIELD LIGHTING VAULT SHALL BE INSTALLED WITHOUT SPLICES.
- 2. ALL CABLE ENDS SHALL BE PREPARED WITH THE USE OF A TAPERING TOOL SPECIFICALLY DESIGNED FOR USE WITH L-824 CABLES.
- 3. PLUG AND RECEPTACLE END FITTINGS SHALL BE CRIMPED ONTO THE CONDUCTOR BY USE OF AN AIRPORT PERSONNEL ACCEPTED HANDHELD RATCHETING TYPE CRIMPING TOOL. EACH CABLE TERMINATION SHALL BE SECURED BY TWO CRIMPS.
- 4. ALL CABLE CONNECTORS SHALL BE MADE WITH L-823 CONNECTOR AND AN L-823 AMERACE SUPER KIT OR EQUAL.
- 5. AT THE POINT OF CONNECTION WITH THE EXISTING FIELD CIRCUITS, INSTALL NEW L-823 PLUGS ON BOTH THE NEW AND EXISTING CABLES. VERIFY INSULATION TYPES OF BOTH NEW AND EXISTING CABLES AND COORDINATE WITH TERMINATION KITS TO ASSURE PROPER AND WATERPROOF FIT.





DRAWING NO.

BY DATE REVISIONS

2021 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

JPT	8/18/23
DESIGNED	DATE
JPT	8/18/23
DRAWN	DATE
JDB	8/18/23
CHECKED	DATE

DAIVIA CLAI	. V	IVOADS	AND
SUBMITTED:		OR	)FESSION:
JAMIL S. SALAS		(SQ) 11	SS
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		∥ Š ( No	C74528
		∥ <b>≅</b> \Exp.	12-31-23/
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SIGNATURE		— ATE C	F CALIFORD
LORDINATURE			//

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 SIGNATURE

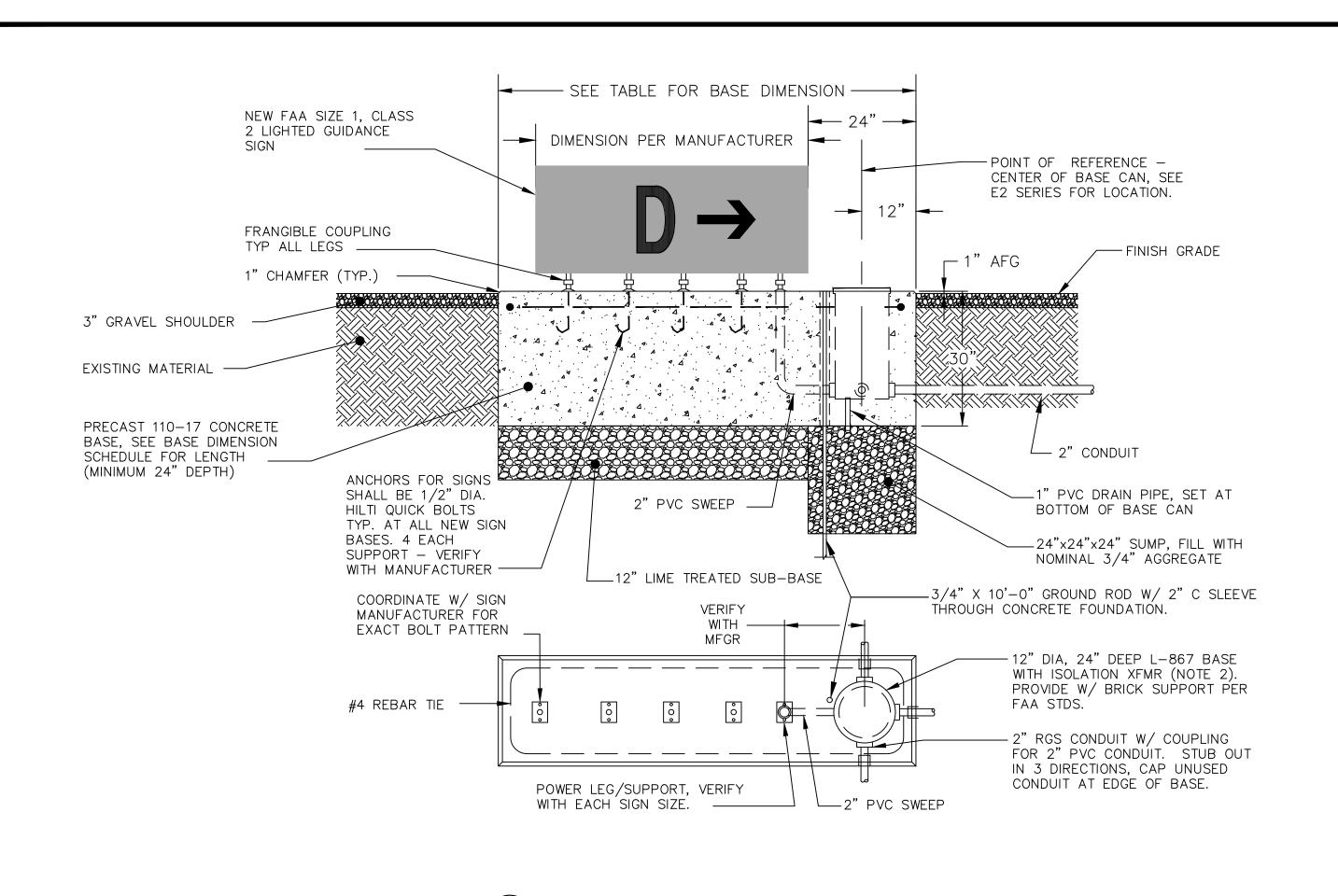
**REID-HILLVIEW AIRPORT** AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

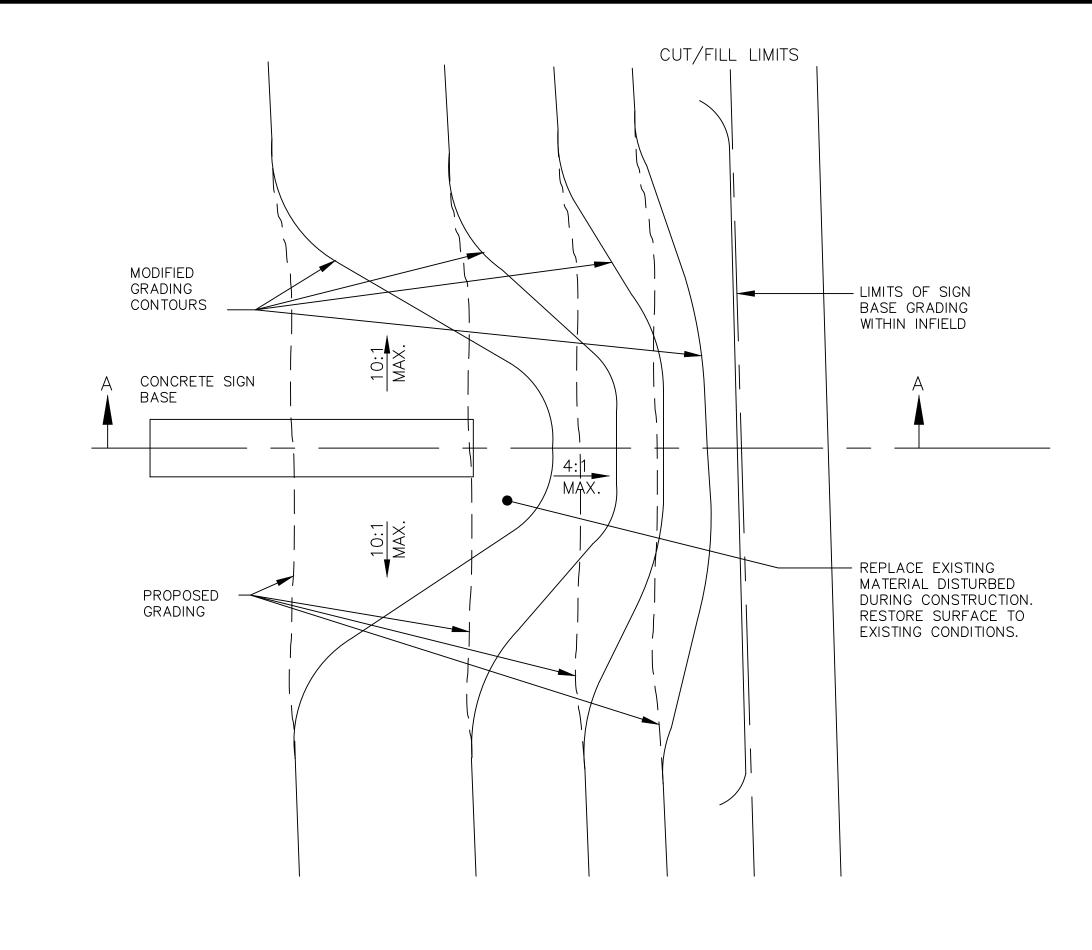
**ELECTRICAL DETAILS** 

SHT NO. 62 OF 68

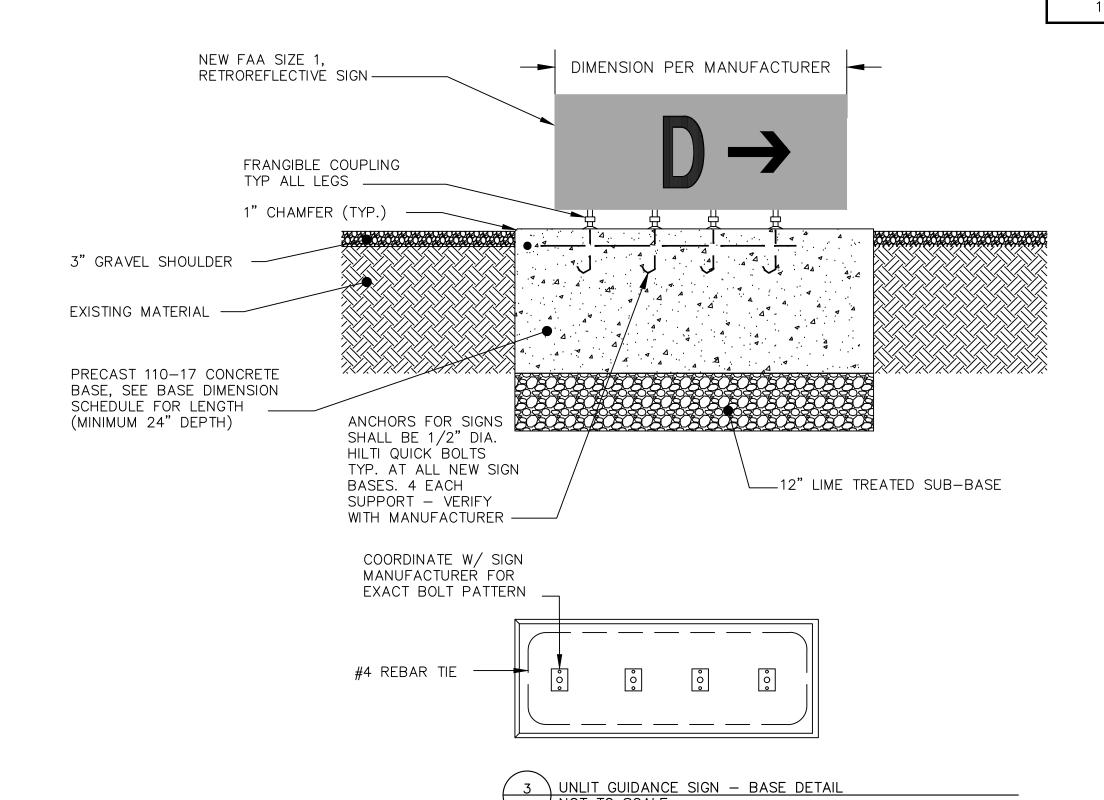
**E4.2** 

FILE NO. PROJECT NO. CONTRACT NO. 192232000





REFER TO (DWGS E2 SERIES) FOR INDIVIDUAL SIGN MODULES AND LEGENDS. COORDINATE EXACT BASE DIMENSIONS WITH SIGN DIMENSIONS. SIGN SIZE | BASE DIMENSIONS ONE MODULE TWO MODULE THREE MODULE FOUR MODULE

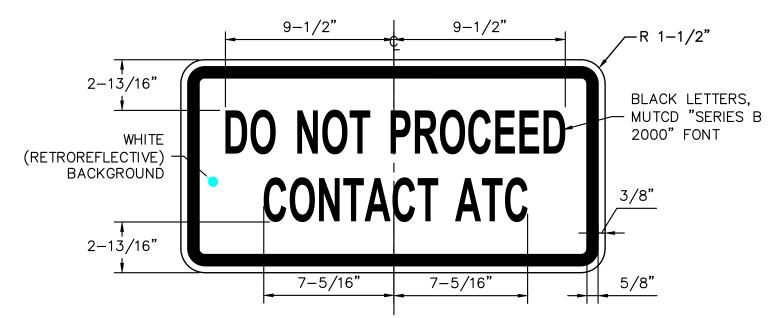


SIGN INSTALLATION NOTES:

- 1. VERIFY ALL DIMENSIONS OF SIGN FACES, SIGN PEDESTALS, AND SIGN FACE SPACING WITH MANUFACTURER BEFORE INSTALLATION.
- 2. PROVIDE NEW TRANSFORMERS WITH NEW SIGNS, SIZE PER MANUFACTURERS RECOMMENDATIONS.

96" L x 24" W | 126" L x 24" W | 156" L x 24" W

- 3. SEE EL SERIES DRAWINGS FOR SIGN CONTENTS AND LOCATIONS.
- 4. REFER TO DETAIL 2/E4.3 FOR GRADING DETAIL AROUND SIGN BASES AS REQUIRED TO MEET BASE REVEAL SLOPE REQUIREMENTS.



- 1. TEXT HEIGHT SHALL BE 2-1/2" HEIGHT, AND 5/16" STROKE WIDTH.
- 2. CONTRACTOR TO USE MUTCD HORIZONTAL RECTANGLE 24"X12" BLANK STANDARD. 3. REFER TO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) STANDARD HIGHWAY SIGNS 2009 REVISIONS 1 AND 2 (ENGLISH) EDITION FOR MORE INFORMATION.

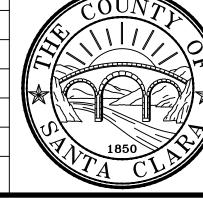


PROJECT NO.



DRAWING NO.

BY DATE REVISIONS



Kimley » Horn

2021 KIMLEY-HORN AND ASSOCIATES, INC. 10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130

JPT	8/18/23
DESIGNED	DATE
<u>JPT</u>	8/18/23
DRAWN	DATE
JDB	8/18/23

CHECKED

SUBMITTED: JAMIL S. SALAS SENIOR CIVIL ENGINEER No C74528 Exp. 12-31-23 DATE SIGNATURE

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 \Exp. 09-30-23

## **REID-HILLVIEW AIRPORT**

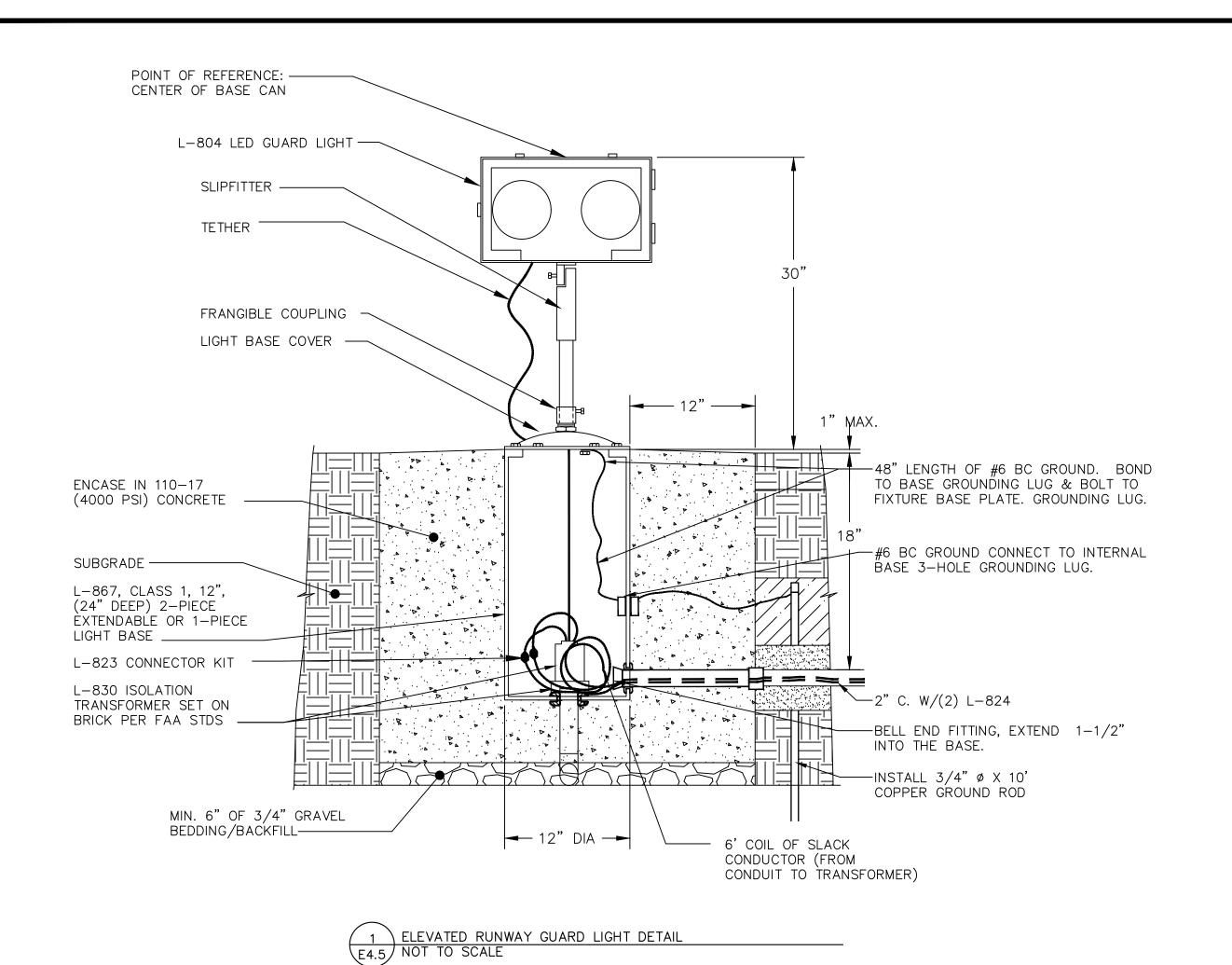
AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

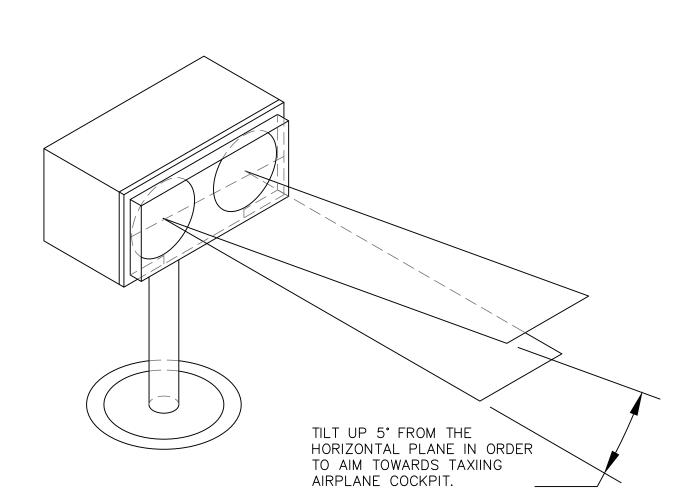
**ELECTRICAL DETAILS** 

SHT NO. 63 OF 68

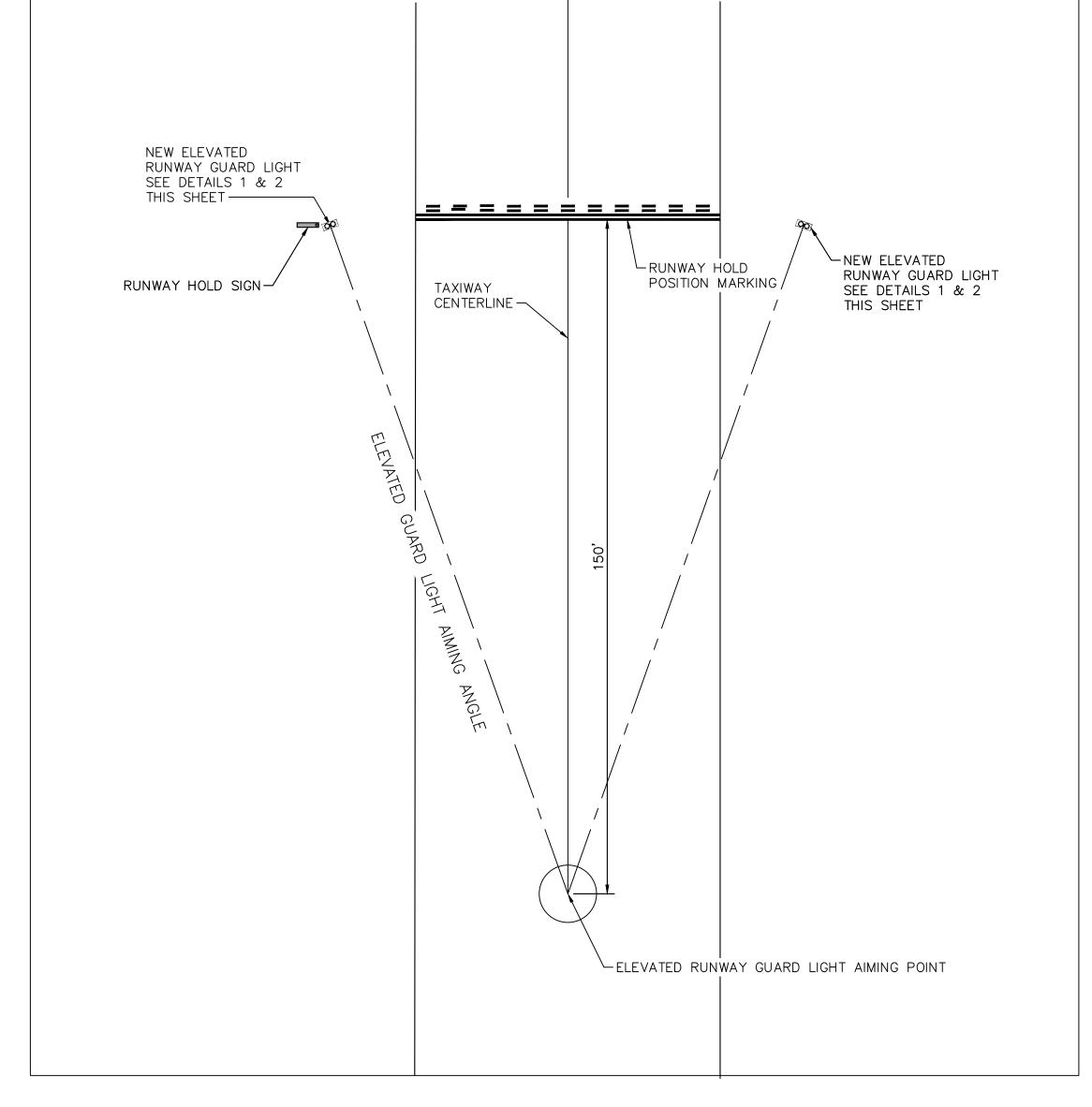
E4.3

FILE NO. CONTRACT NO. 192232000

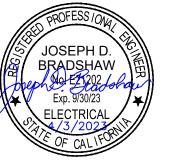




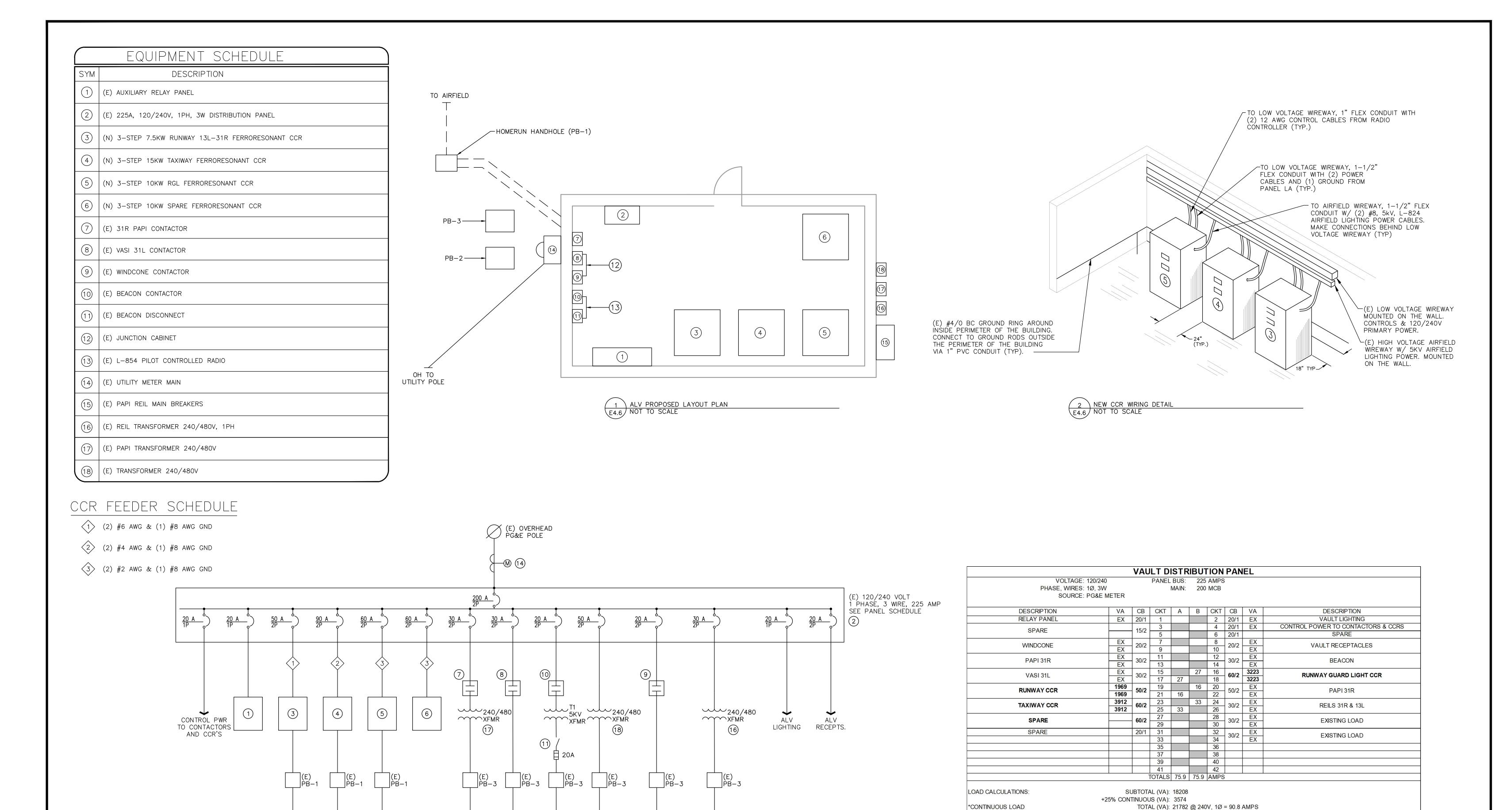
2 ELEVATED RUNWAY GUARD LIGHT AIMING DETAIL E4.5 NOT TO SCALE



TYPICAL PLAN VIEW



COUNT		COUNTY OF SANTA CLARA R	COADS AND AIRPORTS DEPARTMENT	REID-HILLVIEW AIRPORT	DRAWING NO.
Kin	nley»Horn	SUBMITTED:  8/18/23  JAMIL S. SALAS	APPROVAL:  ROCELIA G. KMAK  ROCELIA G. KMAK	AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-B	ID E4.4
	ILEY-HORN AND ASSOCIATES, INC.  DESIGNED  LMADEN BLVD SUITE #1250	DATE SENIOR CIVIL ENGINEER	PRINCIPAL CIVIL ENGINEER	ELECTRICAL DETAILS	
SAN J	JOSE, CALIFORNIA 95113	8/18/23_ DATE	S   No C74528   S   Exp. 12-31-23   Exp. 09-30-23   Exp. 09-	LELCTRICAL DETAILS	SHT NO. 64 OF 68
1850 (669)	800-4130 DRAWN JDB	8/18/23_	CIVIL SIGNATURE	PROJECT NO. 192232000 CONTRACT NO. FILE NO.	



4 VAULT DISTRIBUTION PANEL SCHEDULE E4.6 NOT TO SCALE



		COUNT

REVISIONS

BY DATE

R/W CKT

T/W CKT



RGL CKT

SINGLE-LINE DIAGRAM
E4.6 NOT TO SCALE

Kimley»Horn 2021 KIMLEY-HORN AND ASSOCIATES, INC.

PAPI 31R VASI 31L BEACON

8/18/23 DESIGNED 10 ALMADEN BLVD SUITE #1250 8/18/23 SAN JOSE, CALIFORNIA 95113 DRAWN (669) 800-4130 JDB 8/18/23 CHECKED

PAPI 13L

	SUBMITTED:	PROFESSION
8/23	JAMIL S. SALAS	
DATE	SENIOR CIVIL ENGINEER	E ZILL OF E
8/23		[일 No C74528 )
DATE		Exp. 12-31-23
8/23		CIVIL CIVIL
DATE	SIGNATURE	OF CALIFOR

WINDCONE REILS 31R & 13L

COUNTY OF SANTA CLARA ROADS AND AIRPORTS DEPARTMENT ROCELIA G. KMAK PRINCIPAL CIVIL ENGINEER No C34920 Exp. 09-30-23

SIGNATURE

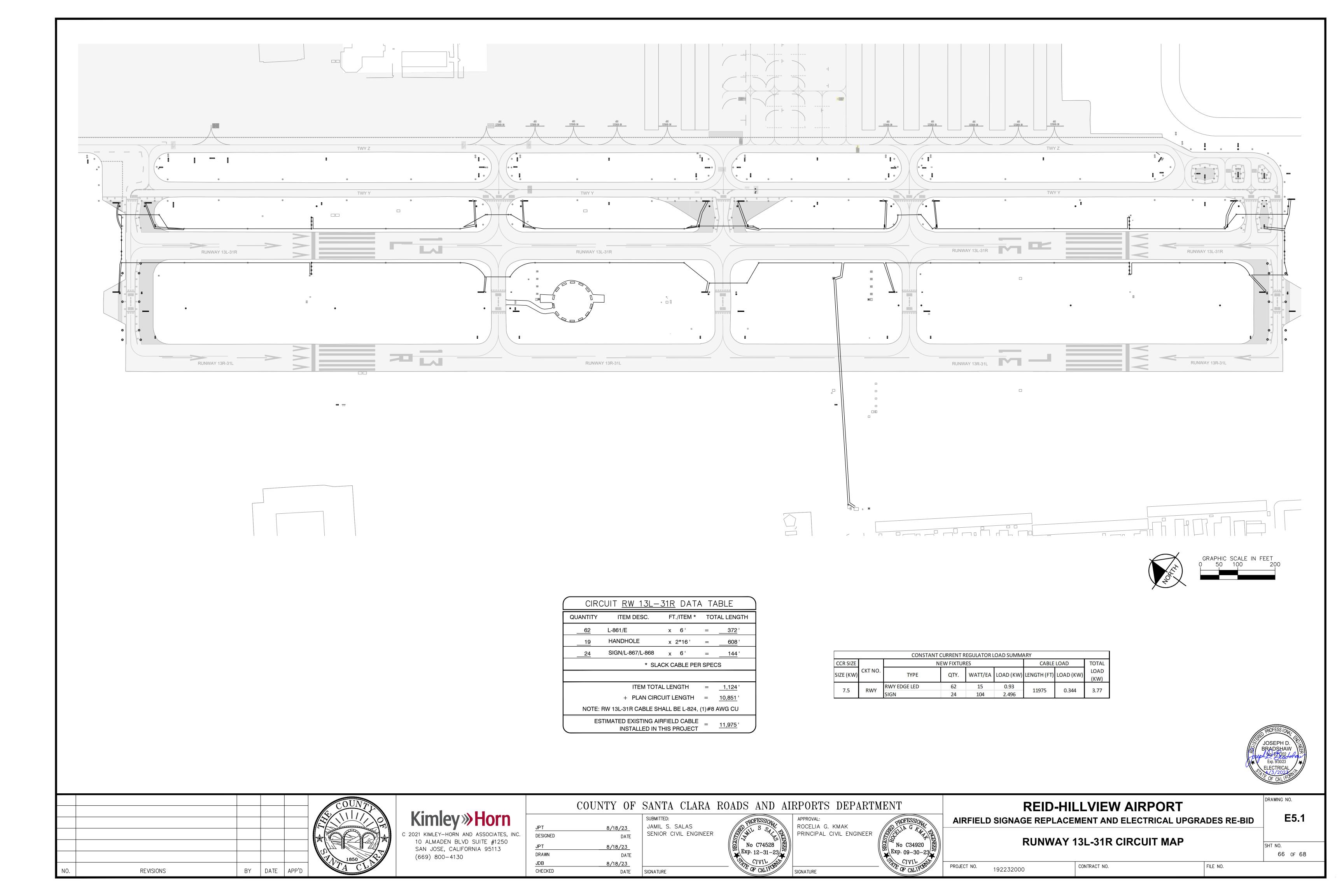
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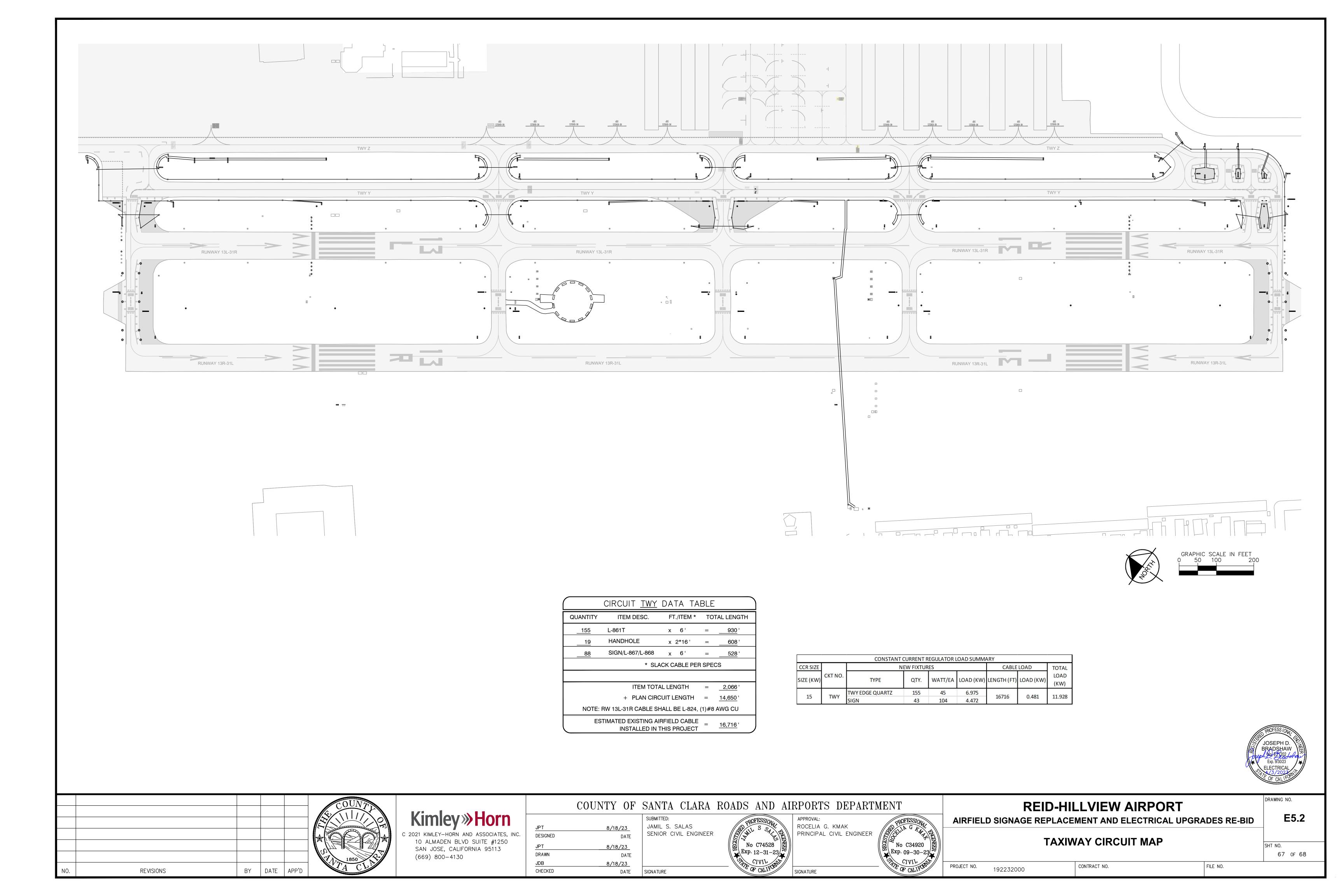
DRAWING NO. E4.5

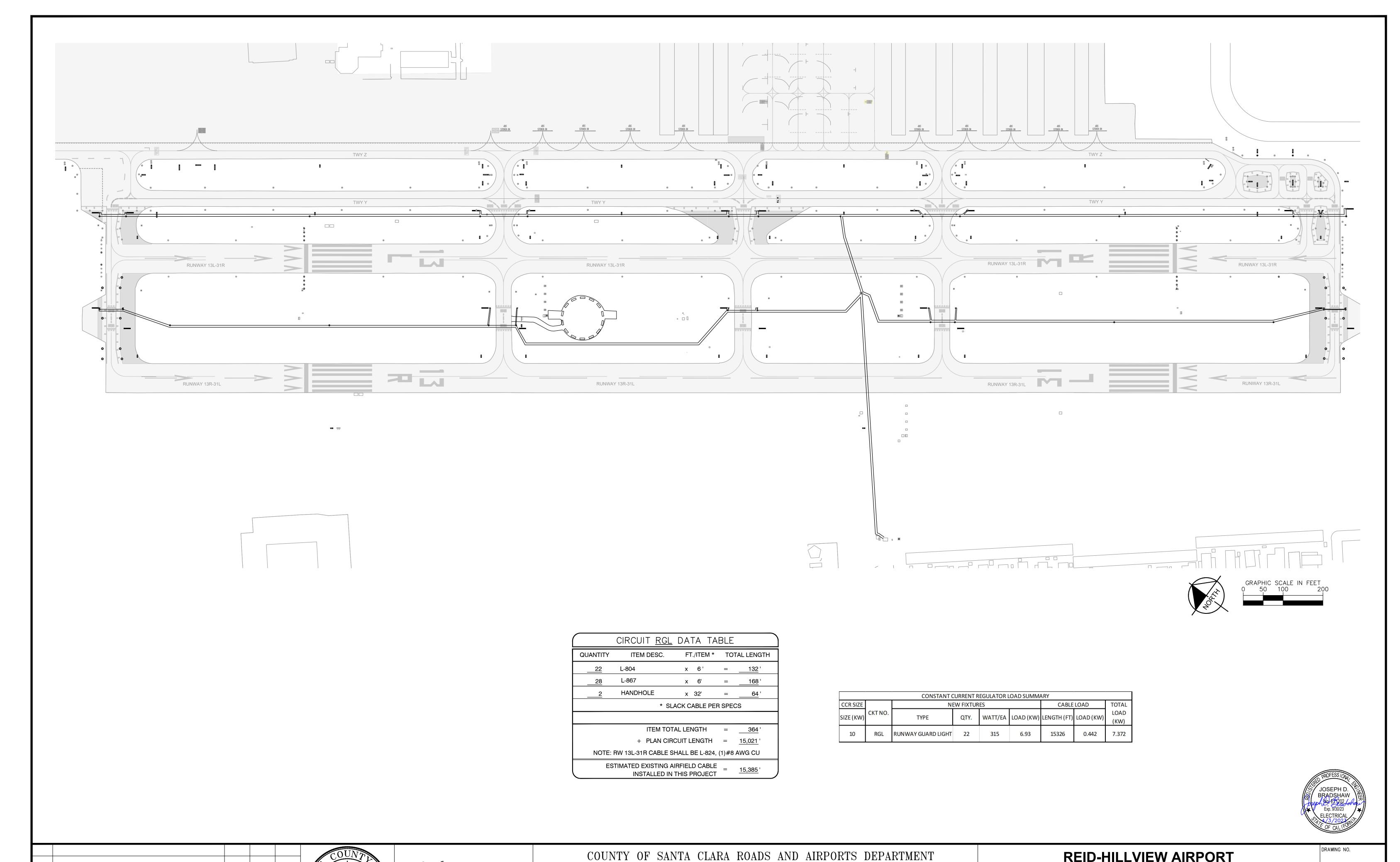
**ELECTRICAL DETAILS** 

SHT NO. 65 OF 68

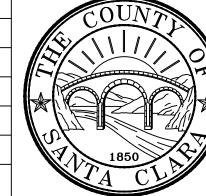
CONTRACT NO. FILE NO. PROJECT NO. 192232000











10 ALMADEN BLVD SUITE #1250 SAN JOSE, CALIFORNIA 95113 (669) 800-4130 CHECKED

PT	8/18/23
ESIGNED	DATE
PT	8/18/23
RAWN	DATE
DB	8/18/23

JAMIL S. SALAS SENIOR CIVIL ENGINEER DATE SIGNATURE

PRINCIPAL CIVIL ENGINEER

PROJECT NO.

192232000

### **REID-HILLVIEW AIRPORT**

AIRFIELD SIGNAGE REPLACEMENT AND ELECTRICAL UPGRADES RE-BID

**RUNWAY GUARD LIGHT CIRCUIT MAP** 

CONTRACT NO.

E5.3

68 OF 68